



# SAN FRANCISCO PLANNING DEPARTMENT

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## Discretionary Review Full Analysis

HEARING DATE MARCH 17, 2011

*Date:* March 10, 2011  
*Case No.:* 2010.0367DD  
*Project Address:* 54-62 Peralta Street  
*Permit Application:* 2009.1231.4050 / 2009.1231.4052  
*Zoning:* RH-2 (Residential, Two-Family) District  
40-X Height and Bulk District  
*Block/Lot:* 5512 / 029, 031  
*Project Sponsor:* M. Brett Gladstone, Esq.  
Gladstone & Associates  
177 Post Street, Penthouse  
San Francisco, CA 94108  
*Staff Contact:* Ben Fu – (415) 558-6613  
ben.fu@sfgov.org  
*Recommendation:* **Do not take DR and approve the project as proposed.**

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### PROJECT DESCRIPTION

The proposal is to construct two new three-story, two-family dwellings on two adjacent down-sloping lots, resulting in a total of four new dwelling units. The project site is located on the north side of Peralta Street, between Holladay Avenue and Hampshire Street, in a RH-2 (Residential, Two-Family House) District and a 40-X Height and Bulk District, and within the Bernal Heights Special Use District (BHSUD).

The project requires a variance from the parking requirement of the BHSUD in the Planning Code, to allow five parking spaces where seven are required. Planning Code Section 242(e)(4) sets the off-street parking requirements. The 54-56 Peralta Avenue building is required to provide three off-street parking spaces, and the 60-62 Peralta Avenue building is required to provide four off-street parking spaces. The 60-62 Peralta Avenue building will provide no off-street parking. The 54-56 Peralta Avenue building will provide five off-street parking spaces that will be accessible from the adjacent building. Two of the parking spaces will be reserved for the two units in the adjacent 60-62 Peralta Avenue building.

### SITE DESCRIPTION AND PRESENT USE

The project sites have always been vacant. 54 Peralta Avenue is measured 30 feet wide by 99 feet deep; 60 Peralta Avenue is measured 30 feet wide by 74 feet deep. The lots are laterally down-sloping with a five-foot grade difference from east to west, or an approximately 17 percent slope. The lots are also down-sloping from south to north (front to rear) with over 20 feet in grade difference, or an approximately 28 percent slope. The subject properties are not related to any important historic event, none of the owners or others associated with the property was historically important.

### SURROUNDING PROPERTIES AND NEIGHBORHOOD

The project site is located within an RH-2 Zoning District. These districts are devoted to one-family and two-family houses, with the latter commonly consisting of two large flats. Per Planning Code Section 206.1, structures in this district are finely scaled and usually do not exceed 25 feet in width or 40 feet in height. Building styles are often more varied than in single-family areas. Considerable ground-level open space is available, and it frequently is private for each unit.

The immediate neighborhood contains a mixture of single-, two- and multi-family dwellings. Although the majority of the properties on the block are single-family dwellings, including the DR Requestor's property, approximately eight of the 20 closest properties contain two units or more. The buildings on the same block face are mostly two stories, and the buildings on the opposite block face are mostly three stories.

### BUILDING PERMIT APPLICATION NOTIFICATION

TYPE	REQUIRED PERIOD	NOTIFICATION DATES	DR FILE DATE	DR HEARING DATE	FILING TO HEARING TIME
311/312 Notice	30 days	October 13, 2010 – November 12, 2010	November 12, 2010	March 17, 2011	124 days

### HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Posted Notice	10 days	March 07, 2011	February 18, 2011	27 days
Mailed Notice	10 days	March 07, 2011	February 18, 2011	27 days

### PUBLIC COMMENT

	SUPPORT	OPPOSED	NO POSITION
Adjacent neighbor(s)	0	1	N/A
Other neighbors on the block or directly across the street	N/A	N/A	N/A
Neighborhood groups	0	1	N/A

### DR REQUESTOR

Bob Besso, 1569 Hampshire Street, adjacent neighbor to the west of the subject property at 60 Peralta Street.

## DR REQUESTOR'S CONCERNS AND PROPOSED ALTERNATIVES

**Issue #1:** Proposed buildings' scales are out of character with the neighborhood.

**Issue #2:** Insufficient design information for significant sidewalk grade change.

**Issue #3:** Safety and flooding concerns regarding removal of retaining wall for driveway/garage.

**Issue #4:** Inadequate and inoperable design for off-street parking.

**Issue #5:** Suggested landscaping or setback to soften façade omitted from design.

**Issue #6:** Lack of cooperation or communications from owner and developer.

The DR Requestor proposes the following changes:

- Reduce building height, mass and density of the proposed development.
- Provide off-street parking for all vehicles in compliance with Planning Code.
- Cooperation/ Communication with developer and owner.
- Require a public works public hearing of proposed changes to the public right of way adjacent to the property.

Please refer to the attached *Discretionary Review Application* for additional information.

## PROJECT SPONSOR'S RESPONSE

- Reduce the width of each building by four feet to provide a side setback adjacent to the DR Requestor's property.
- Minimized the amount of excavation by eliminating the car lift and a steep ramp leading up to it, and replacing it with a one-car sliding pallet.
- Provide a three-foot rear setback at the northeast corner of the proposed building at 54 Peralta Street to allow additional light and air to the adjacent building at 48 Peralta Street.
- Provide a four-foot side setback on the west-facing side property line, or adjacent to the DR Requestor's property, to allow additional separation from the DR Requestor's rear yard of approximately 19 feet.
- Enhance sidewalk lighting for better security.
- Add landscaping at the front and rear.

Please refer to the attached *Response to Discretionary Review* for additional information.

## PROJECT ANALYSIS

### Neighborhood Context and Scale

The proposed project meets the intent of the zoning district with two-family dwellings on each vacant lot and provides code-complying rear yard and on-site open space. The block is characterized by a mixture of two- and three-story buildings containing single-, two-, and multi-family dwellings. The buildings on the opposite block face are mostly three stories. Although the neighborhood appears predominately single-family dwellings, the zoning allows for two-family dwellings. The proposal at 54 Peralta is

approximately 1.5 feet higher than the adjacent building at 48 Peralta measured from the ridge and 3 feet higher measured from the mid-point of the roof. Due to the steep lateral down-slope from east to west, the proposed 60 Peralta at 29 feet tall is approximately four feet lower than the proposed building at 54 Peralta, which is approximately 27 feet in height.

The DR Requestor's building at 1569 Hampshire (1) faces a different street, (2) is separated from the proposed project with its 19-foot rear yard, and (3) is approximately eight feet below the proposed 60 Peralta due the steep lateral down-slope from east to west.

The top story of the project is set back from the front façade, which complies with the Residential Design Guidelines (RDG) with respect to building scale in pages 24 – 25 of the RDG. Although the project is taller than the adjacent buildings, it is reasonable to allow an exposed upper story which is setback as recommended by the RDG. The setback also preserves the prevailing street wall height. The project is fully compliant with the Planning Code and the RDG.

#### Parking

The project provides a total of five off-street parking spaces for the proposed four units, exceeding the standard residential one-to-one requirement. The project sites are also in close proximity of Muni lines 8, 9, 27, and 33. All five spaces are provided at 54 Peralta, accessed by a 10-foot wide curb cut and driveway, thus minimizing the number of curb cuts on the narrow and steeply sloping street.

#### Rear Yard

Within BHSUD, the minimum required rear yard depth for an RH-2 District is 45 percent of the total depth of the lot on which the building is situated. Rear yards shall be provided at grade level and at each succeeding level or story of the building. Rear yards are provided to enhance the mid-block open space. The project meets the Code requirement by providing the required rear yard. 45 percent of the total lot depth at 54 Peralta is approximately 44 feet six inches; a 44-foot six-inch rear yard is provided.

45 percent of the total lot depth at 60 Peralta is approximately 33 feet. The proposed top story is further setback on the side by 4 feet and by approximately 16 feet at the rear, in addition to the 7-foot setback at the front. The four-foot side setback contributes to the DR Requestor's 19-foot rear yard, providing a total separation of approximately 23 feet between the two buildings.

It should be noted that the DR Requestor's building at 1569 Hampshire is non-complying in terms of rear yard. The minimum required rear yard for the DR Requestor's property is approximately 31 feet; the building encroaches into the required rear yard by 12 feet, leaving a rear yard of approximately 19 feet. Any adverse impacts to the DR Requestor's property are exacerbated by the non-complying nature of the structure, not by the project, which is code-complying. If the DR Requestor's property complied with the current code standards, it would enjoy a larger rear yard and reasonably less impact. Finally, provision of code complying rear yard respects the mid-block open space.

#### Light and Air

The proposed project is setback approximately 23 feet from the DR Requestor at 1569 Hampshire Street. The DR Requestors would be minimally affected by the proposal in terms of light and air. The project

proposes a four-foot side setback to allow further separation between the buildings even though there is no established pattern for side setbacks or side spacing between buildings.

#### View and Privacy

Discretionary Review shall not be used to alter or disapprove a building permit application based solely on these issues. There must be an extraordinary situation where a proposed project would have an unusual impact on privacy to neighboring properties in order for the request of a Discretionary Review to be considered.

The D.R. requestors would still enjoy ambient light, air, view, and some privacy through all of the windows. Given the dense urban environment, it is reasonable to assume that some level of impacts to view or privacy would occur.

#### **VARIANCE**

Planning Code Section 242(e)(4) sets the off-street parking requirements in the BHSUD. The 54-56 Peralta Avenue building is required to provide three off-street parking spaces, and the 60-62 Peralta Avenue building is required to provide four off-street parking spaces. The 60-62 Peralta Avenue building will provide no off-street parking. The 54-56 Peralta Ave building will provide five off-street parking spaces that will be accessible from the adjacent building. Two of the parking spaces will be reserved for the two units in the adjacent 60-62 Peralta Avenue building. Therefore, the project requires a variance from the parking requirement of the Bernal Heights Special Use District in the Planning Code, to allow five parking spaces where seven are required.

#### **ENVIRONMENTAL REVIEW**

The Department has determined that the proposed project is exempt from environmental review, pursuant to CEQA Guideline Section 15303.

#### **RESIDENTIAL DESIGN TEAM REVIEW**

The RDT supports the 7'-0" setback at the top floor due to the unique grade change along the public right-of-way. The setback maintains the existing two-story scale at the street, and makes the upper floor subordinate to the primary façade. (RDG, pg. 24-25, 28-29).

The RDT supports the scale and massing of the proposed buildings, as they are consistent with the scale and massing of the surrounding buildings, with regard to the height, depth, and fenestration pattern. The building is no greater than 30'-0" above grade, which is compatible with the surrounding context. (RDG, pg. 24-26).

The RDT supports the design of the proposed parking, in that the amount of building frontage dedicated to parking access is minimized by its consolidation in one building. (RDG, pg. 34-37).

Concerns about changes to the public right of way, including drainage, flooding, and safety, are not RDG issues.

With the proposed changes, the discretionary review requests are considered not to be extraordinary or exceptional.

Under the Commission's pending DR Reform Legislation, this project would be referred to the Commission, as this project involves new construction on a vacant lot.

#### BASIS FOR RECOMMENDATION

- The project is within the permitted building envelope.
- The project complies with the height limit, respects the topography and provides the required rear yard.
- The project respects the mid-block open space and is consistent with the Residential Design Guidelines, which requires front setback for exposed upper stories.
- The project is generally compatible with the mass and scale of properties along the block-face, which contains a mixed pattern of development.
- The project is an appropriately scaled infill development.
- All four units are considered family housing by providing three three-bedroom units and one two-bedroom unit.

<b>RECOMMENDATION:</b> Do not take DR and approve the project as proposed.
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#### Attachments:

Environmental Determination

Block Book Map

Sanborn Map

Aerial Photographs

Context Photos

Section 311 Notice

DR Application

Sponsor Submittal:

- Response to DR Application
- 3-D Rendering
- Reduced Plans

## Design Review Checklist

### NEIGHBORHOOD CHARACTER (PAGES 7-10)

QUESTION	
The visual character is: (check one)	
Defined	X
Mixed	

**Comments:** The block consists of mostly multi-family dwellings of various heights and building depths. The block has an established pattern of mid-block open space, although many properties have non-complying structures in the rear yard.

### SITE DESIGN (PAGES 11 - 21)

QUESTION	YES	NO	N/A
<b>Topography (page 11)</b>			
Does the building respect the topography of the site and the surrounding area?	X		
Is the building placed on its site so it responds to its position on the block and to the placement of surrounding buildings?	X		
<b>Front Setback (pages 12 - 15)</b>			
Does the front setback provide a pedestrian scale and enhance the street?	X		
In areas with varied front setbacks, is the building designed to act as transition between adjacent buildings and to unify the overall streetscape?			
Does the building provide landscaping in the front setback?	X		
<b>Side Spacing (page 15)</b>			
Does the building respect the existing pattern of side spacing?			X
<b>Rear Yard (pages 16 - 17)</b>			
Is the building articulated to minimize impacts on light to adjacent properties?	X		
Is the building articulated to minimize impacts on privacy to adjacent properties?	X		
<b>Views (page 18)</b>			
Does the project protect major public views from public spaces?			X
<b>Special Building Locations (pages 19 - 21)</b>			
Is greater visual emphasis provided for corner buildings?			X
Is the building facade designed to enhance and complement adjacent public spaces?			X
Is the building articulated to minimize impacts on light to adjacent cottages?			X

**Comments:** The top story is set back from the front façade and the rear, which complies with the RDG with respect to building scale. The west-facing side property lines of the proposed buildings are setback by 4 feet to allow relief to the adjacent side setback, windows and rear yard. The overall building height is limited to less than 30 feet.

**BUILDING SCALE AND FORM (PAGES 23 - 30)**

QUESTION	YES	NO	N/A
<b>Building Scale (pages 23 - 27)</b>			
Is the building's height and depth compatible with the existing building scale at the street?	X		
Is the building's height and depth compatible with the existing building scale at the mid-block open space?	X		
<b>Building Form (pages 28 - 30)</b>			
Is the building's form compatible with that of surrounding buildings?	X		
Is the building's facade width compatible with those found on surrounding buildings?	X		
Are the building's proportions compatible with those found on surrounding buildings?	X		
Is the building's roofline compatible with those found on surrounding buildings?	X		

**Comments:** The subject block and the immediate neighborhood consist of a mixture of single-, two- and multi-unit dwellings. The proposal at 54 Peralta is approximately 1.5 feet higher than the adjacent building at 48 Peralta measured from the ridge and 3 feet higher measured from the mid-point of the roof. Due to the steep lateral down-slope from east to west, the proposed 60 Peralta at 29 feet tall is approximately four feet lower than the proposed building at 54 Peralta, which is approximately 27 feet in height. The top story is setback from the front and the rear, and minimizes the impact to adjacent buildings. The project provides the minimum required rear yard of 45 percent, and a maximum building height of 30 feet.

**ARCHITECTURAL FEATURES (PAGES 31 - 41)**

QUESTION	YES	NO	N/A
<b>Building Entrances (pages 31 - 33)</b>			
Does the building entrance enhance the connection between the public realm of the street and sidewalk and the private realm of the building?	X		
Does the location of the building entrance respect the existing pattern of building entrances?	X		
Is the building's front porch compatible with existing porches of surrounding buildings?	X		
Are utility panels located so they are not visible on the front building wall or on the sidewalk?	X		
<b>Bay Windows (page 34)</b>			
Are the length, height and type of bay windows compatible with those found on surrounding buildings?			X
<b>Garages (pages 34 - 37)</b>			
Is the garage structure detailed to create a visually interesting street frontage?	X		
Are the design and placement of the garage entrance and door compatible with the building and the surrounding area?	X		

Is the width of the garage entrance minimized?	X		
Is the placement of the curb cut coordinated to maximize on-street parking?	X		
<b>Rooftop Architectural Features (pages 38 - 41)</b>			
Is the stair penthouse designed to minimize its visibility from the street?			X
Are the parapets compatible with the overall building proportions and other building elements?			X
Are the dormers compatible with the architectural character of surrounding buildings?			X
Are the windscreens designed to minimize impacts on the building's design and on light to adjacent buildings?			X

**Comments:** The architectural features are compatible with the block-face pattern. The punched and framed entryway is maintained to show prominence. The design also respects the street façade by setting back the top story from the front building wall.

**BUILDING DETAILS (PAGES 43 - 48)**

QUESTION	YES	NO	N/A
<b>Architectural Details (pages 43 - 44)</b>			
Are the placement and scale of architectural details compatible with the building and the surrounding area?	X		
<b>Windows (pages 44 - 46)</b>			
Do the windows contribute to the architectural character of the building and the neighborhood?	X		
Are the proportion and size of the windows related to that of existing buildings in the neighborhood?	X		
Are the window features designed to be compatible with the building's architectural character, as well as other buildings in the neighborhood?	X		
Are the window materials compatible with those found on surrounding buildings, especially on facades visible from the street?	X		
<b>Exterior Materials (pages 47 - 48)</b>			
Are the type, finish and quality of the building's materials compatible with those used in the surrounding area?	X		
Are the building's exposed walls covered and finished with quality materials that are compatible with the front facade and adjacent buildings?	X		
Are the building's materials properly detailed and appropriately applied?	X		

**Comments:** The building details are compatible with the block-face pattern. Buildings in the neighborhood primarily consist of wood siding and stucco as exterior building materials and wood framed windows and molding. The proposed building materials, and architectural elements are also consistent with the neighborhood context.



**SAN FRANCISCO  
PLANNING DEPARTMENT**

Date received:

**Environmental Evaluation Application**

The California Environmental Quality Act (CEQA) requires public agencies to review the environmental impacts of proposed projects. In San Francisco, environmental review under CEQA is administered by the Major Environmental Analysis (MEA) division of the Planning Department. The environmental review process begins with the submittal of a completed Environmental Evaluation (EE) Application to the Planning Department. Only the current EE Application form will be accepted. No appointment is required but staff is available to meet with applicants upon request.

The EE Application will not be processed unless it is completely filled out and the appropriate fees are paid in full. Checks should be made payable to the San Francisco Planning Department. See the current *Schedule of Application Fees* and contact the staff person listed below for verification of the appropriate fees. Fees are generally non-refundable. **Documents in italics are available online at [sfgov.org/planning](http://sfgov.org/planning).**

The EE Application is comprised of four parts. Part 1 is a checklist to ensure that the EE Application is complete; Part 2 requests basic information about the site and the project; Part 3 is a series of questions to help determine if additional information is needed for the EE Application; and Part 4 is a project summary table.

The complete EE Application should be submitted to the Planning Department staff as follows: For projects greater than 10,000 square feet in size and where Part 3 Questions #3, #8, #10, or #11 are answered in the affirmative, or for projects that require mitigation measures, please send the application materials to the attention of Ms. Fordham or Ms. Pereira. For all other projects, please send the application materials to the attention of Mr. Bollinger.

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<b>PART 1 – EE APPLICATION CHECKLIST</b>	<b>Provided</b>	<b>Not Applicable</b>
Two copies of this application with all blanks filled in	<input checked="" type="checkbox"/>	
Two sets of project drawings (see "Additional Information" at the end of page 4,)	<input checked="" type="checkbox"/>	
Photos of the project site and its immediate vicinity, with viewpoints labeled	<input checked="" type="checkbox"/>	
Fee	<input checked="" type="checkbox"/>	
<i>Supplemental Information Form for Historical Resource Evaluation</i> and/or Historic Resource Evaluation Report, as indicated in Part 3 Questions 1 and 2	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Geotechnical Report, as indicated in Part 3 Questions 3a and 3b	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<i>Tree Disclosure Statement</i> , as indicated in Part 3 Question 4	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Phase I Environmental Site Assessment, as indicated in Part 3 Question 8	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Additional studies (list)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Applicant's Affidavit.** I certify the accuracy of the following declarations:

- The undersigned is the owner or authorized agent of the owner(s) of this property.
- The information presented is true and correct to the best of my knowledge.
- I understand that other applications and information may be required.

Signed (owner or agent):   
(For Staff Use Only) Case No. 2010.0367E

Date: 3/5/2011  
Address: 54-54 Peralta St  
Block/Lot: 5512/029

**PART 2 – PROJECT INFORMATION**

**Owner/Agent Information**

Property Owner	<u>Tom Aquilina</u>	Telephone No.	<u>415-706-4780</u>
Address	<u>1856 17<sup>th</sup> Avenue</u>	Fax No.	<u></u>
	<u>San Francisco, CA 94122</u>	Email	<u></u>
Project Contact	<u>Reza Khoshnevisan</u>	Telephone No.	<u>415-922-0200</u>
Company	<u>SIA Consulting Corp.</u>	Fax No.	<u>415-922-0203</u>
Address	<u>1256 Howard Street</u>	Email	<u>reza@siaconsult.com</u>
	<u>San Francisco, CA 94103</u>		

**Site Information**

Site Address(es):	<u>54-56 Peralta Avenue</u>		
Nearest Cross Street(s)	<u>Holladay Avenue &amp; Hampshire Street</u>		
Block(s)/Lot(s)	<u>5512/029</u>	Zoning District(s)	<u>RH-2</u>
Site Square Footage	<u>2970sf</u>	Height/Bulk District	<u>40'</u>
Present or previous site use	<u>Vacant Lot</u>		
Community Plan Area (if any)	<u></u>		

**Project Description - please check all that apply**

<input type="checkbox"/> Addition	<input checked="" type="checkbox"/> Change of use	<input type="checkbox"/> Zoning change	<input checked="" type="checkbox"/> New construction
<input type="checkbox"/> Alteration	<input type="checkbox"/> Demolition	<input type="checkbox"/> Lot split/subdivision or lot line adjustment	
<input type="checkbox"/> Other (describe)	<u></u>	Estimated Cost	<u>\$650,000</u>

Describe proposed use New two unit residential building

Narrative project description. Please summarize and describe the purpose of the project.  
The vacant lot at Block/Lot: 5512/29 is located on the west side of Peralta Avenue between Holladay Avenue and Hampshire Street. The proposed project would construct a new three story, two-unit residential building on a 2,970sf vacant lot.

Unit #1(54 Peralta) will occupy the front portion of second and thir floors of the building and will consist of 2 bedrooms and 2 1/2 full basths with a total square footage of 1,057sf. Unit #2(56 Peralta) will occupy the rear portion of second and third floors and will include 3 bedrooms and 3 full baths with a total square footage of 1,249sf. There proposed garage will provide four off-street parking spaces with a Klaus parking pallet and will be located on the first floor of the building with a square footage of 1,438sf. Two of these parking spaces will be assigned to proposed project at Lot 31(60-62 Peralta).

The adjacent vacant lot at Lot 31(60-62 Peralta) will be developed to accommodate two new residential units.

**SAN FRANCISCO DEPARTMENT OF CITY PLANNING**  
**CATEGORICALLY EXEMPT FROM ENVIRONMENTAL REVIEW**

**CLASS 3 - New Construction**

CEQA Guidelines Section 15303 consists of construction of new multifamily residential structures totaling no more than six dwelling units in an unorganized area. Brett Bollinger 3/09/11

SAN FRANCISCO  
PLANNING DEPARTMENT  
MAY 11 2011

Approved Planning Dept. Brett Bollinger

PART 3 – ADDITIONAL PROJECT INFORMATION	Yes	No
<p>1. Would the project involve a major alteration of a structure constructed 50 or more years ago or a structure in an historic district?</p> <p>If yes, submit a <i>Supplemental Information Form for Historical Resource Evaluation</i>. Instructions on how to fill out the form are outlined in the <i>San Francisco Preservation Bulletin No. 16</i> (see pages 28-34 in Appendix B).</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>2. Would the project involve demolition of a structure constructed 50 or more years ago or a structure located in an historic district?</p> <p>If yes, a Historic Resource Evaluation Report (HRER)* will be required. The scope of the HRER will be determined in consultation with the Department's Preservation Coordinator.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>3a. Would the project result in excavation or soil disturbance/modification greater than 10 feet below grade?</p> <p>If yes, how many feet below grade would be excavated? _____</p> <p>What type of foundation would be used (if known)? _____</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>3b. Is the project site located in an area of potential geotechnical hazard as identified in the San Francisco General Plan or on a steep slope or would the project be located on a site with an average slope of 20% or more?</p> <p>If yes to either Question 3a or 3b, please submit a Geotechnical Report.*</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>4. Would the project involve expansion of an existing building envelope, or new construction, or grading, or new curb cuts, or demolition?</p> <p>If yes, please submit a <i>Tree Disclosure Statement</i>.</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>5. Would the project result in ground disturbance of 5,000 gross square feet or more?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>6. Would the project result in any construction over 40 feet in height?</p> <p>If yes, apply for a Section 295 (Proposition K) Shadow Study. This application is available on the Planning Department's website and <b>should be submitted at the Planning Information Center</b>, 1660 Mission Street, First Floor.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>7. Would the project result in a construction of a structure 80 feet or higher?</p> <p>If yes, an initial review by a wind expert, including a recommendation as to whether a wind analysis* is needed, may be required, as determined by Department staff.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>8. Would the project involve work on a site with an existing or former gas station, auto repair, dry cleaners, or heavy manufacturing use, or a site with underground storage tanks?</p> <p>If yes, please submit a Phase I Environmental Site Assessment (ESA).* A Phase II ESA (for example, soil testing) may be required, as determined by Department staff.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>9. Would the project require any variances, special authorizations, or changes to the Planning Code or Zoning Maps?</p> <p>If yes, please describe.</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>10. Is the project related to a larger project, series of projects, or program?</p> <p>If yes, please describe.</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>11. Is the project in Eastern Neighborhoods or Market &amp; Octavia Community Plan Area?</p> <p>If yes, and the project would be over 55 feet tall or 10 feet taller than an adjacent building built before 1963, please submit an elevation or renderings showing the project with the adjacent buildings.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

\* Report or study to be prepared by a qualified consultant who is contracted directly by the project sponsor.

**PART 4 – PROJECT SUMMARY TABLE**

If you are not sure of the eventual size of the project, provide the **maximum** estimates.

Gross Square Footage (GSF)	Existing Uses	Existing Uses to be Retained	Net New Construction and/or Addition	Project Totals
Residential	0	0	2,306	2,306
Retail	0	0	0	0
Office	0	0	0	0
Industrial	0	0	0	0
Parking	0	0	1,438	1,438
Other (specify use)	0	0	0	0
<b>Total GSF</b>	<b>0</b>	<b>0</b>	<b>3,744</b>	<b>3,744</b>
Dwelling units	0	0	2	2
Hotel rooms	0	0	0	0
Parking spaces	0	0	5	5
Loading spaces	0	0	0	0
Number of buildings	0	0	1	1
Height of building(s)	0	0	26'-11"	26'-11"
Number of stories	0	0	3	3

Please describe any additional project features that are not included in this table:

**Additional Information: Project drawings in 11x17 format** should include existing and proposed site plans, floor plans, elevations, and sections, as well as all applicable dimensions and calculations for existing and proposed floor area and height. The plans should clearly show existing and proposed off-street parking and loading spaces; driveways and trash loading areas; vehicular and pedestrian access to the site, including access to off-street parking and parking configuration; and bus stops and curbside loading zones within 150 feet of the site. A **transportation study** may be required, depending on existing traffic conditions in the project area and the potential traffic generation of the proposed project, as determined by the Department’s transportation planners. **Neighborhood notification** may also be required as part of the environmental review processes.



# SAN FRANCISCO PLANNING DEPARTMENT

## Environmental Evaluation Application

The California Environmental Quality Act (CEQA) requires public agencies to review the environmental impacts of proposed projects. In San Francisco, environmental review under CEQA is administered by the Major Environmental Analysis (MEA) division of the Planning Department. The environmental review process begins with the submittal of a completed Environmental Evaluation (EE) Application to the Planning Department. Only the current EE Application form will be accepted. No appointment is required but staff is available to meet with applicants upon request.

The EE Application will not be processed unless it is completely filled out and the appropriate fees are paid in full. Checks should be made payable to the San Francisco Planning Department. See the current *Schedule of Application Fees* and contact the staff person listed below for verification of the appropriate fees. Fees are generally non-refundable. **Documents in italics are available online at [sfgov.org/planning](http://sfgov.org/planning).**

The EE Application is comprised of four parts. Part 1 is a checklist to ensure that the EE Application is complete; Part 2 requests basic information about the site and the project; Part 3 is a series of questions to help determine if additional information is needed for the EE Application; and Part 4 is a project summary table.

The complete EE Application should be submitted to the Planning Department staff as follows: For projects greater than 10,000 square feet in size and where Part 3 Questions #3, #8, #10, or #11 are answered in the affirmative, or for projects that require mitigation measures, please send the application materials to the attention of Ms. Fordham or Ms. Pereira. For all other projects, please send the application materials to the attention of Mr. Bollinger.

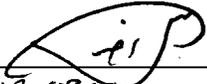
Brett Bollinger  
1650 Mission Street, Suite 400  
San Francisco, CA 94103  
(415) 575-9024, [brett.bollinger@sfgov.org](mailto:brett.bollinger@sfgov.org)

Chelsea Fordham, or Monica Pereira  
1650 Mission Street, Suite 400  
San Francisco, CA 94103  
(415) 575-9071, [chelsea.fordham@sfgov.org](mailto:chelsea.fordham@sfgov.org)  
(415) 575-9107, [monica.pereira@sfgov.org](mailto:monica.pereira@sfgov.org)

PART 1 – EE APPLICATION CHECKLIST	Provided	Not Applicable
Two copies of this application with all blanks filled in	<input checked="" type="checkbox"/>	
Two sets of project drawings (see "Additional Information" at the end of page 4,)	<input checked="" type="checkbox"/>	
Photos of the project site and its immediate vicinity, with viewpoints labeled	<input checked="" type="checkbox"/>	
Fee	<input checked="" type="checkbox"/>	
<i>Supplemental Information Form for Historical Resource Evaluation</i> and/or Historic Resource Evaluation Report, as indicated in Part 3 Questions 1 and 2	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Geotechnical Report, as indicated in Part 3 Questions 3a and 3b	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<i>Tree Disclosure Statement</i> , as indicated in Part 3 Question 4	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Phase I Environmental Site Assessment, as indicated in Part 3 Question 8	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Additional studies (list)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Applicant's Affidavit.** I certify the accuracy of the following declarations:

- The undersigned is the owner or authorized agent of the owner(s) of this property.
- The information presented is true and correct to the best of my knowledge.
- I understand that other applications and information may be required.

Signed (owner or agent):   
(For Staff Use Only) Case No. 2010.0347E

Date: 3/5/2011  
Address: 60-62 Perata St.  
Block/Lot: 3512/031

**PART 2 – PROJECT INFORMATION**

**Owner/Agent Information**

Property Owner	<u>Tom Aquilina</u>	Telephone No.	<u>415-706-4780</u>
Address	<u>1856 17<sup>th</sup> Avenue</u>	Fax No.	<u></u>
	<u>San Francisco, CA 94122</u>	Email	<u></u>
Project Contact	<u>Reza Khoshnevisan</u>	Telephone No.	<u>415-922-0200</u>
Company	<u>SIA Consulting Corp.</u>	Fax No.	<u>415-922-0203</u>
Address	<u>1256 Howard Street</u>	Email	<u>reza@siaconsult.com</u>
	<u>San Francisco, CA 94103</u>		

**Site Information**

Site Address(es):	<u>60-62 Peralta Avenue</u>		
Nearest Cross Street(s)	<u>Holladay Avenue &amp; Hampshire Street</u>		
Block(s)/Lot(s)	<u>5512/031</u>	Zoning District(s)	<u>RH-2</u>
Site Square Footage	<u>2220sf</u>	Height/Bulk District	<u>40'</u>
Present or previous site use	<u>Vacant Lot</u>		
Community Plan Area (if any)	<u></u>		

**Project Description - please check all that apply**

<input type="checkbox"/> Addition	<input checked="" type="checkbox"/> Change of use	<input type="checkbox"/> Zoning change	<input checked="" type="checkbox"/> New construction
<input type="checkbox"/> Alteration	<input type="checkbox"/> Demolition	<input type="checkbox"/> Lot split/subdivision or lot line adjustment	
<input type="checkbox"/> Other (describe)	<u></u>	Estimated Cost	<u>\$650,000</u>

Describe proposed use New two unit residential building

Narrative project description. Please summarize and describe the purpose of the project.  
The vacant lot at Block/Lot: 5512/31 is located on the west side of Peralta Avenue between Holladay Avenue and Hampshire Street. The proposed project would construct a new four story, two-unit residential building on a 2,200sf vacant lot.

Unit #1(60 Peralta) will occupy the first and second floors of the building and will consist of 3 bedrooms and 3 full baths with a total square footage of 1,692sf. Unit #2(62 Peralta) will occupy the third and fourth floors and will include 3 bedrooms and 2 full baths with a total square footage of 1,509sf. There are no proposed off-street parking spaces included in the proposed scope of work.

The adjacent vacant lot at Lot 29(54-56 Peralta Avenue) will be developed to accommodate two new residential units with four parking spaces, two of these parking spaces will be assigned to the proposed project at Lot 31(60-62 Peralta).

**SAN FRANCISCO DEPARTMENT OF CITY PLANNING  
CATEGORICALLY EXEMPT FROM ENVIRONMENTAL REVIEW**

**CLASS 3 - New Construction**  
CEQA Guideline Section 15303 consists of construction of new multi-family residential structure totaling no more than six dwelling units in an urbanized area.

Brett Bollinger 03/09/11  
Approved Planning Dept. Brett Bollinger

PART 3 – ADDITIONAL PROJECT INFORMATION	Yes	No
1. Would the project involve a major alteration of a structure constructed 50 or more years ago or a structure in an historic district?  If yes, submit a <i>Supplemental Information Form for Historical Resource Evaluation</i> . Instructions on how to fill out the form are outlined in the <i>San Francisco Preservation Bulletin No. 16</i> (see pages 28-34 in Appendix B).	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Would the project involve demolition of a structure constructed 50 or more years ago or a structure located in an historic district?  If yes, a Historic Resource Evaluation Report (HREER)* will be required. The scope of the HREER will be determined in consultation with the Department's Preservation Coordinator.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3a. Would the project result in excavation or soil disturbance/modification greater than 10 feet below grade?  If yes, how many feet below grade would be excavated? _____  What type of foundation would be used (if known)? _____	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3b. Is the project site located in an area of potential geotechnical hazard as identified in the San Francisco General Plan or on a steep slope or would the project be located on a site with an average slope of 20% or more?  If yes to either Question 3a or 3b, please submit a Geotechnical Report.*	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Would the project involve expansion of an existing building envelope, or new construction, or grading, or new curb cuts, or demolition?  If yes, please submit a <i>Tree Disclosure Statement</i> .	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Would the project result in ground disturbance of 5,000 gross square feet or more?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Would the project result in any construction over 40 feet in height?  If yes, apply for a Section 295 (Proposition K) Shadow Study. This application is available on the Planning Department's website and <b>should be submitted at the Planning Information Center</b> , 1660 Mission Street, First Floor.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. Would the project result in a construction of a structure 80 feet or higher?  If yes, an initial review by a wind expert, including a recommendation as to whether a wind analysis* is needed, may be required, as determined by Department staff.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. Would the project involve work on a site with an existing or former gas station, auto repair, dry cleaners, or heavy manufacturing use, or a site with underground storage tanks?  If yes, please submit a Phase I Environmental Site Assessment (ESA).* A Phase II ESA (for example, soil testing) may be required, as determined by Department staff.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. Would the project require any variances, special authorizations, or changes to the Planning Code or Zoning Maps?  If yes, please describe. Parking Variance	<input checked="" type="checkbox"/>	<input type="checkbox"/>
10. Is the project related to a larger project, series of projects, or program?  If yes, please describe. The new development of two adjacent vacant lots	<input checked="" type="checkbox"/>	<input type="checkbox"/>
11. Is the project in Eastern Neighborhoods or Market & Octavia Community Plan Area?  If yes, and the project would be over 55 feet tall or 10 feet taller than an adjacent building built before 1963, please submit an elevation or renderings showing the project with the adjacent buildings.	<input type="checkbox"/>	<input checked="" type="checkbox"/>

\* Report or study to be prepared by a qualified consultant who is contracted directly by the project sponsor.

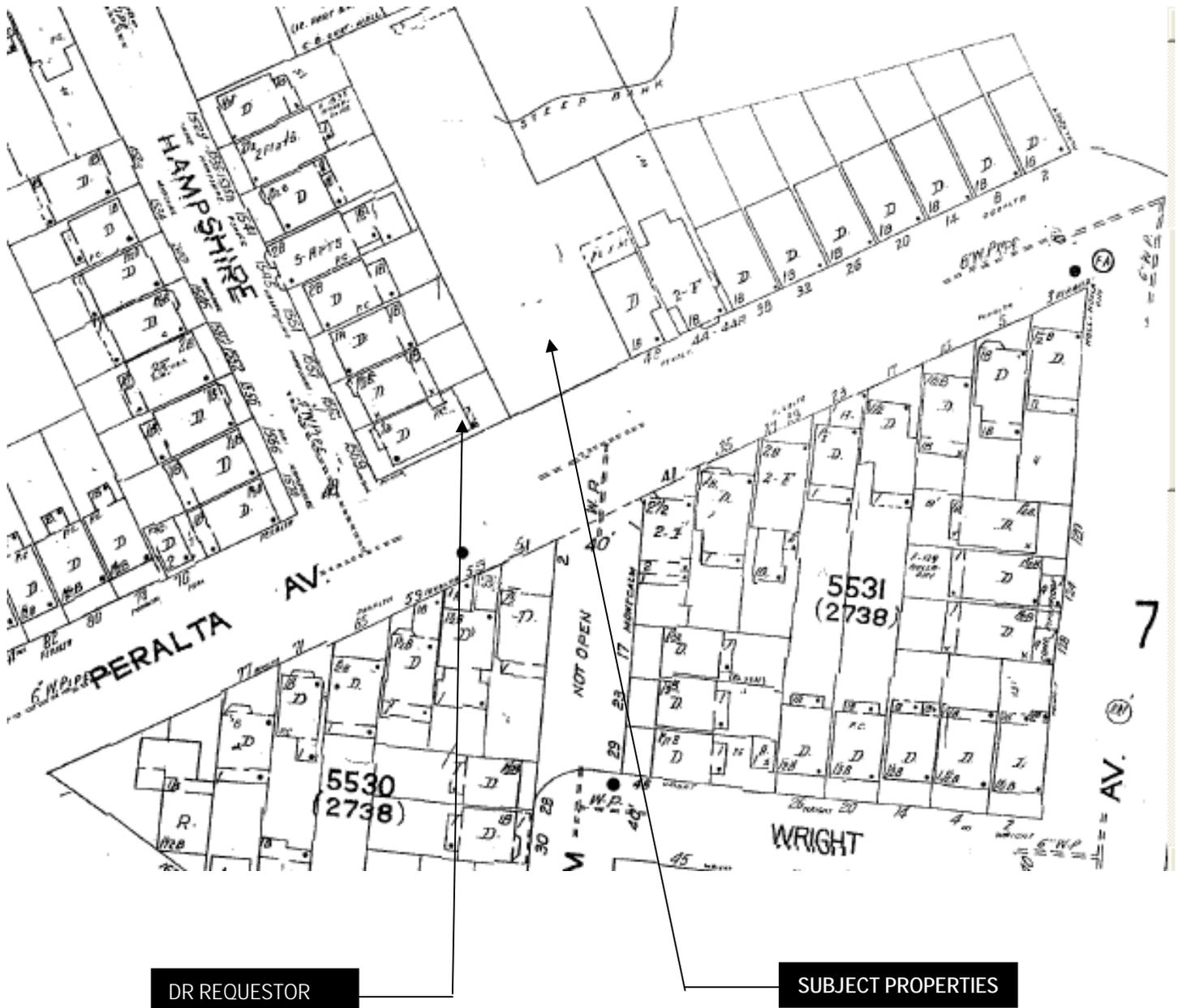
**PART 4 – PROJECT SUMMARY TABLE**

If you are not sure of the eventual size of the project, provide the **maximum** estimates.

Gross Square Footage (GSF)	Existing Uses	Existing Uses to be Retained	Net New Construction and/or Addition	Project Totals
Residential	0	0	3,201	3,201
Retail	0	0	0	0
Office	0	0	0	0
Industrial	0	0	0	0
Parking	0	0	0	0
Other (specify use)	0	0	0	0
<b>Total GSF</b>	<b>0</b>	<b>0</b>	<b>3,201</b>	<b>3,201</b>
Dwelling units	0	0	2	2
Hotel rooms	0	0	0	0
Parking spaces	0	0	0	0
Loading spaces	0	0	0	0
Number of buildings	0	0	1	1
Height of building(s)	0	0	28'9"	28'9"
Number of stories	0	0	4	4
Please describe any additional project features that are not included in this table:				
<p><b>Additional Information:</b> Project drawings in 11x17 format should include existing and proposed site plans, floor plans, elevations, and sections, as well as all applicable dimensions and calculations for existing and proposed floor area and height. The plans should clearly show existing and proposed off-street parking and loading spaces; driveways and trash loading areas; vehicular and pedestrian access to the site, including access to off-street parking and parking configuration; and bus stops and curbside loading zones within 150 feet of the site. A <b>transportation study</b> may be required, depending on existing traffic conditions in the project area and the potential traffic generation of the proposed project, as determined by the Department's transportation planners. <b>Neighborhood notification</b> may also be required as part of the environmental review processes.</p>				



# Sanborn Map\*



\*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Discretionary Review Hearing  
Case Number 2010.0367DDV  
54-62 Peralta Avenue

# Aerial Photo



DR REQUESTOR

SUBJECT PROPERTIES



SUBJECT PROPERTIES

DR REQUESTOR



Discretionary Review Hearing  
Case Number 2010.0367DDV  
54-62 Peralta Avenue

# Aerial Photo



SUBJECT PROPERTIES

DR REQUESTOR





# SAN FRANCISCO PLANNING DEPARTMENT

1650 Mission Street Suite 400 San Francisco, CA 94103

## NOTICE OF BUILDING PERMIT APPLICATION (SECTION 311)

On December 31, 2009, the Applicant named below filed Building Permit Application No. 2009.12.31.4050 and 2009.12.31.4052 (New Construction of Two Buildings) with the City and County of San Francisco.

CONTACT INFORMATION		PROJECT SITE INFORMATION	
Applicant:	Reza Khoshnevisan	Project Addresses:	54-56 and 60-62 Peralta Ave
Address:	1256 Howard Street	Cross Streets:	Holladay Ave and Hampshire St
City, State:	San Francisco, CA 94103	Assessor's Block /Lot No.:	5512/029 and 031
Telephone:	(415) 922-0200	Zoning Districts:	RH-2 / 40-X

Under San Francisco Planning Code Section 311, you, as a property owner or resident within 150 feet of this proposed project, are being notified of this Building Permit Application. You are not obligated to take any action. For more information regarding the proposed work, or to express concerns about the project, please contact the Applicant above or the Planner named below as soon as possible. If your concerns are unresolved, you can request the Planning Commission to use its discretionary powers to review this application at a public hearing. Applications requesting a Discretionary Review hearing must be filed during the 30-day review period, prior to the close of business on the Expiration Date shown below, or the next business day if that date is on a week-end or a legal holiday. If no Requests for Discretionary Review are filed, this project will be approved by the Planning Department after the Expiration Date.

PROJECT SCOPE			
<input type="checkbox"/> DEMOLITION	and/or	<input checked="" type="checkbox"/> NEW CONSTRUCTION	or <input type="checkbox"/> ALTERATION
<input type="checkbox"/> VERTICAL EXTENSION		<input type="checkbox"/> CHANGE # OF DWELLING UNITS	<input type="checkbox"/> FACADE ALTERATION(S)
<input type="checkbox"/> HORIZ. EXTENSION (FRONT)		<input type="checkbox"/> HORIZ. EXTENSION (SIDE)	<input type="checkbox"/> HORIZ. EXTENSION (REAR)

PROJECT FEATURES (FOR EACH BLDG)	PROPOSED CONDITIONS	
	54-56 Peralta Ave	60-62 Peralta Ave
FRONT SETBACK.....	+/- 2 feet	+/- 2 feet
FRONT SETBACK (at Top Floor).....	+/- 7 feet	+/- 7 feet
SIDE SETBACK.....	+/- 4 feet (southern side)	+/- 4 feet (southern side)
BUILDING DEPTH (Lowest Floor).....	+/- 54 feet 6 inches	+/- 41 feet
REAR YARD.....	+/- 44 feet 7 inches	+/- 33 feet 4 inches
HEIGHT OF BUILDING (from front curb).....	+/- 29 feet	+/- 29 feet
NUMBER OF STORIES (front).....	3	3
NUMBER OF STORIES (rear).....	3 plus Crawlspace	4
NUMBER OF PARKING SPACES.....	4 (2 provided for 60-62 Peralta Ave)	0 (2 provided at 60-62 Peralta Ave)
NUMBER OF DWELLING UNITS.....	2	2

### PROJECT DESCRIPTION

The proposal is to construct two new two-family homes on down-sloping adjacent lots (currently vacant), resulting in a total of four new dwelling units. The 52-54 Peralta Ave building will contain four off-street parking spaces, of which two will be reserved for the units in the proposed adjacent building at 60-62 Peralta Ave, resulting in one off-street parking space for each of the four new units. Providing only four parking spaces for the two buildings requires a variance from Planning Code Section 242(e). The parking variance for this project is *tentatively* scheduled to be heard by the Zoning Administrator on **September 22, 2010**. Separate notice will be issued for the variance hearing.

PLANNER'S NAME: Corey Teague  
 PHONE NUMBER: (415) 575-9081  
 EMAIL: corey.teague@sfgov.org

DATE OF THIS NOTICE: 10-13-10  
 EXPIRATION DATE: 10-12-10

# NOTICE OF BUILDING PERMIT APPLICATION GENERAL INFORMATION ABOUT PROCEDURES

Reduced copies of the site plan and elevations (exterior walls), and floor plans (where applicable) of the proposed project, including the position of any adjacent buildings, exterior dimensions, and finishes, and a graphic reference scale, have been included in this mailing for your information. Please discuss any questions with the project Applicant listed on the reverse. You may wish to discuss the plans with your neighbors and neighborhood association or improvement club, as they may already be aware of the project. Immediate neighbors to the project, in particular, are likely to be familiar with it.

Any general questions concerning this application review process may be answered by the Planning Information Center at 1660 Mission Street, 1st Floor (415/ 558-6377) between 8:00 a.m. - 5:00 p.m. Please phone the Planner listed on the reverse of this sheet with questions specific to this project.

If you determine that the impact on you from this proposed development is significant and you wish to seek to change the proposed project, there are several procedures you may use. We strongly urge that steps 1 and 2 be taken.

1. Seek a meeting with the project sponsor and the architect to get more information, and to explain the project's impact on you and to seek changes in the plans.
2. Call the nonprofit organization Community Boards at (415) 920-3820. They are specialists in conflict resolution through mediation and can often help resolve substantial disagreement in the permitting process so that no further action is necessary.
3. Where you have attempted, through the use of the above steps, or other means, to address potential problems without success, call the assigned project planner whose name and phone number are shown at the lower left corner on the reverse side of this notice, to review your concerns.

If, after exhausting the procedures outlined above, you still believe that exceptional and extraordinary circumstances exist, you have the option to request that the Planning Commission exercise its discretionary powers to review the project. These powers are reserved for use in exceptional and extraordinary circumstances for projects, which generally conflict with the City's General Plan and the Priority Policies of the Planning Code; therefore the Commission exercises its discretion with utmost restraint. This procedure is called Discretionary Review. If you believe the project warrants Discretionary Review by the Planning Commission over the permit application, you must make such request **within 30 days of this notice, prior to the Expiration Date shown on the reverse side**, by completing an application (available at the Planning Department, 1660 Mission Street, 1st Floor, or on-line at [www.sfgov.org/planning](http://www.sfgov.org/planning)). You must submit the application to the Planning Information Center during the hours between 8:00 a.m. and 5:00 p.m., with all required materials, and a check for \$300.00, for each Discretionary Review request payable to the Planning Department. If the project includes multi building permits, i.e. demolition and new construction, a separate request for Discretionary Review must be submitted, with all required materials and fee, for each permit that you feel will have an impact on you. Incomplete applications will not be accepted.

If no Discretionary Review Applications have been filed within the Notification Period, the Planning Department will approve the application and forward it to the Department of Building Inspection for its review.

## BOARD OF APPEALS

An appeal of the approval (or denial) of the permit application by the Planning Department or Planning Commission may be made to the Board of Appeals within 15 days after the permit is issued (or denied) by the Superintendent of the Department of Building Inspection. Submit an application form in person at the Board's office at 1650 Mission Street, 3rd Floor, Room 304. For further information about appeals to the Board of Appeals, including their current fees, contact the Board of Appeals at (415) 575-6880.

CASE NUMBER:

# APPLICATION FOR Discretionary Review Application

## 1. Owner/Applicant Information

DR APPLICANT'S NAME: <b>Mr. Bob Besso</b>		
DR APPLICANT'S ADDRESS: <b>1569 Hampshire Street, San Francisco, California</b>	ZIP CODE: <b>94110</b>	TELEPHONE: <b>415 550-1985</b>

PROPERTY OWNER WHO IS DOING THE PROJECT ON WHICH YOU ARE REQUESTING DISCRETIONARY REVIEW NAME: <b>Tom Aquilina</b>		
ADDRESS: <b>1856 17th Avenue, San Francisco, CA</b>	ZIP CODE: <b>94122</b>	TELEPHONE: <b>(415) Unlisted</b> <b>415-640-0589</b>

CONTACT FOR DR APPLICATION: Same as Above <input checked="" type="checkbox"/>		
ADDRESS:	ZIP CODE:	TELEPHONE: ( )
E-MAIL ADDRESS:		

## 2. Location and Classification

STREET ADDRESS OF PROJECT: <b>54 - 56 Peralta Avenue, San Francisco, California</b>		ZIP CODE: <b>94110</b>
CROSS STREETS: <b>Holladay Avenue and Hampshire Street</b>		
ASSESSORS BLOCK/LOT: <b>5512 / 029</b>	LOT DIMENSIONS: <b>30' X 99'</b>	LOT AREA (SQ FT): <b>2950</b>
ZONING DISTRICT: <b>RH2</b>		HEIGHT/BULK DISTRICT: <b>40X</b>

## 3. Project Description

Please check all that apply  
 Change of Use  Change of Hours  New Construction  Alterations  Demolition  Other

Additions to Building: Rear  Front  Height  Side Yard

Present or Previous Use: **Vacant - Existing neighborhood art installation.**

Proposed Use: **2 unit residential**

Building Permit Application No. **2009.12.31.4052**

Date Filed: **12-31-09**

**RECEIVED**

**NOV 17 2010**

**CITY & COUNTY OF S.F.**  
DEPT. OF CITY PLANNING  
PIC

**10.0367D**

4. Actions Prior to a Discretionary Review Request

Prior Action	YES	NO
Have you discussed this project with the permit applicant?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Did you discuss the project with the Planning Department permit review planner?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Did you participate in outside mediation on this case?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

5. Changes Made to the Project as a Result of Mediation

If you have discussed the project with the applicant, planning staff or gone through mediation, please summarize the result, including any changes there were made to the proposed project.

Please note: We contacted Community Boards, however they replied we are not a candidate for mediation since the owner and developer dropped out of communication with the Bernal Heights East Slope Design Review Board upon receipt of their letter dated June 5, 2010. Kindly see Review Board letter in our Application documentation. Thank you.

#1 What are the reasons for requesting Discretionary Review? The project meets the minimum standards of the Planning Code. What are the exceptional and extraordinary circumstances that justify Discretionary Review of the project? How does the project conflict with the City's General Plan or the Planning Code's Priority Policies or Residential Design Guidelines? Please be specific and site specific sections of the Residential Design Guidelines.

There are several exceptional and extraordinary circumstances that justify Discretionary Review of this project. These circumstances are as follows:

- Size and scope of project out of character with the neighborhood
- Insufficient design information for significant sidewalk grade change
- Safety and flooding concerns regarding removal of retaining wall for driveway/garage
- Inadequate and inoperable design for off street parking
- Suggested landscaping or set back to soften facade omitted from design
- Lack of cooperation or communications from owner and developer

#### **Size and Scope of Project out of Character with the Neighborhood**

The Bernal Heights Special Use District under Sec. 242 Letter (e) Number 6 under **Design** states: **"Requests for Planning Commission review shall be governed by Subsection 311(d) of this Code. In addition to applicable guidelines cited by Section 311, the Elsie Street Plan and the East Slope Building Guidelines shall be used as guidelines to determine neighborhood compatibility of new construction and alterations in the respective areas covered by those guidelines."**

As concerned and neighboring residents we have also found that the plan as proposed does not follow the BERNAL HEIGHTS SPECIAL USE DISTRICT Sec. 242 (b) under **Purposes** it states:

**"In order to reflect the special characteristics and hillside topography of an area of the City that has a collection of older buildings situated on lots generally smaller than the lot patterns in other low-density areas of the City, and to encourage development in context and scale with the established character, there shall be a Bernal Heights Special Use District."** The units as proposed are in reality a four unit apartment complex on two lots with less than required parking located in only one of the buildings.

Another set of specific areas of contention is contained within Section 4 of the East Slope Design Review Guidelines that, under the title **Building Bulk and Architectural Massing** states:

#### **INTENT**

**"Our objectives in the Building Bulk Guidelines are neatly summed up in already existing City Planning Policies. To quote from the Urban Design Element of the Master Plan, our intent is to:**

1. Minimize the blockage of sun from adjacent downhill properties;
2. Lower the first level of occupancy to a level enabling ready access to rear yard open space;
3. Deter the possibilities of visually dominant buildings with blank and uninteresting exteriors which do not relate well to surrounding development;
4. Promote harmony in the visual relationships and transitions between new and older buildings; and,
5. Encourage the Construction of buildings which meet the ground and reflect the slope of the hill."

The plan, as submitted by the Developer, fails to meet guideline intent items 1, 3, 4, & 5.

As members of the Far Northeast Bernal Heights Neighborhood Block Club, which represents affected neighbors, we feel this proposed development will irreparably upset the fabric & character of our neighborhood (Please see pages 6A-11A).

#### Insufficient Design Information for Significant Sidewalk Grade Change

The plans show a sidewalk grade increase of nearly 4% from 16.6% to 20% (Please see page 21A). This slope increase is to create a level entry into the proposed underground parking area. This increase in sidewalk slope creates a hardship for many of the older residents in our neighborhood and without adequate information or dialog with the Developer and Owner we are concerned about the feasibility and safety of this design.

#### Safety and Flooding Concerns Regarding Removal of Retaining Wall for Driveway/Garage

Of special concern is the proposed removal of the curb and retaining wall in front of this development, which currently channels rainwater down Peralta Avenue and prevents the flooding of residences on Hampshire Street. Peralta Avenue becomes Peralta Creek during a rainstorm. The placement and size of the street curbs and retaining walls is to accommodate ground stability which requires controlling water runoff (Please see historical photos page 24A). With the retaining wall breached at this point, and a ten foot section of it removed, rainwater will flow down the sidewalk, causing erosion and safety concerns for the residences downhill.

#### Inadequate and Inoperable Design for Off Street Parking

The Developer is seeking a Parking Variance. The parking plan as proposed doesn't meet the requirements of the Bernal Heights Special Use District under Sec. 242 Letter (e) Number 4 nor does it meet the minimum standards of the Planning Code. The square footage as submitted to the city requires 7 off street parking spaces. The sub-grade parking garage design shows four parking spaces facilitated by the use of an electric parking pallet. The plans shows the pallet accommodating four vehicles, shifting them to and from the steep driveway ramp. Upon consultation with the manufacturer, we found that the parking pallet shown in the submitted plans "Klaus PQ -AC/PQ-DC", will not accommodate vehicles as the plans suggests, due to the wheel safety stop, and non-beveled edge, at one end of the pallet (Please see pages 12A & 13A). As a result, this specific parking pallet, can only accommodate vehicles from one direction, rendering 2 of the 4 spaces unserviceable by the pallet. Even if the parking pallet was operable for 4 vehicles, the parking allocation would not be acceptable to the neighbors nor was it accepted by the East Slope Design Review Board in the letter dated June 5, 2010 (Please see pages 4A & 5A) that was sent to the Developer which stated: **The Board feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project's reliance on neighborhood parking. The Board recognizes that off-street parking cannot be provided at 60-62 Peralta and the difficulty in providing additional off-street parking within 54-56 Peralta, however, we feel that the parking deficit is more a reflection of the size of the buildings and not site access.**

The affected neighbors are completely against granting a parking variance for this project and are in agreement with the Design Review Board and its guidelines.

#### Suggested Landscaping or set back to Soften Façade Omitted from Design

Both buildings in this proposed project are set back the minimum amount allowed. This leaves little room for the landscaping needed to soften the edges of the building, add texture, and create intimacy. We have noted that there are trees placed on the plans for the front of these structures.

In actuality, there is insufficient room for such plantings to be installed and retain the required space for pedestrian foot traffic, rendering these plantings unacceptable (Please see page 17A).

### **Lack of Cooperation or Communications from Developer and Owner**

The Project as submitted is not consistent with the guidelines developed by the Bernal Heights East Slope Design Review Board. This volunteer body functioning for more than 20 years is an important representative group that works to insure design & new home construction in Bernal Heights remains consistent with the character of the area of Bernal Heights under its purview as recognized by the Planning Code.

In a letter dated June 5, 2010 from the East Slope Design Review Board to the Developer of this project: SIA Consulting Corporation (Please see page 4A) it stated that: **“The Board regrets to inform you that we cannot recommend that the Department of City Planning approve this project as proposed. The Board also cannot recommend approval of the parking variances that are required to construct the project as currently planned. The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines.”**

The East Slope Design Review Board presented this letter dated June 5, 2010 as part of their role to facilitate a continuing communication between the Developer and the Owner with the affected neighbors. The letter as noted cannot recommend the project to the Planning Department. When the Developer and Owner received this letter they chose to end the communication process with the affected neighbors and with the Design Review Board itself and go straight ahead to submitting for a Building Permit.

#2. The Residential Design Guidelines assume some impacts to be reasonable and expected as part of construction. Please explain how this project would cause unreasonable impacts. If you believe your property, the property of others or the neighborhood would be adversely affected, please state who would be affected, and how:

This project, as currently proposed, would cause unreasonable and adverse impacts to our neighborhood in several ways. These impacts are as follows:

- Dangerous vehicular entries and exits across the sidewalk and street.
- Towering buildings blocking light and eliminating privacy
- Removal of retaining wall and curb causing a water runoff hazard
- Unrealistic parking plan that will increase demand on street parking

### **Dangerous Vehicular Entries and Exits across Existing Sidewalk**

One of the homes most directly affected by this portion of the proposed project is 48 Peralta, owned since 1969 by Lucy Gomez, who is 85 years old. The greatly increased vehicular traffic across the sidewalk (not more than 4 feet from her property line) presents an extraordinary public safety hazard, especially to people, like Lucy, who are advanced in years, and to children whom are not as aware of hazards. The vehicles making use of the subterranean parking spaces, as designed in the plan, will be accessing the driveway from an angle partially obscured by the sidewalk retaining wall. Vehicle drivers exiting the garage, backing up a 20 degree plus ramp (Please see page 14A), will be crossing the sidewalk and entering the street essentially blind to existing pedestrian and traffic conditions. Many neighborhood residents use this sidewalk to get to public transportation, walk their dogs, and transport their children in strollers. The crossing of this sidewalk by drivers backing up a steep ramp with obscured vision is a recipe for disaster. Additionally, the blind street access is further complicated by this block's high retaining wall that separates East and West bound traffic. This block of Peralta is used by

many Upper Bernal Heights neighbors coming down the hill to Cesar Chavez, and they notoriously take advantage of the downhill slope and one way traffic to "step on it" as they head west on Peralta.

### **Towering Buildings Blocking Light and Eliminating Privacy**

Lucy Gomez, at 48 Peralta will also lose all sunlight on the western side of her residence due to the exterior facades of the east side of the proposed project. The light from all seven windows of 48 Peralta will be blocked by the windowless, unarticulated and towering exterior wall of 54-56 Peralta (Please see pages 17A & 22A).

The second group of properties that are directly affected by the negative aspects of this proposed project are the homes on the East side of Hampshire Street, adjacent to the western side of the lots. The plans as proposed, do not include a adequate stepping down of the rooftops, which is inconsistent with the surrounding homes and hillside (Please see pages 25A & 27A). The overwhelming western facade of this towering project will obliterate nearly all sunlight and privacy for the abutting homes located on Hampshire Street. On it's western face, this inadequately stepped down project towers some 40 to 50 feet over the back yards of 1569, 1563, and 1557 Hampshire Street. These homes on Hampshire Street will have their daylight diminished to that of a narrow canyon, with neighbors peering down into their homes (Please see pages 18A, 19A, 20A).

### **Removal of Retaining Wall Causing Water Runoff into Adjacent Properties**

The retaining walls that line this portion of Peralta are there for structural and other reasons, and they also keep rain water flowing towards street drains and away from family residences. This proposed retaining wall cut (Please see page 26A), and removal of a ten foot section of curb and wall on the declining side of Peralta, would dramatically change the path and velocity of surface water runoff. Channeling surface water to the sidewalk at the proposed curb cut would create a torrent of water on the steep downhill, towards Hampshire Street homes during winter rain months. This cut would cause public access and safety concerns as the surface water would not find a street drain until the bottom of the hill at Cesar Chavez Street.

### **Unrealistic Parking Plan that will Increase "On Street" Parking**

The presented plan for parking vehicles (they are asking for variance from seven) in the 54-56 Peralta location is unrealistic. Entry into the property will involve several back and forth driving maneuvers due to the center street retaining wall (Please see page 23A), to line up with the garage opening. This retaining wall already causes reliance on street parking for those residents with restricted or no off street parking options. To gain access to either of the proposed projects' two lots, the developer will have to remove a minimum of one existing street parking space to accommodate any driveway. Furthermore, because of the unusual and complicated parking scheme as submitted by the developer, we believe that few of the proposed off-street parking places will be used for parking, but rather for storage or other purposes, further increasing the burden on the neighborhood. More significant, if this parking plan is approved and is attempted to be used as designed, we are seriously concerned for the neighborhood's health and safety. The planned garage is below street grade, inconsistent with the neighborhood buildings, and necessitates a steep driveway for access to and from the building. The plans show that all vehicles will back up this steep ramp, blindly crossing the sidewalk and into the street before the driver has any reasonable vision of existing pedestrian or vehicle circumstances. We see this as a significant safety concern for pedestrians, especially children and the elderly.

#3. What alternatives or changes to the proposed project, beyond the changes (if any) already made would respond to the exceptional and extraordinary circumstances and reduce the adverse effects noted above in question #1?

**The alternatives that we suggest to the project are as follows:**

**Neighborhood Context:**

**Reduce the height, mass & density of the proposed development**, to respect the neighborhood context (Please see page 25A), and preserve its character, as well as conform with the Bernal Heights East Slope Design Review Board Guidelines as outlined in its letter to the developer, dated June 5, 2010. The residents on Peralta, Hampshire and Holladay are against upsetting the character of our neighborhood with the construction of what is essentially a giant four-unit apartment building in our midst. Given solutions to other problems that we identified with this project, we find that the construction of a total of one home on each lot is in keeping with the letter and intent of the Special Use District as well as The Board's Guidelines.

If any structure is to be built on the property at 60-62 Peralta, which is directly adjacent to lots on Hampshire Street, it should be stepped down in conformity with the hill and set back from the sidewalk so as not to completely obliterate all light and privacy on the adjoining properties. Our homes are single-family residences, some having a small in-law unit at ground level behind their front stairs.

**Parking:**

**Provide off-street parking for all vehicles in compliance with Guidelines:**

In its letter to the developer dated June 5, 2010:

**“The Boards feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project’s reliance on neighborhood parking. The Board recognizes that off-street parking cannot be provided at 60-62 Peralta and the difficulty in providing additional off-street parking within 54-56 Peralta, however, we feel that the parking deficit is more a reflection of the size of the buildings and not site access.”**

Grade level parking (no steep driveway) for all vehicles in compliance with Bernal Heights Special Use District under Sec. 242 Letter (e) Number 4, in a building with a significant set back from the sidewalk, would greatly reduce our public safety concerns.

**Cooperation/Communication with Developer and Owner**

The Far North East Bernal Heights Block Club would welcome a continued dialog with the Developer and the Owner to create a realistic design for two single-family dwellings on these two adjacent lots.

**REQUIRE A PUBLIC WORKS PUBLIC HEARING OF PROPOSED CHANGES TO THE PUBLIC RIGHT-OF-WAY ADJACENT TO THE PROPERTY**

A Public Works Hearing is mentioned in the Board's letter to the developer, dated June 5, 2010.

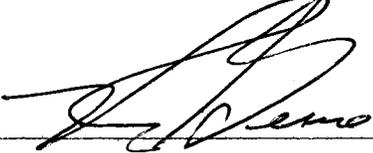
**Public right-of-way issues yet to be addressed:**

- **Steep Sidewalk:** The Bernal Heights Design Review Board suggests the developer provide stairs instead of a steeper slope to the sidewalk in front of this development. We still have many questions as we feel this would not accommodate either wheelchairs or strollers and would still make it more difficult for our many senior residents to transverse our neighborhood.
- **Retaining Wall and Curb Removal & Flooding:** As stated in question #1 we are especially concerned about the removal of the curb and retaining wall in front of this development's proposed driveway access. This curb and wall currently channels rainwater down Peralta Avenue. Peralta Avenue is banked to the North curbside as it starts a steep decent right in front of the planned development (see Historical photos attached). With the retaining wall and curb breached, rainwater coming down that entire block of Peralta would be channeled by the slope to the sidewalk in front of the development and onto the sidewalk toward Hampshire Street, causing health and safety concerns to the residences below.
- **Sidewalk Right of Way, Building Set-Back and Landscaping:** The proposed buildings are set back a minimal amount essentially leaving room for only the sidewalk. However, the plans as submitted, indicate trees in the sidewalk which would impede the public right of way. As stated in The Board's letter, "**Should this type of greenery not be provided then we request that the set-backs be increased to allow for front yards on both lots.**" Any structures on these lots should be set-back further from the sidewalk to assure public right of way and safety, as well as to accommodate landscaping.

# Applicant's Affidavit

Under penalty of perjury the following declarations are made:

- a: The undersigned is the owner or authorized agent of the owner of this property.
- b: The information presented is true and correct to the best of my knowledge.
- c: The other information or applications may be required.

Signature: 

Date: 11-11-10

Robert Besso owner  
Print name, and indicate whether owner, or authorized agent:

\_\_\_\_\_  
Owner / Authorized Agent (circle one)

10.0367D

Application for Discretionary Review	
CASE NUMBER: <small>For Staff Use only</small>	

# Discretionary Review Application Submittal Checklist

Applications submitted to the Planning Department must be accompanied by this checklist and all required materials. The checklist is to be completed and signed by the applicant or authorized agent.

REQUIRED MATERIALS (please check correct column)	DR APPLICATION
Application, with all blanks completed	<input type="checkbox"/>
Address labels (original), if applicable	<input type="radio"/>
Address labels (copy of the above), if applicable	<input type="radio"/>
Photocopy of this completed application	<input type="checkbox"/>
Photographs that illustrate your concerns	<input type="checkbox"/>
Covenant or Deed Restrictions	<input type="checkbox"/>
Check payable to Planning Dept.	<input type="checkbox"/>
Letter of authorization for agent	<input type="checkbox"/>
Other: Section Plan, Detail drawings (i.e. windows, door entries, trim), Specifications (for cleaning, repair, etc.) and/or Product cut sheets for new elements (i.e. windows, doors)	<input type="checkbox"/>

- NOTES:
- Required Material.
  - Optional Material.
  - Two sets of original labels and one copy of addresses of adjacent property owners and owners of property across street.

For Department Use Only  
Application received by Planning Department:

By: \_\_\_\_\_ Date: \_\_\_\_\_

10.0367D

1A



Lewis Randal  
2 Peralta  
San Francisco, CA 94110  
#5512 / #3A

Paul Phillips  
3 Peralta  
San Francisco, CA 94110  
#5531 / #1

Chizuru Fujita  
5 Peralta  
San Francisco, CA 94110  
#5531 / #19

Carl Passero  
8 Peralta  
San Francisco, CA 94110  
#5512 / #3B

Thomas Dorte  
14 Peralta  
San Francisco, CA 94110  
#5512 / #3C

Natasha Tuck  
15 Peralta  
San Francisco, CA 94110  
#5531 / #18

Chris Ho & Kirsten Irgens-Moller  
17 Peralta  
San Francisco, CA 94110  
#5531 / #17

Patrick Leonie  
20 Peralta  
San Francisco, CA 94110  
#5512 / #3D

Clare Wren  
23 Peralta  
San Francisco, CA 94110  
#5531 / #16

Ruth and Edmond Farias  
26 Peralta  
San Francisco, CA 94110  
#5512 / #3E

Philip and Mary Alotis  
27 Peralta  
San Francisco, CA 94110  
#5531 / #15

Philip and Mary Alotis  
29 Peralta  
San Francisco, CA 94110  
#5531 / #15

Seth Jacobson  
32 Peralta  
San Francisco, CA 94110  
#5512 / #3F

Seth Jacobson  
401 Roland Way #220  
Oakland, CA 94621  
#5512 / #3F

Michael Keeth  
35 Peralta  
San Francisco, CA 94110  
#5531 / #14

Frank and Monica Wallace  
38 Peralta  
San Francisco, CA 94110  
#5512 / #3G

Vivian NaJarro  
41 Peralta  
San Francisco, CA 94110  
#5531 / #13

Vivian NaJarro  
1235 Mc Alister St #112  
San Francisco, CA 94115  
#5531 / #13

Max Kirkberg  
44 Peralta  
San Francisco, CA 94110  
#5512 / #4

Lucy Gomez  
48 Peralta  
San Francisco, CA 94110  
#5512 / #5A

Rochelle Kimball  
51 Peralta  
San Francisco, CA 94110  
#5530 / #1

Jose Gutierrez  
53 Peralta  
San Francisco, CA 94110  
#5530 / #16

Harry Lampiris  
59 Peralta  
San Francisco, CA 94110  
#5530 / #15

Stephan Tyler  
247 28th Street  
San Francisco, CA 94131  
#5530 / #15

Christa Dibiase  
65 Peralta  
San Francisco, CA 94110  
#5530 / #14

Amy Darius  
71 Peralta  
San Francisco, CA 94110  
#5530 / #13

Elizabeth Pittinos  
75 Peralta  
San Francisco, CA 94110  
#5530 / #12

Teresa Kennett  
76 Peralta  
San Francisco, CA 94110  
#5513 / #10

Jason Porter  
77 Peralta  
San Francisco, CA 94110  
#5530 / #12

Suzanne Skull  
78 Peralta  
San Francisco, CA 94110  
#5513 / #11A



10.0367D

2A

Saeed Mirfattah  
80 Peralta  
San Francisco, CA 94110  
#5513 / #11B

Michael Perry  
82 Peralta  
San Francisco, CA 94110  
#5513 / #11C

Richard and Kathleen Fine  
84 Peralta  
San Francisco, CA 94110  
#5513 / #12A

David Krakower  
86 Peralta  
San Francisco, CA 94110  
#5513 / #12B

Charles Turner  
88 Peralta  
San Francisco, CA 94110  
#5513 / #13

Douglas Haijsman  
90 Peralta  
San Francisco, CA 94110  
#5513 / #14

Frank Camp  
92 Peralta  
San Francisco, CA 94110  
#5513 / #15

Robert Besso  
709 York Street  
San Francisco, CA 94110  
#5512 / #8

Yuko Murakami  
1528 Hampshire  
San Francisco, CA 94110  
#5513 / #3

Baily Smith  
1529 Hampshire  
San Francisco, CA 94110  
#5512 / #5

Fred Hampton  
1534 Hampshire  
San Francisco, CA 94110  
#5513 / #4

Frank Scrivini  
1535 Hampshire  
San Francisco, CA 94110  
#5512 / #14

Margo Freistadt  
1540 Hampshire  
San Francisco, CA 94110  
#5513 / #5

Rudy Concepcion  
1541 Hampshire  
San Francisco, CA 94110  
#5512 / #13

Brad Drain  
1545 Hampshire  
San Francisco, CA 94110  
#5512 / #30

Yukiko Tominaga  
1546 Hampshire  
San Francisco, CA 94110  
#5513 / #6

R Hernandez  
1550 Hampshire  
San Francisco, CA 94110  
#5513 / #7

Keith Willson  
1552 Hampshire  
San Francisco, CA 94110  
#5513 / #7

Paul Espanoza  
1555 Hampshire  
San Francisco, CA 94110  
#5512 / #32

Erik Bourget  
1556 Hampshire  
San Francisco, CA 94110  
#5513 / #8

Norman and Sharon Kaman  
1563 Hampshire  
San Francisco, CA 94110  
#5512 / #9

Mark Lowe  
1566 Hampshire  
San Francisco, CA 94110  
#5513 / #9

Mark Lowe  
30 Grandview Ave. #303  
San Francisco, CA 94114  
#5513 / #9

Mike McCabe  
1569 Hampshire  
San Francisco, CA 94110  
#5512 / #8

Teresa Kennett  
1570 Hampshire  
San Francisco, CA 94110  
#5513 / #10

Reza Khoshenvisan  
1256 Howard Street  
San Francisco, CA 94103  
#5512 / #29 #31



# SAN FRANCISCO PLANNING DEPARTMENT

1650 Mission Street Suite 400 San Francisco, CA 94103

## NOTICE OF BUILDING PERMIT APPLICATION (SECTION 311)

On December 31, 2009, the Applicant named below filed Building Permit Application No. 2009.12.31.4050 and 2009.12.31.4052 (New Construction of Two Buildings) with the City and County of San Francisco.

CONTACT INFORMATION		PROJECT SITE INFORMATION	
Applicant:	Reza Khoshnevisan	Project Addresses:	54-56 and 60-62 Peralta Ave
Address:	1256 Howard Street	Cross Streets:	Holladay Ave and Hampshire St
City, State:	San Francisco, CA 94103	Assessor's Block /Lot No.:	5512/029 and 031
Telephone:	(415) 922-0200	Zoning Districts:	RH-2 / 40-X

Under San Francisco Planning Code Section 311, you, as a property owner or resident within 150 feet of this proposed project, are being notified of this Building Permit Application. You are not obligated to take any action. For more information regarding the proposed work, or to express concerns about the project, please contact the Applicant above or the Planner named below as soon as possible. If your concerns are unresolved, you can request the Planning Commission to use its discretionary powers to review this application at a public hearing. Applications requesting a Discretionary Review hearing must be filed during the 30-day review period, prior to the close of business on the Expiration Date shown below, or the next business day if that date is on a week-end or a legal holiday. If no Requests for Discretionary Review are filed, this project will be approved by the Planning Department after the Expiration Date.

### PROJECT SCOPE

- |   |        |  |    |  |
|---|--------|--|----|--|
| <input type="checkbox"/> DEMOLITION               | and/or | <input checked="" type="checkbox"/> NEW CONSTRUCTION | or | <input type="checkbox"/> ALTERATION              |
| <input type="checkbox"/> VERTICAL EXTENSION       |        | <input type="checkbox"/> CHANGE # OF DWELLING UNITS  |    | <input type="checkbox"/> FACADE ALTERATION(S)    |
| <input type="checkbox"/> HORIZ. EXTENSION (FRONT) |        | <input type="checkbox"/> HORIZ. EXTENSION (SIDE)     |    | <input type="checkbox"/> HORIZ. EXTENSION (REAR) |

### PROJECT FEATURES (FOR EACH BLDG)      PROPOSED CONDITIONS

	54-56 Peralta Ave	60-62 Peralta Ave
FRONT SETBACK.....	+/- 2 feet	+/- 2 feet
FRONT SETBACK (at Top Floor).....	+/- 7 feet	+/- 7 feet
SIDE SETBACK.....	+/- 4 feet (southern side)	+/- 4 feet (southern side)
BUILDING DEPTH (Lowest Floor).....	+/- 54 feet 6 inches	+/- 41 feet
REAR YARD.....	+/- 44 feet 7 inches	+/- 33 feet 4 inches
HEIGHT OF BUILDING (from front curb).....	+/- 29 feet	+/- 29 feet
NUMBER OF STORIES (front).....	3	3
NUMBER OF STORIES (rear).....	3 plus Crawlspace	4
NUMBER OF PARKING SPACES.....	4 (2 provided for 60-62 Peralta Ave)	0 (2 provided at 60-62 Peralta Ave)
NUMBER OF DWELLING UNITS.....	2	2

### PROJECT DESCRIPTION

The proposal is to construct two new two-family homes on down-sloping adjacent lots (currently vacant), resulting in a total of four new dwelling units. The 52-54 Peralta Ave building will contain four off-street parking spaces, of which two will be reserved for the units in the proposed adjacent building at 60-62 Peralta Ave, resulting in one off-street parking space for each of the four new units. Providing only four parking spaces for the two buildings requires a variance from Planning Code Section 242(e). The parking variance for this project is *tentatively* scheduled to be heard by the Zoning Administrator on September 22, 2010. Separate notice will be issued for the variance hearing.

PLANNER'S NAME:	Corey Teague	DATE OF THIS NOTICE:	10-13-10
PHONE NUMBER:	(415) 575-9081	EXPIRATION DATE:	10-12-10
EMAIL:	corey.teague@sfgov.org		



# Bernal Heights

## East Slope Design Review Board

Terry Milne, external secretary • 321 Rutledge • San Francisco 94110 • [285-8978]

June 5, 2010

SIA Consulting Corporation  
1256 Howard Street  
San Francisco, CA 94103

Re: 54 – 62 Peralta Avenue

Dear Applicant,

The Bernal Heights East Slope Design Review Board held a neighborhood meeting on Monday April 19, 2010 to review your revised plans for a project that proposes to construct a two-unit residential building on each of two adjoining lots that are currently vacant. The April meeting was a follow-up to comments from the Board that was discussed at our previous meetings on July 28 and August 11, 2009.

The Board regrets to inform you that we cannot recommend that the Department of City Planning approve this project as proposed. The Board also cannot recommend approval of the parking variances that are required to construct the project as currently planned. The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines.

The Board's primary concern about your project is your request for a variance for three parking spaces. The drawings indicate that the total area of 54 – 56 Peralta Avenue will be 2,342 s.f., which requires three off-street parking spaces pursuant to Section 241 of the Planning Code. The drawings also indicate that the total area of 60 – 62 Peralta Avenue will be 3,245 s.f., which requires four off-street parking spaces. You have proposed a total of four off-street parking spaces, with two of the spaces deeded to 60 – 62 Peralta, where a total of seven spaces are required. The Board feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project's reliance on neighborhood parking. The Board recognizes that off-street parking cannot be provided at 60 – 62 Peralta and the difficulty in providing additional off-street parking within 54 – 56 Peralta, however, we feel that the parking deficit is more a reflection of the size of the buildings and not site access.

The Board also has concerns about the scale of 60 – 62 Peralta at the street wall. The top floor of 60 – 62 Peralta is set back 7' from the front wall and the building looms over the rear yard of the adjacent building to the west on Hampshire Street. With the minimal front setback the top floor would be clearly visible from the street thus interrupting the stepping of the rooflines at the street wall. To address this situation the Board suggests setting the top floor back a minimum of 15' from the front wall and setting it back 5' from the west side property line.

You indicated that your project would necessitate changes to the public right-of-way adjacent to the property. Your plans indicate that the sidewalk slope would become steeper but the full scope of the proposed changes is unclear. We suggest providing a call-out detail of the sidewalk changes with an existing and proposed transverse section because it is an important element of your project that needs to be accurately understood by all who

review your project. The sidewalk at this location is fairly steep and we would prefer that stairs be added instead of a steeper slope. Furthermore, we want to know if a Public Works hearing will be required for the changes.

5A

Both buildings are set back a minimal amount leaving little room for landscaping. The Board suggests adding planter/window boxes to the façade where appropriate to make up for the lack of landscaping. This type of greenery will help soften the edges of the building, add texture, and create intimacy.

Neighbors in attendance at the meeting expressed similar concerns to those of the Board and have drafted a letter describing their concerns (see attached). The Board strongly urges you to reconsider these aspects of your project and we welcome another meeting if you are interested in resolving these issues.

The Board wishes to thank you for presenting the plans to the neighborhood. Since the Board is not a City agency, it does not have the power to either approve or disapprove the permit application.

Cordially,

Jeff Saydah, Chair, Bernal ESDRB

May 18, 2010

Dear members of the Bernal Heights East Slope Design Review Board,

This is a letter from the neighbors most directly affected by the proposed project at 56-60 Peralta Ave.

We appreciate the time that the Bernal Heights Design Review Board has taken with this proposed development. As you know, this project has caused quite a bit of concern in the neighborhood given the scale of the development proposed & the severe, negative impact it will cause to many residents living on Peralta, Hampshire, Holladay and other adjacent streets. This particular neighborhood & surrounding blocks have a long history of single family homes & we find it entirely out of character to be faced with a 4-unit development on such precarious lots with so many negative impacts to the surrounding neighbors.

Our main areas of concern:

**Parking:** The design for the 4-car garage is completely unrealistic as much as the developer would have us believe otherwise. They are asking for a 3-car variance in an area without a lot of options for parking. They will be removing a street parking space with the construction of the garage. This is potentially putting 8 or more cars on our street & that is just too much given the already overburdened vehicle density homeowners are experiencing. Also, we are completely against granting any sort of parking variance for any number of vehicles. They need to come up with a realistic plan for how they will deal with 7 parking spaces without the use of any kind of parking variance.

**Roof Heights:** The planset presented at the latest meeting shows building heights that are not consistent with the existing slope of the neighboring houses.

**Sidewalk Slope:** Several neighbors have expressed concern that the developer is proposing to create a steeper sloped sidewalk in order to accommodate an already suspicious looking garage design. This is a serious issue as we have many longtime aging residents in the neighborhood who walk & this would put them at risk. As well, this may be against ADA requirements as regards new construction.

**Backyard & Daylight Plane:** Adjacent neighbor's backyard & daylight plane are being completely obscured by the height of the proposed building.

This is just too much development for such small lots. These lots if built upon should not be overbuilt just so the current owner & developer can make large profits at the expense of the neighborhood, area homeowners & local building regulations. Why not build a total of two houses on the two lots? - that would be more in following with the existing houses in the neighborhood.

We respect your commitment and experience in helping to represent Bernal Residents deal with new construction. In this case we see a developer driving the process to build

with only profits in mind without regard for the current architecture or residents. Their plans & presentations have tried to paper over the fact that they are trying to build too much on too small of a space. We are all completely against this proposal as it has been presented & would like to see your support in our efforts to block this proposal.

We are not however completely against building on this space. A lot owner should be allowed to build if they follow city ordinances & neighborhood guidelines. Two lots, two houses. Any more than that given the constraints of the lots & parking is just unrealistic & would burden this neighborhood significantly.

We would like to have this letter submitted to the city as part of the Bernal Heights East Slope Design Review Board materials. Thank you for your attention to this matter.

Below are the signatures we have collected from neighbors most directly affected by this project & who agree with the contents of this letter.

CC: SF Planning Department

Signed,

Address:

CLARE H. WREN

23 PERALTA AVE SF CA 94110

Paul A. Mickelson

29 Peralta S.F. CA.

Philip A. Gotti

27 PERALTA

Carol St...

41 PERALTA

Mary He...

41 Peralta SF. CA 94110

Ang Pader

41 Peralta SF, CA 94110

Wendy...

65 PERALTA S.F. CA 94110

Wendy...

35 Peralta S.F. CA

[Signature]

35 Peralta Ave.

Sindi Smit

18 Peralta St. SF. CA

2 w/ signatures

8A

\_\_\_\_\_

\_\_\_\_\_

Gabriel Pro

44 Peralta

Joan Kearney

1569 Hampshire St.

Rochelle Kimball

51 PERALTA AVE.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Signed,

Address:

H. Igawa-Mollu

17 Peralta Ave SF 94110

Lewis C. Randall

2 Peralta Ave SF 94110

Chujik

5 PERALTA AVE 94110

Jim O

105 HOLLADAY AVE APT B

Carl M. Passer

8 PERALTA AVE.

Jim Pat

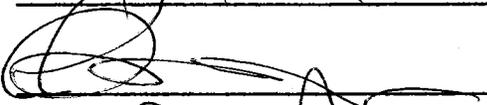
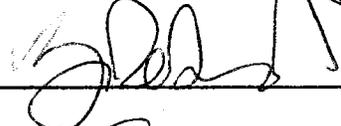
20 Peralta Ave

Max C Kirkley

44 Peralta Ave

Robert Bruce

44 Peralta Ave

<u>Danelle Maddy</u>	<u>44 PERALTA</u>
<u>Lucy Gomez</u>	<u>48 Peralta</u>
<u>DANIAN Gomez</u>	<u>48 PERALTA</u>
<u>Sara Gilroy</u>	<u>1569 Hampshire St</u>
<u>Diane Barton. Pin</u>	<u>1563 Hampshire St.</u>
<u>Leah Sheldon</u>	<u>1557 Hampshire St.</u>
<u>Saeed Mirfattal</u>	<u>80 Peralta Ave.</u>
<u>Dick Fine</u>	<u>84 Peralta</u> 
	<u>17 PENNA N.</u>
 zia Desantis	<u>76 Peralta Ave</u>
 Nick DeSantis	<u>76 Peralta Ave</u>
 Lucinda CAKE	<u>120 Holladay Ave.</u>

Signature:  
Thomas Duarte  
Paul Murphy

Address:  
14 Peralta Ave SF  
14 Peralta Ave SF

10-21-2010

## Far Northeast Bernal Heights Neighborhood Block Club

Re: Proposed Development at 54-62 Peralta Ave

Our neighborhood and Block Club met with the developer SIA Consulting Corporation, and the property owner, through a series of meetings convened by the Bernal Heights East Slope Design Review Board. Following our last meeting with the developer, the Review Board issued letter to the developer, dated June, 5, 2010, listing a number of issues that needed to be addressed prior to a recommendation of the project to the planning department. Among other things, the Review Board letter states "The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines". And, after identifying similar neighborhood concerns, "The Board strongly urges you to reconsider these aspects of your project and we welcome another meeting if you are interested in resolving these issues".

Unfortunately rather than coming back to the neighborhood Review Board with revised plans for a follow-up neighborhood meeting, the developer ignored the recommendations of the Board and proceeded to file for a parking variance. This variance filling by the developer and owner shows complete disregard of the neighborhoods concerns and the Bernal Heights East Slope Design Review Board.

### Far Northeast Bernal Heights Neighborhood Context:

**Neighborhood Consistency:** Both the East Slope Design Review Board and our Block Club asked this developer to scale back the size of this multi-unit development to bring it in line with the fabric and character of our neighborhood. Many of our homes are Victorian in age, single family residences, some of which have a small, in-law rental unit at ground level. The Review Board letter describes the top floor of 60 – 62 as looming over the rear yard of the adjacent building to the west on Hampshire Street and interrupting the stepping of the rooflines at the street wall. "To address this situation the Board suggests setting the top floor back a minimum of 15' from the front wall and setting it back 5' from the west side of the property line." We've all seen & experienced the egregious consequences of large scale developments amid a fabric of single-family homes in other parts of our city and we value the architectural treasure of our Victorian middle class neighborhoods.

**Transportation:** Our neighborhood is on a very steep hillside, making access to what little public transportation that is available, difficult at best. We are not served directly by public transportation, and our nearest BART station is a half hour walk for a young person in good health. Owning a vehicle in this neighborhood is therefore a necessity for most, and parking is already difficult.

**Parking:** The developer is requesting a parking variance from the seven spaces required by our neighborhood guidelines and has provided only 4 spaces underground in one of the two buildings. Parking in 3 of these 4 spaces requires use of a sliding parking pallet. The pallet however will not do the job for these 3 spaces as it can be loaded/unloaded from only one direction. In addition, to quote the Review's letter, "The Board feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project's reliance on neighborhood parking." The Board further states, "...we feel that the parking deficit is more a reflection of the size of the buildings and not site access." In addition the development will remove a minimum of 10' of existing street parking with the addition of their garage driveway.

**Sidewalk Slope/Changes to Public Right-of-Way:** Our neighborhood's sidewalks already have a steep grade and this project proposes to create an even steeper slope on Peralta Avenue to build a level driveway access to the proposed underground garage. This is a change to the public right-of-way and a serious issue of great concern to our many longtime, aging residents in the neighborhood, some living here for more than forty years. A steeper sidewalk puts them at risk of injury. The Review Board states in their June 5<sup>th</sup> letter, "...the full scope of the proposed changes in unclear. We suggest providing a call-out detail of the sidewalk changes with an existing and proposed transverse section because it is an important element of your project that needs to be understood by all who review the project. The sidewalk at this location is very steep and we would prefer that stairs be added instead of a steeper slope. Furthermore, we want to know if a Public Works hearing will be required for the changes." Stairs, however

**Northeast Bernal Heights Neighborhood Block Club****2**

**Sidewalk Slope/Changes to Public Right-of-Way, (cont.):** are not an acceptable solution to the neighbors as these prevent public access by wheel chairs, baby strollers, etc..

**Flooding:** There are unresolved drainage issues for downslope neighbors that relate to removing a ten foot section of the retaining wall and curb along Peralta Avenue to accommodate the garage driveway of this proposed development. When it rains Peralta Avenue becomes Peralta Creek with a downhill torrent of rain water roaring down our steep street. The retaining wall on Peralta Avenue, in front of this proposed development, holds the water in the street and prevents it from flooding the downhill houses on Hampshire Street. It is of great concern that removing a ten foot section of this retaining wall and curb will result in flooding our downhill Victorians along the east side of Hampshire.

**Roof Heights & Stepping Rooflines:** The development's design as presented shows proposed building heights that are not consistent with the neighborhood, (they are higher than the uphill neighbor's house), with the existing slope & the elevations of the neighboring houses. The development is a monolithic structure not respecting the hillside with its stepping rooflines at both the street wall on Peralta Avenue and the Victorian houses stepping down Hampshire.

**Daylight Access:** The adjacent houses on both Hampshire and Peralta have their access to daylight over shadowed by the immensity of this development. The adjacent homes on Hampshire will have all morning light completely obliterated by building 60-62, which would loom over them by nearly 20 feet above roof peaks and 40 feet above back yards!

**Landscaping:** The Review Board states, "Both buildings are set back a minimal amount leaving little room for landscaping." They recommend landscaping to, "...soften the edges of the buildings, add texture and create intimacy." Their suggestion is the addition of planter/window boxes to the facade where appropriate to make-up for the lack of landscaping. The developer's plans show trees planted in front of the development between the sidewalk and the street that are impossible. There is no space between the already narrow sidewalk and the street.

### **We are not opposed to any construction.**

Our main objection to this multi-family development is its size, and variance to parking requirements. More appropriate for the neighborhood context is a single family residences.

- The developer should be required to respect the neighborhood's character of single family homes with stepped rooflines. The installation of story poles would greatly help in our review of this proposal.
- The Developer should not be allowed to rely on neighborhood street parking, but provide off street parking that meets the Bernal Heights East Slope Design Review Board Guidelines. We believe that the proposed parking garage is inadequate, impractical and dangerous to pedestrians, as vehicle access and egress will be blind to the garage user. If a motorized pallet is allowed, the developer should be required to specify on his plans a pallet that actually performs in the space specified in the plans.
- The Developer needs to come up with a drainage plan that will address the runoff water that will impact the downhill neighbors due to the removal of a section of the retaining wall and curb. A Public Works hearing should be a requirement for this development to proceed.
- The Developer should be required to address a call-out detail of the sidewalk changes as discussed so these can be accurately understood. Neither stairs nor a steeper slope are acceptable to the neighbors.
- The Developer needs to amend their design so that the backyard and daylight access of the existing adjacent residences are not completely obscured with a huge building looming over them.



12A

**Product Data**  
**Parking Pallet**  
**PQ-DC/PQ-AC**  
 crosswise shifting

Parking pallet PQ-DC  
 with current supply from rail  
 Parking pallet PQ-AC  
 with current supply over trailing  
 cable

**Dimensions:**  
 All space requirements are minimum  
 finished dimensions. Tolerances for space  
 requirements <sup>+3</sup>/<sub>0</sub>. Dimensions in cm.



Type	LP	L	B
PQ-210	182	366	210
PQ-220	192	366	220
PQ-236*			

\* = Standard type

We generally recommend to use type  
 PQ-DC-236 where possible.

**Suitable for:**

Standard passenger car and station wagon.

**Car dimensions**

<b>length</b>	<b>max. 5.00 m</b>
<b>width</b>	<b>max. 1.80 (PQ-210)</b> <b>max. 1.90 (PQ-220)</b> <b>max. 2.08 (PQ-236)</b>
<b>weight</b>	<b>max. 2000 kg</b>
<b>wheel load</b>	<b>max. 500 kg</b>

Standard passenger cars are vehicles  
 without any sports options such as  
 spoilers, low-profile tyres etc.



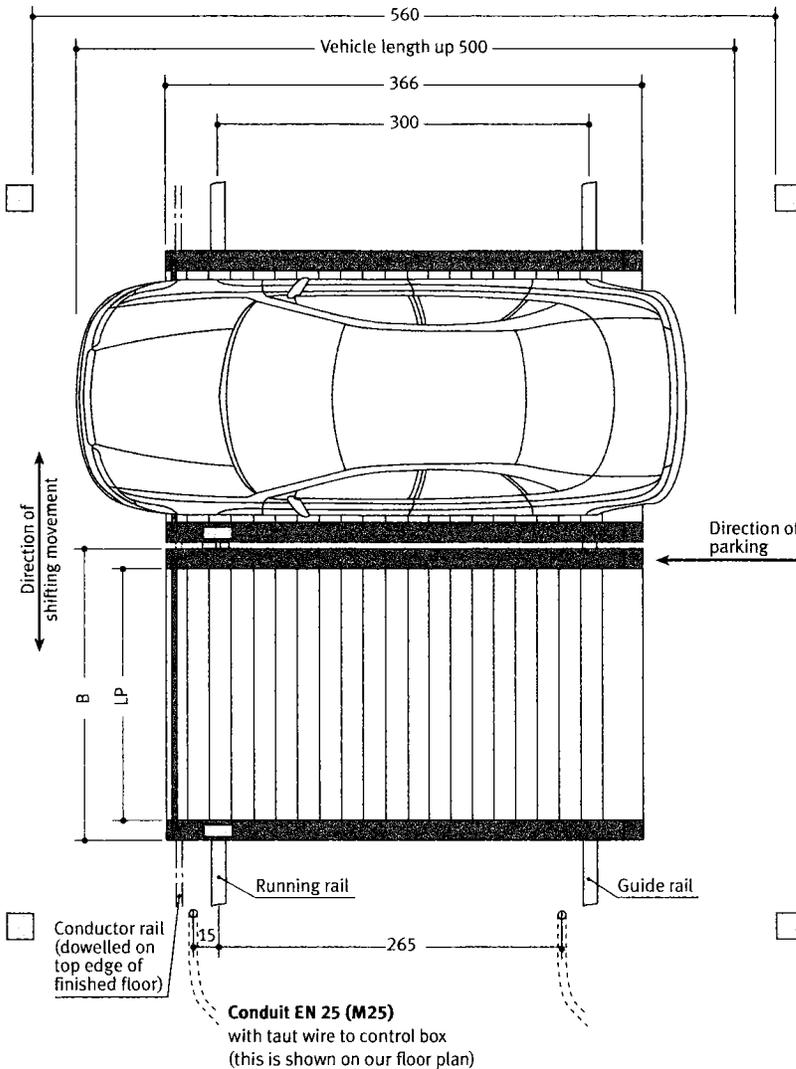
**Klaus Multiparking GmbH**  
 Hermann-Krum-Straße 2  
 D-88319 Aitrach

Phone **+49-75 65-508-0**  
 Fax **+49-75 65-508-88**

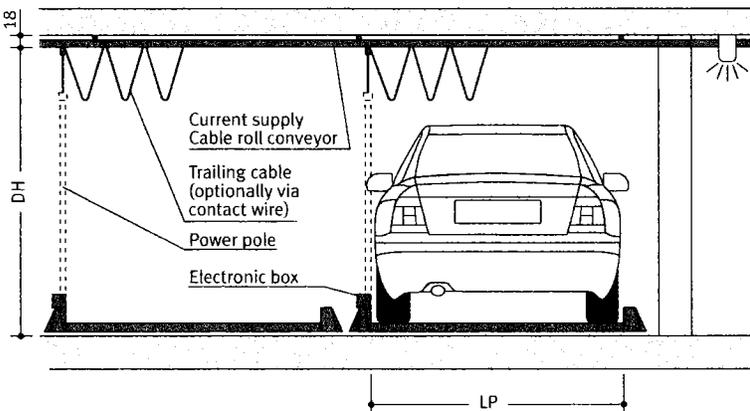
E-Mail **info@multiparking.com**  
 Internet **www.multiparking.com**

- Page 1  
Dimensions  
Car data
- Page 2  
Dimensions  
Recess/  
Rail system
- Page 3  
Eveness/  
Tolerances  
Function
- Page 4  
Electrical  
installation  
Technical  
data
- Page 5  
To be per-  
formed by the  
customer  
Description

**Parking Pallet PQ-DC**  
 (with current supply from rail)



**Parking Pallet PQ-AC**  
 (with current supply over trailing cable/optionally via contact wire)



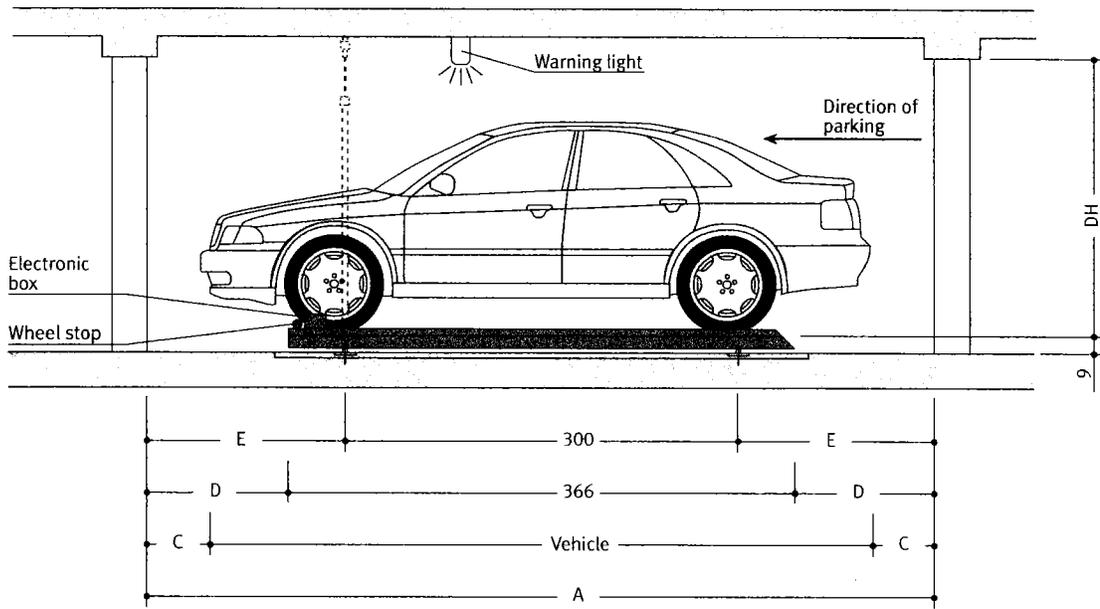
**Notes**

A safety clearance of 30 cm must be maintained between the front or rear bumpers of vehicles on parking pallets and any fixed parts of the surroundings or other vehicles in accordance with DIN EN 14 010. At a max. vehicle length of 500 cm, this means a length dimension of 560 cm between the columns. The length dimension of 560 cm can only then be shortened if the max. vehicle or parking place length is reduced or light barriers are used.

The operating console must be mounted in such a way that the operator can see the entire system during operation and the motion sequences can be observed and monitored.

13A

- Page 1  
Dimensions  
Car data
- Page 2  
Dimensions  
Recess/  
Rail system
- Page 3  
Eveness/  
Tolerances  
Function
- Page 4  
Electrical  
installation  
Technical  
data
- Page 5  
To be performed by the customer  
Description



A	Vehicle	C	D	E	Please note the following on parking space	DH
560	500	30	97	130	Parking space and pallet conform to German regulations and DIN EN 14010	acc. to local requirements
530	500	15	82	115	Where the unit is equipped with lighth barriers, parking space and pallet also conform to the above	acc. to local requirements
<530	<500	15	<82	<115	Light barrier is essential. Parking space does not conform to German regulations. <b>Note that length of vehicle is restricted!</b>	acc. to local requirements

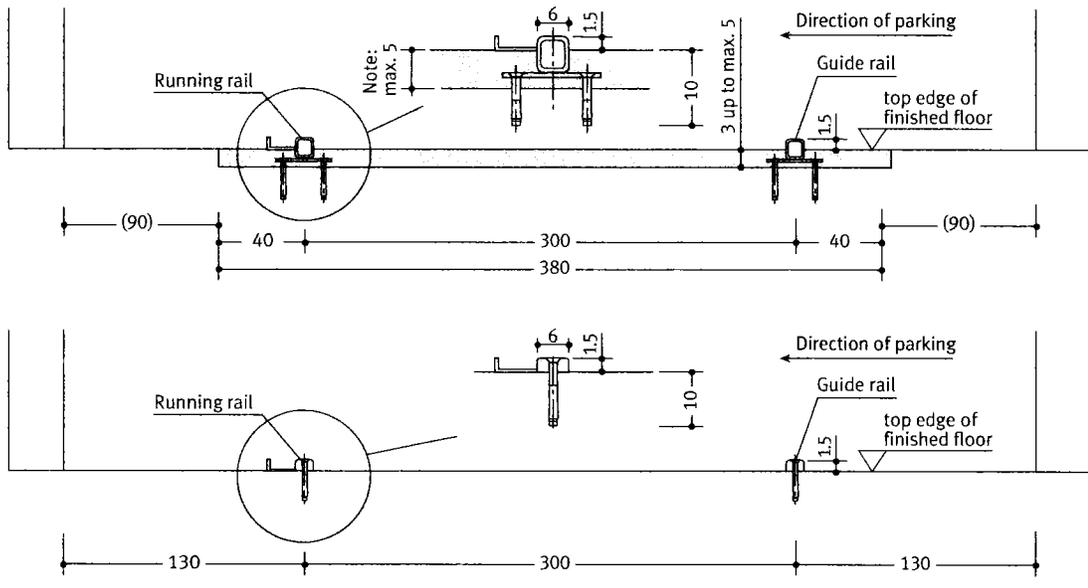
Dependent upon the structural conditions of the garage, several different options are available for installation of the rails.

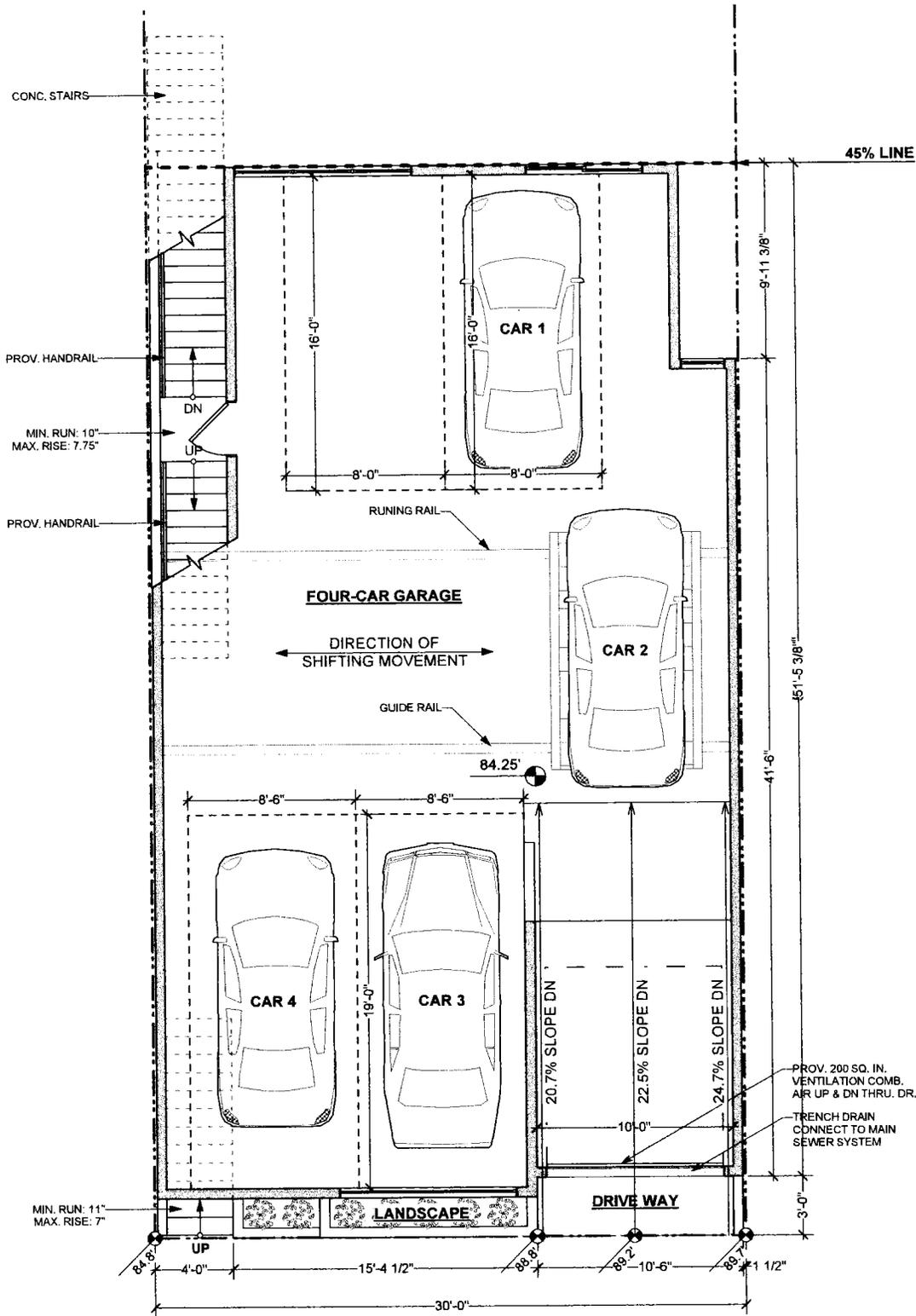
When executing the carriageway, according to the raw bottom floor combined with a cement screed, attention must be paid to the regulation that the thickness of the floor materials is determined by the structurally admissible tolerances. The set-up of the rails amounts to 3 cm (height of floor screed 4 cm).

Another variant consists in that recesses in the finished carriageway for the rails are provided by customer. After the rails have eventually been laid, the area under the rails, as well as the recesses must be topped up with concrete by the customer. When exact evenness of the carriageway has successfully been accomplished, the rails may subsequently also be dowelled onto it.

Detailed plans for correct recessing of rails are available from the local agency of Klaus.

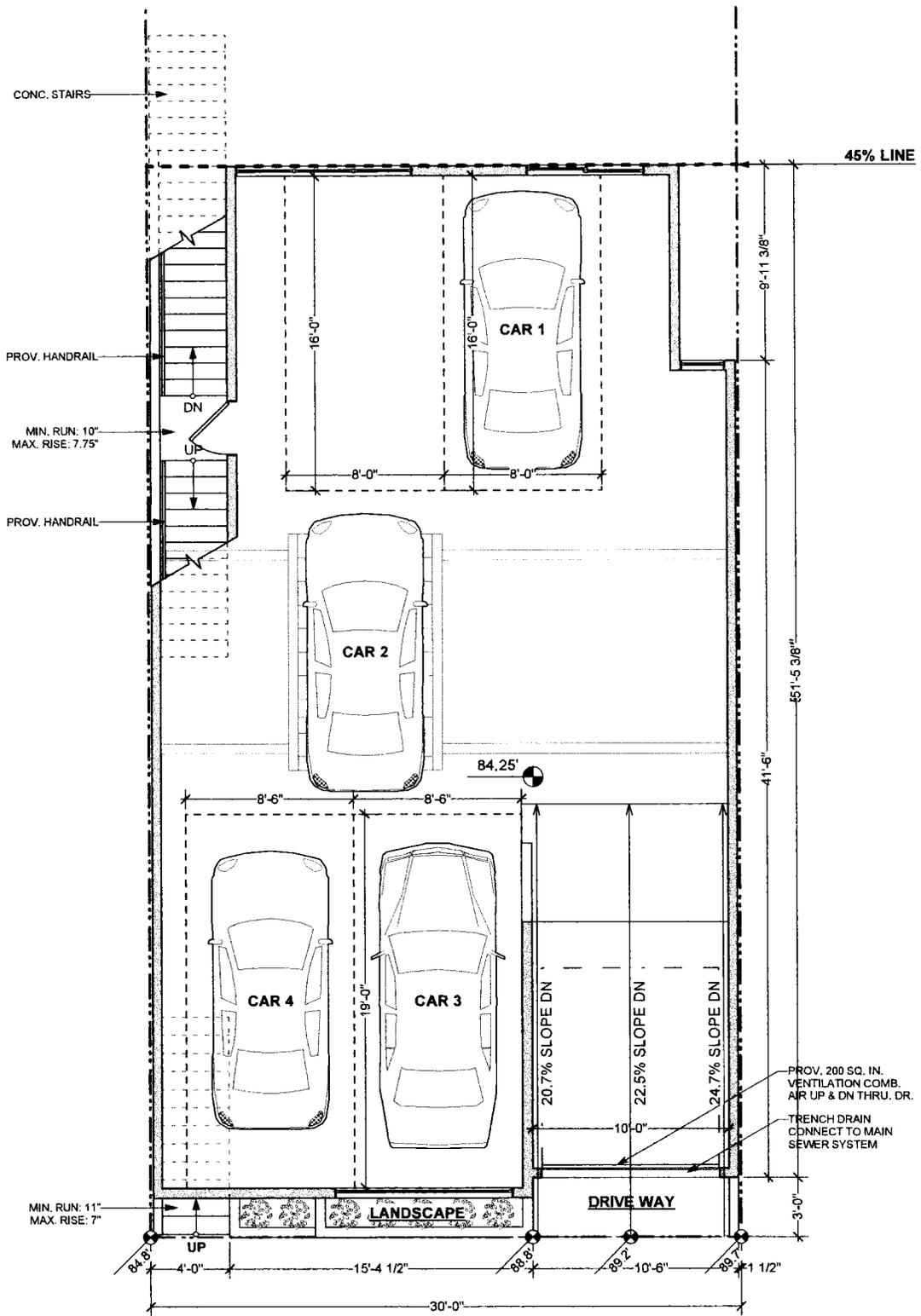
**Note:** Tolerances for the evenness of the carriageway must be strictly complied with in accordance with DIN (= German Industrial Standard) No. 18202, chart 3, line 3.





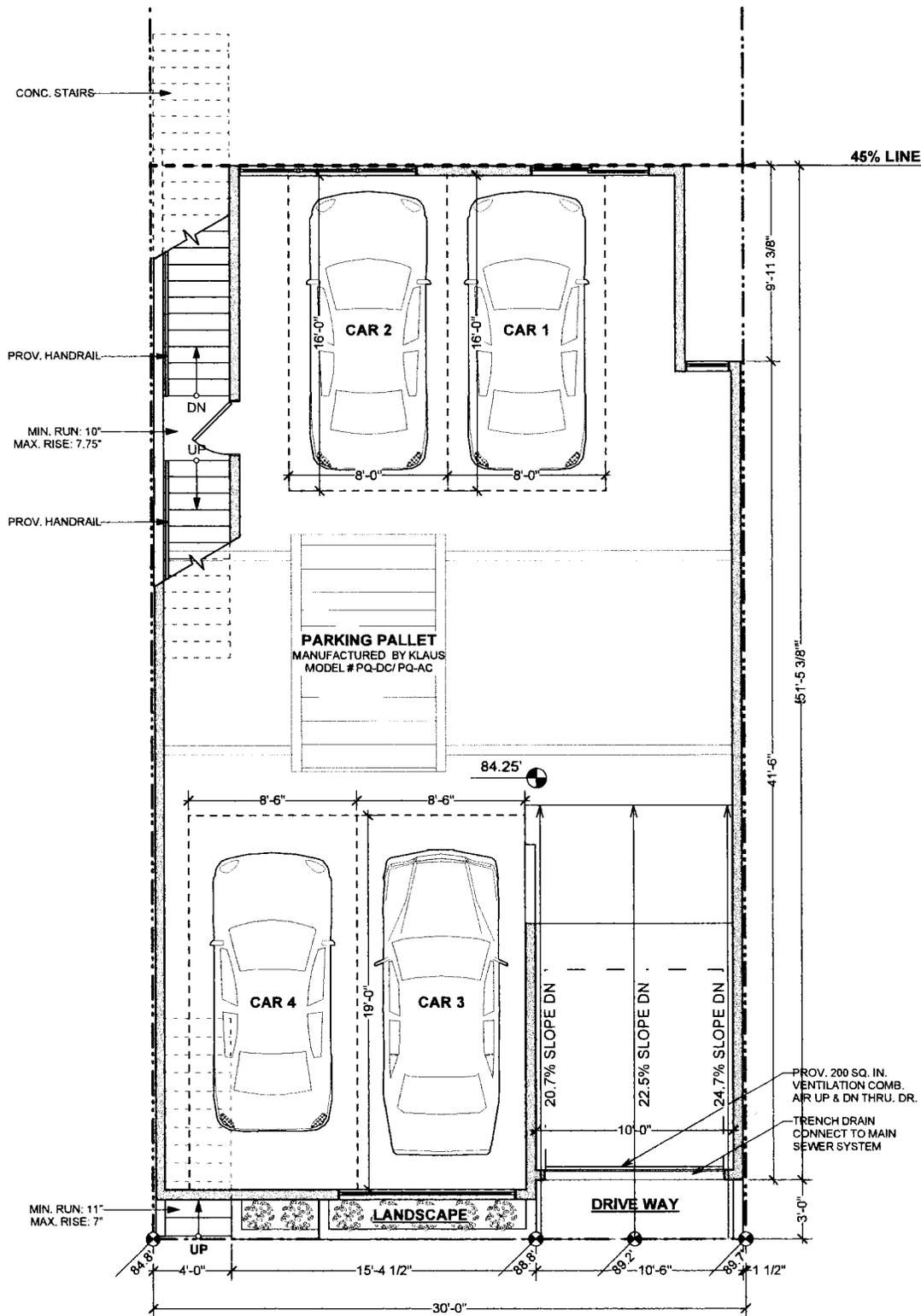
54-62 PERALTA AVE.  
SAN FRANCISCO, CA





54-62 PERALTA AVE.  
SAN FRANCISCO, CA





54-62 PERALTA AVE.  
 SAN FRANCISCO, CA



54-56 Peralta Ave.  
 (17A)

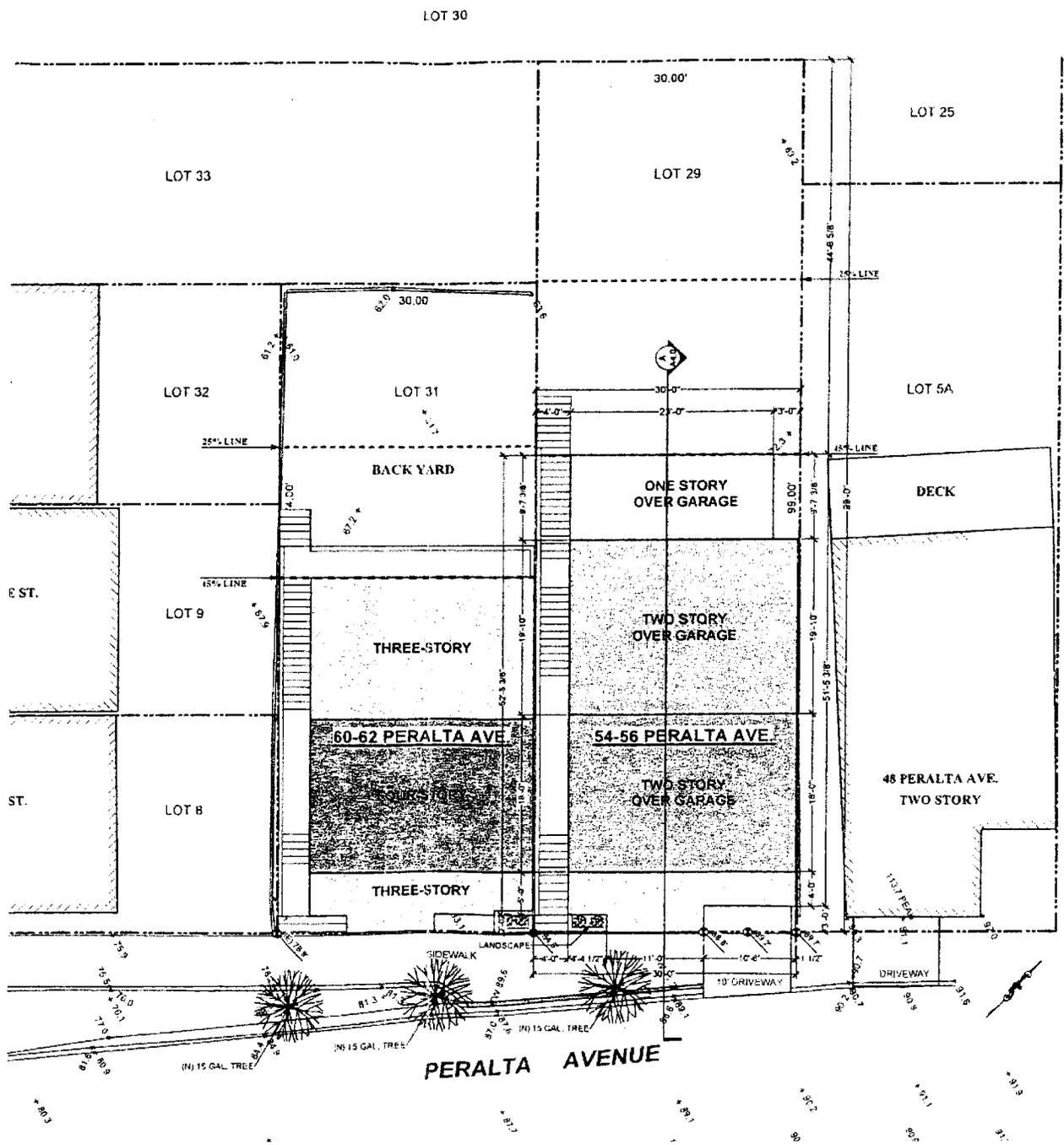
PROJECT NAME  
**54-56 PERALTA AVE**  
 SAN FRANCISCO, CA



SIA CONSULTING CORPORATION  
 1256 HOWARD STREET  
 SAN FRANCISCO CA 94103  
 TEL: (415) 922.0200  
 FAX: (415) 922.0203  
 WEBSITE WWW.SIACONSULT.COM

SHEET TITLE

**SITE PLAN & NOTES**



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ISSUES / REVISIONS		
NO.	DATE	DESCRIPTION

Sheet A-1.0  
 Job No. 08-1364A  
 SIA Consulting Corporation  
 Revised 8-26-10

54-56 Peralta Ave.

18A

A  
B  
C  
D  
E

1569 HAMPSHIRE ST.

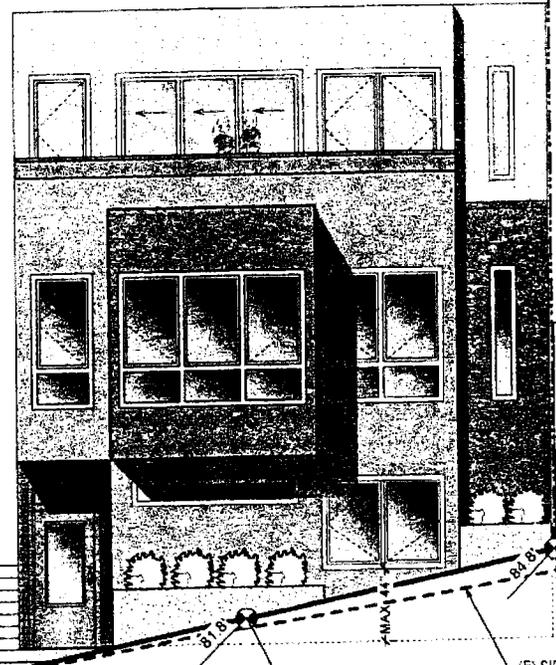
60 PERALTA AVE.

ROOF, FINISH ELEV.  
111.55'

FOURTH FLR. FINISH ELEV.  
100.55'

THIRD FLR. FINISH ELEV.  
90.55'

SECOND FLR. FINISH ELEV.  
80.55'



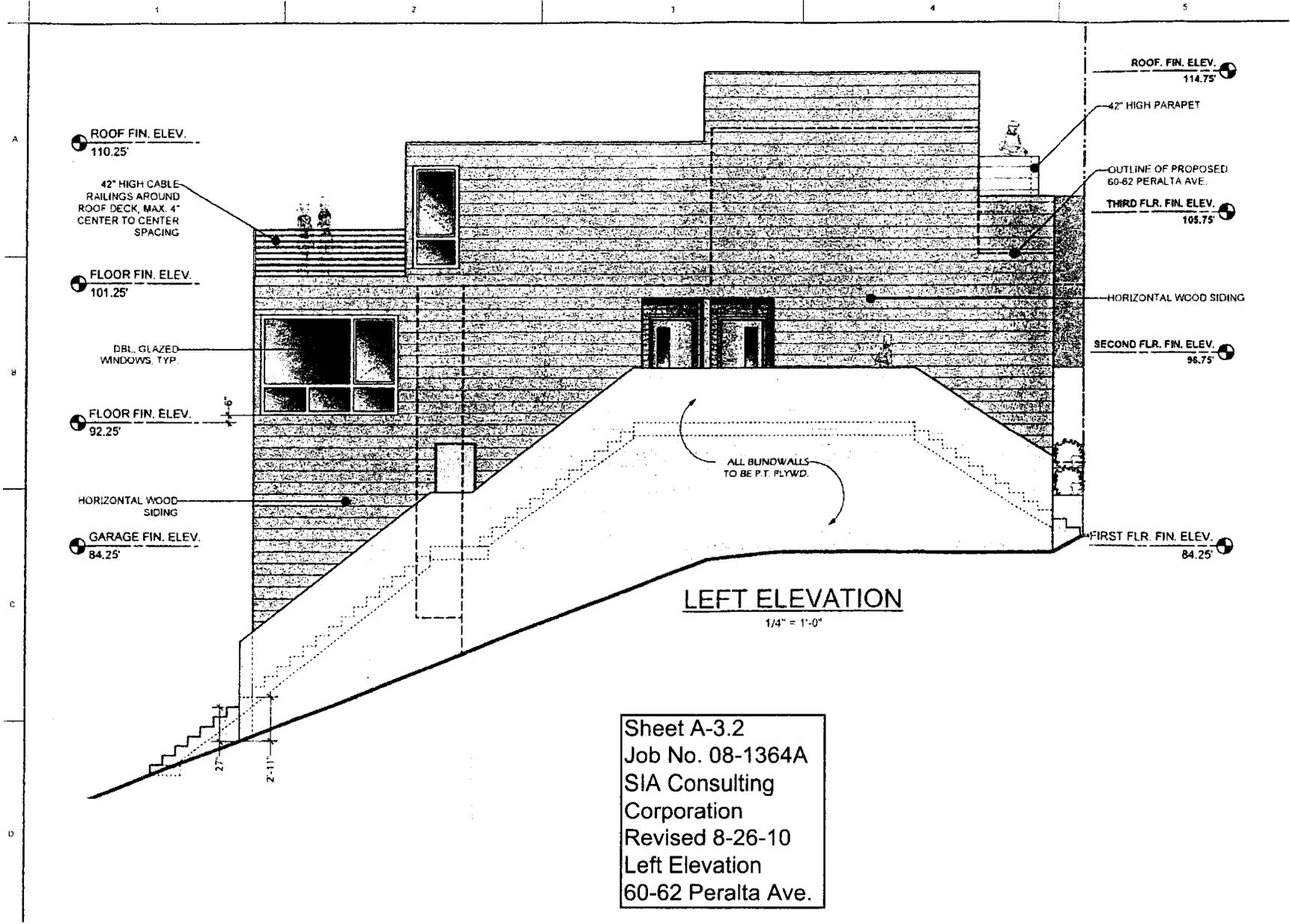
Sheet A-3.0  
 Job No. 08-1364A  
 SIA Consulting  
 Corporation  
 Revised 8-26-10  
 Front Elevation  
 60 - 62 Peralta Ave.

FRONT ELEVATION

1/4" = 1'-0"

54-56 Peralta Ave.

19A

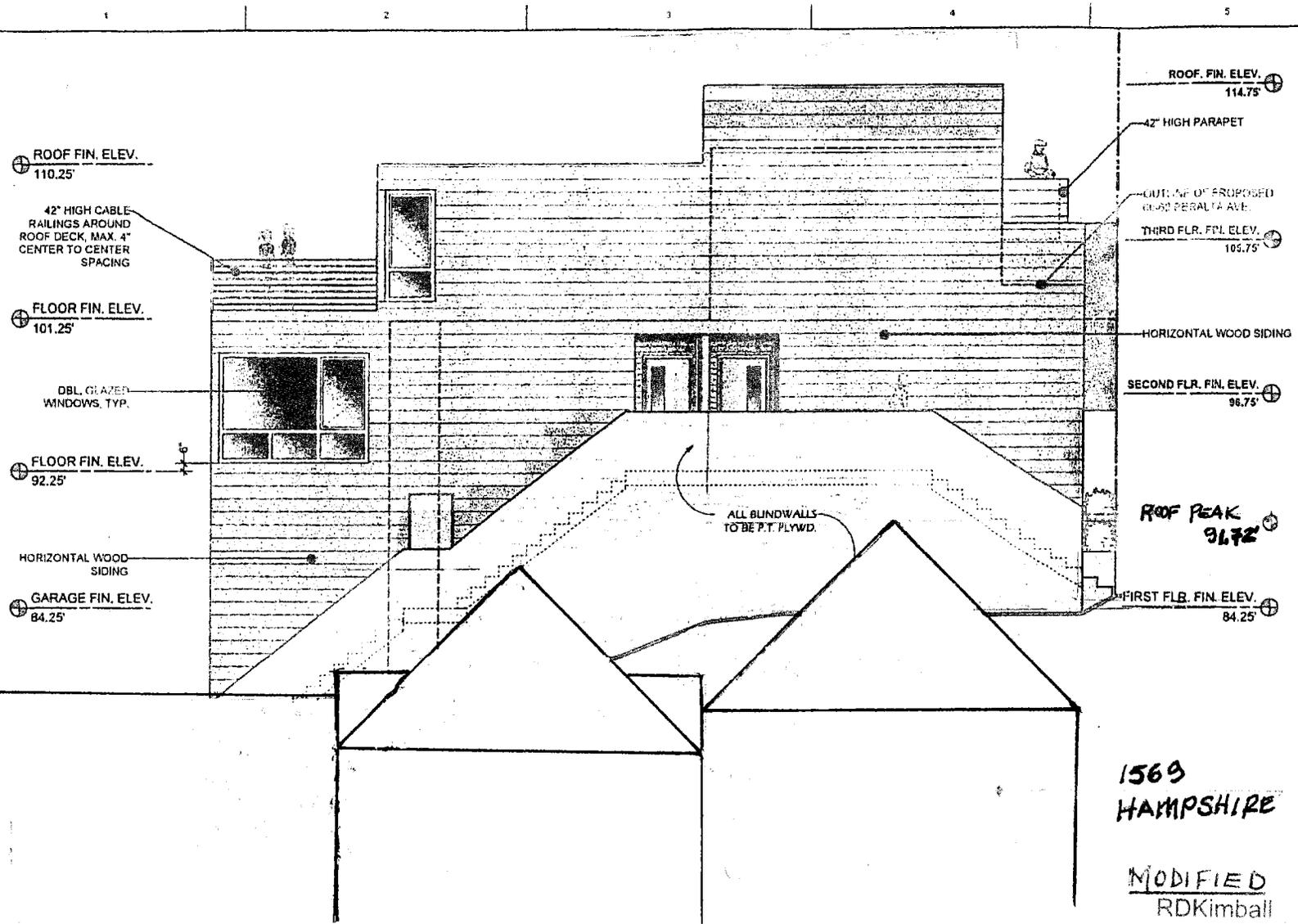


Sheet A-3.2  
 Job No. 08-1364A  
 SIA Consulting  
 Corporation  
 Revised 8-26-10  
 Left Elevation  
 60-62 Peralta Ave.

54-56 Peralta Ave  
20A

Hampshire Street Roof Lines in Elevation  
Overlaid on Left Elevation

Sheet A-3.2  
Job No. 08-1364A  
SIA Consulting Corporation  
Revised 8-26-10  
Left Elevation  
60-62 Peralta Ave.



1569  
HAMPSHIRE

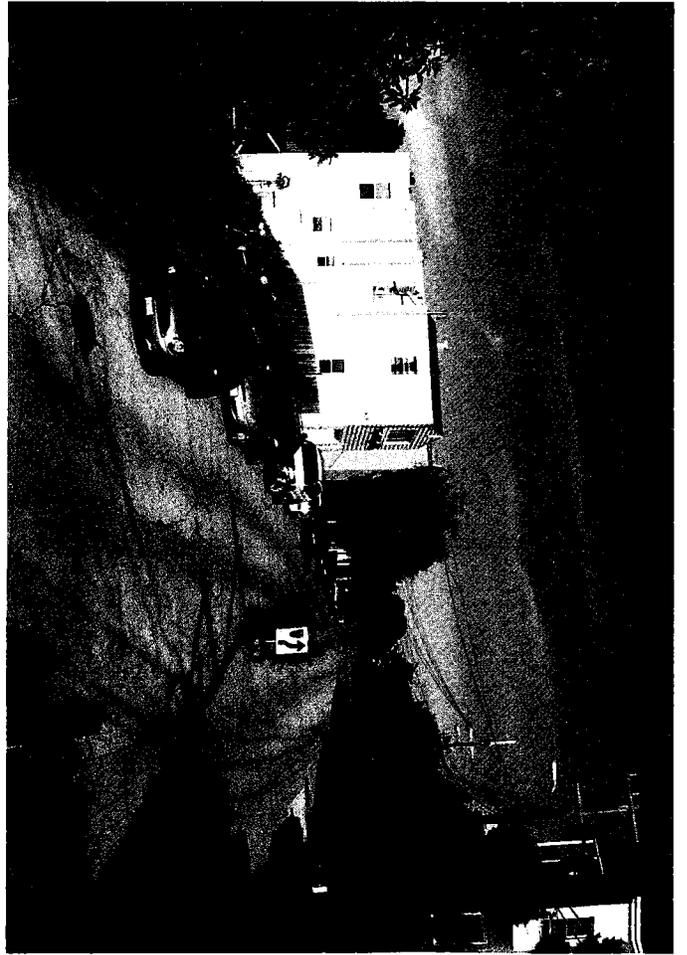
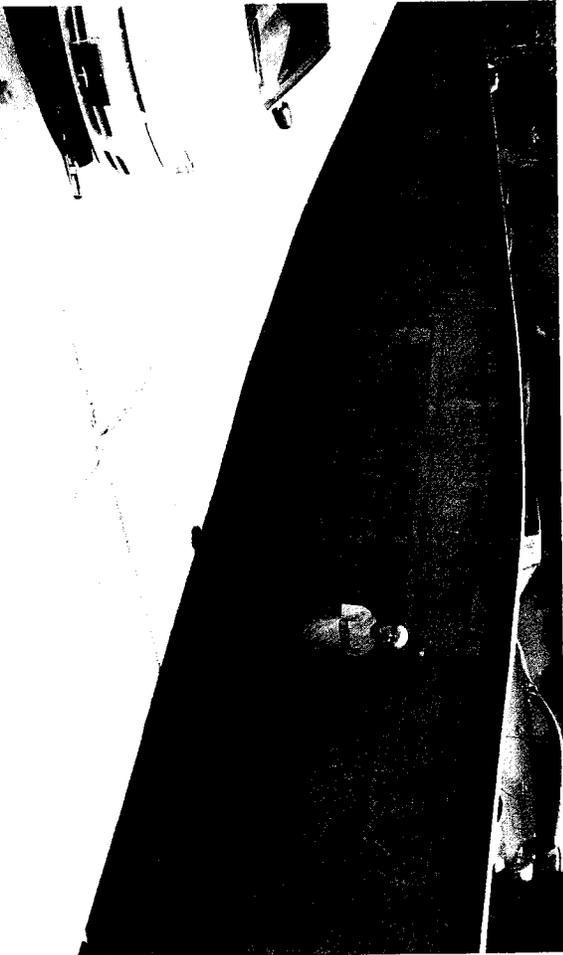
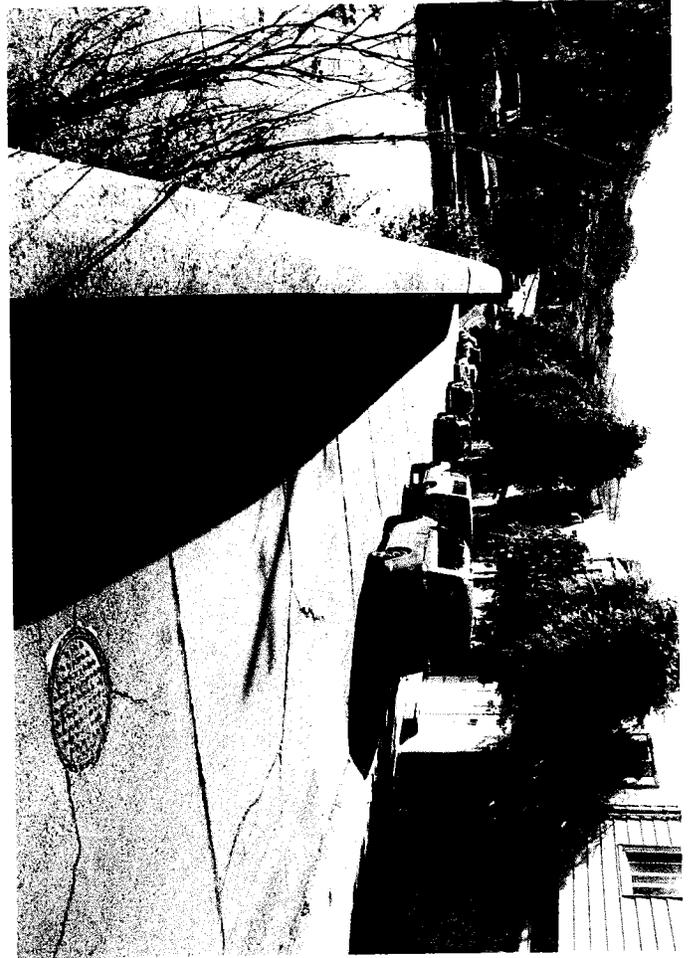
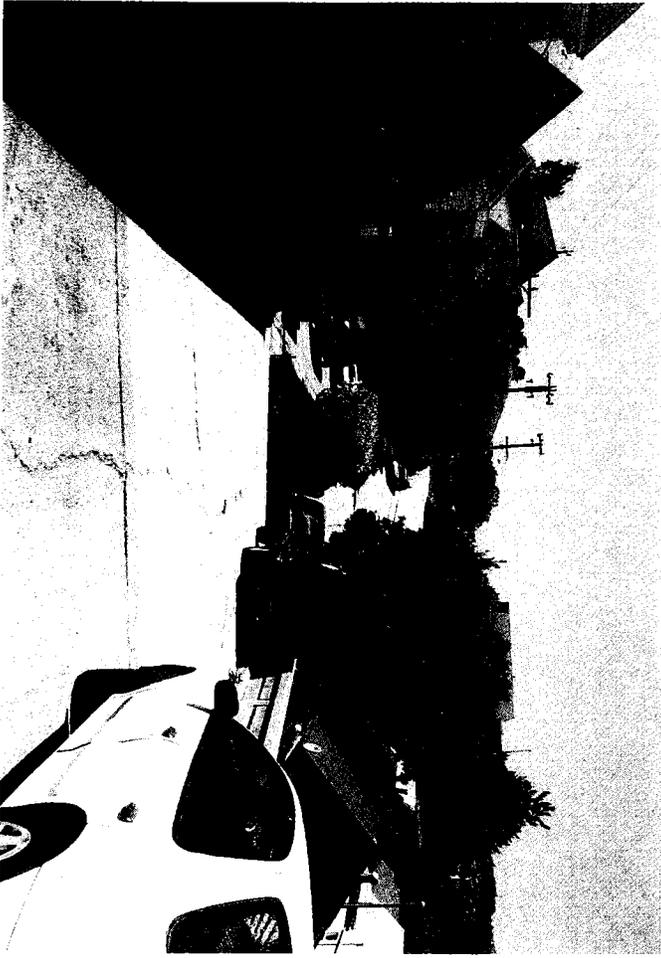
MODIFIED  
RDKimball  
11/11/10





54-56 Peralta Ave.

(23A)



View West to East  
on Peralta Ave

From Hampshire to  
Holladay

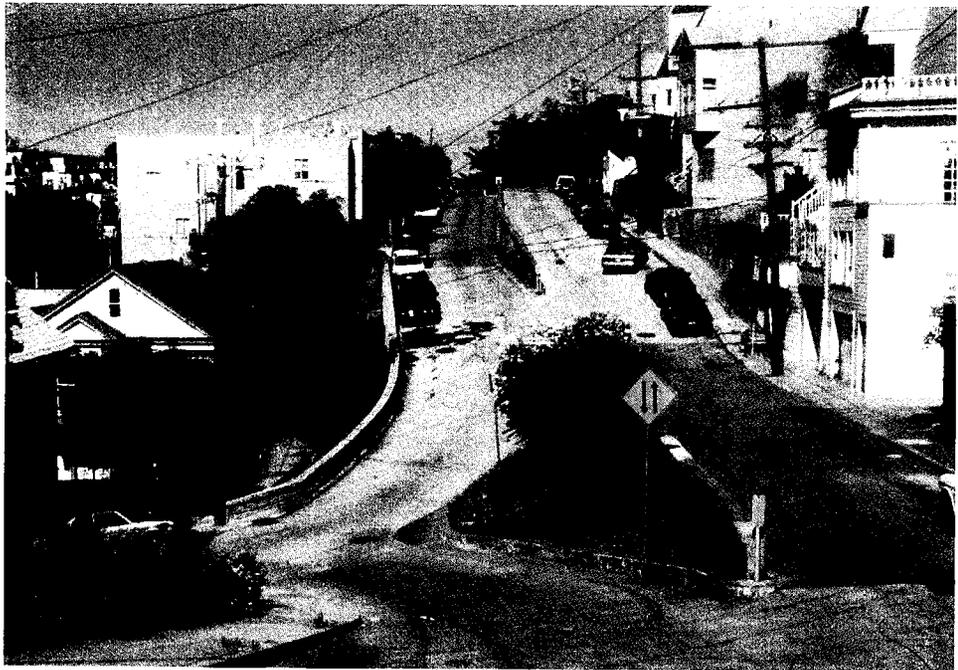
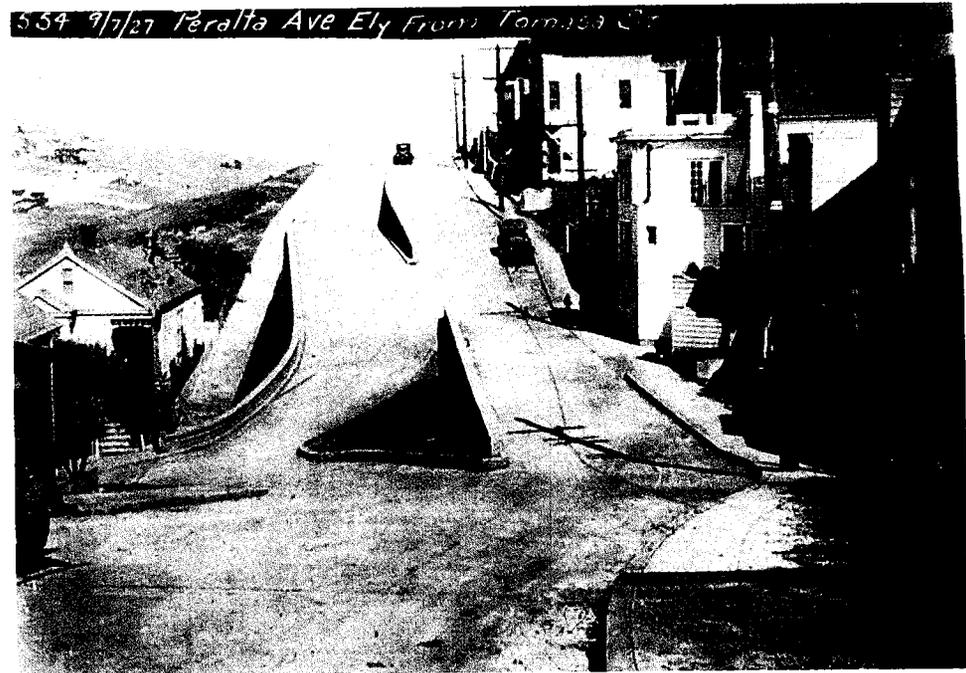
View East to West on  
Peralta Ave.

From Holladay to Hampshire

Peralta Ave Retaining  
Wall at Street Center  
in front of 54-62  
Peralta

Peralta Retaining Walls  
in front of 54-62  
Peralta

54-54 Peralta Ave.  
(24A)



HISTORICAL  
VIEW WEST TO EAST  
ON PERALTA FROM HAMPSHIRE  
TO HOLLADAY SHOWING NEW  
RETAINING WALLS.

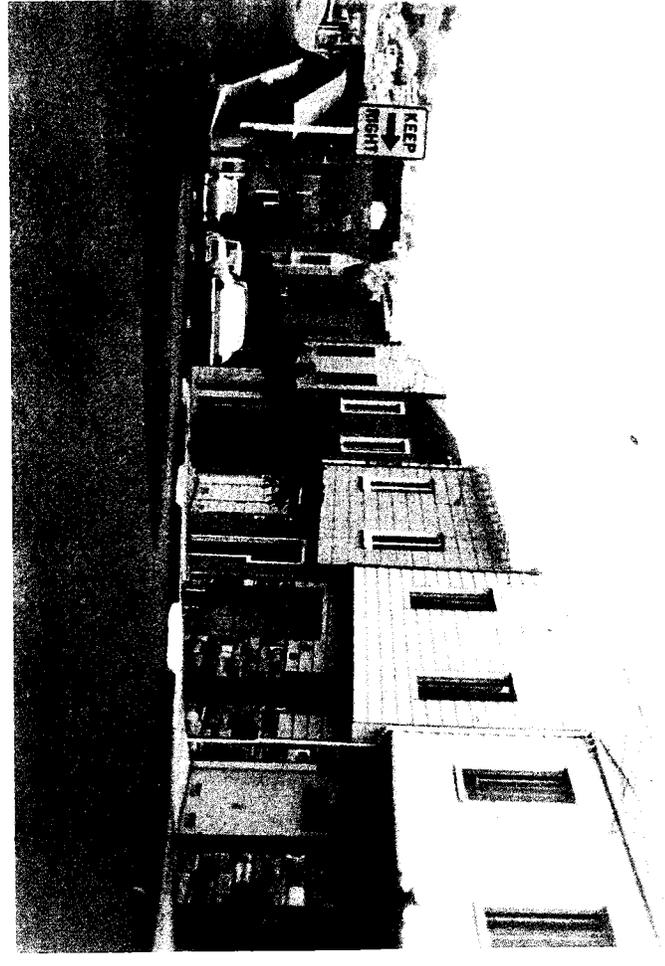
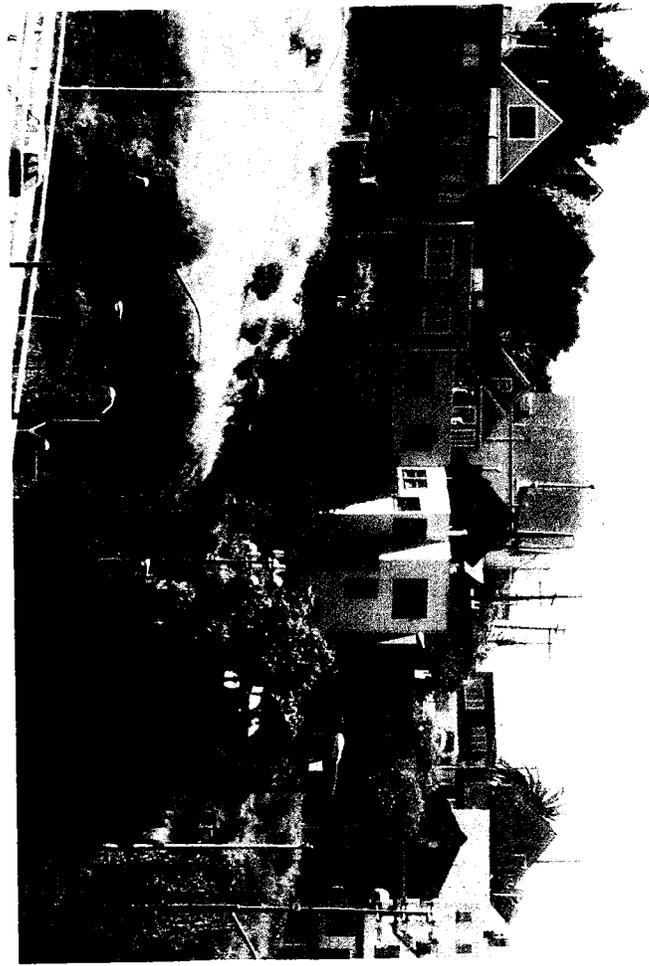
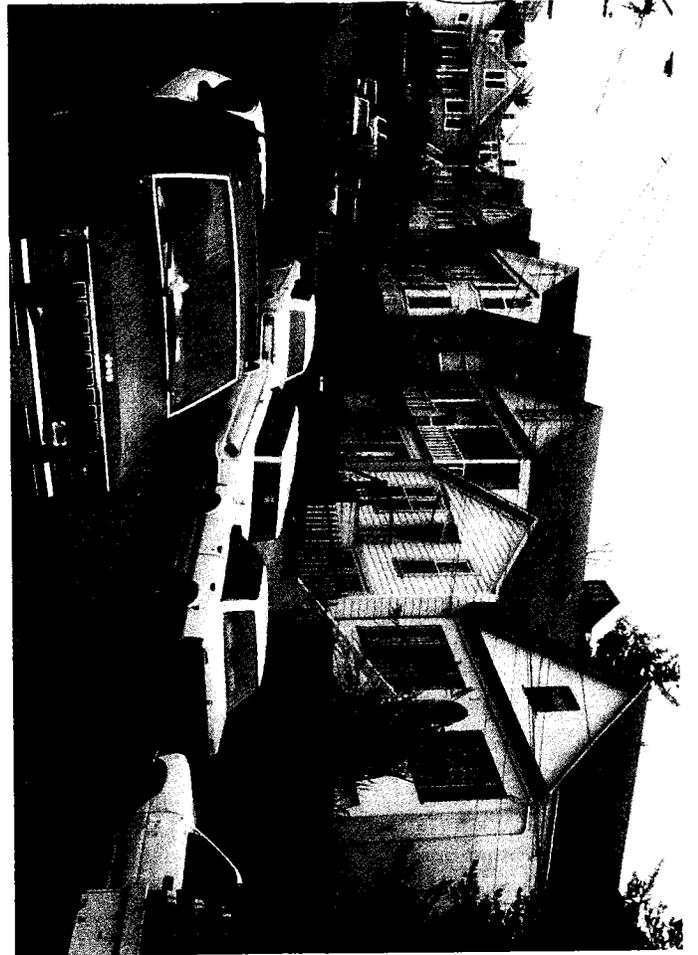
VIEW EAST TO WEST  
ON PERALTA SHOWING  
CENTER RETAINING WALL  
AND FACADES ON SOUTH  
SIDE.

HISTORICAL  
VIEW WEST TO EAST  
ON PERALTA AVENUE  
FROM HAMPSHIRE TO HOLLADAY

CURRENT VIEW  
WEST TO EAST  
ON PERALTA AVE.  
FROM HAMPSHIRE TO HOLLADAY

54-56 Peralta Ave.

25A



HISTORICAL

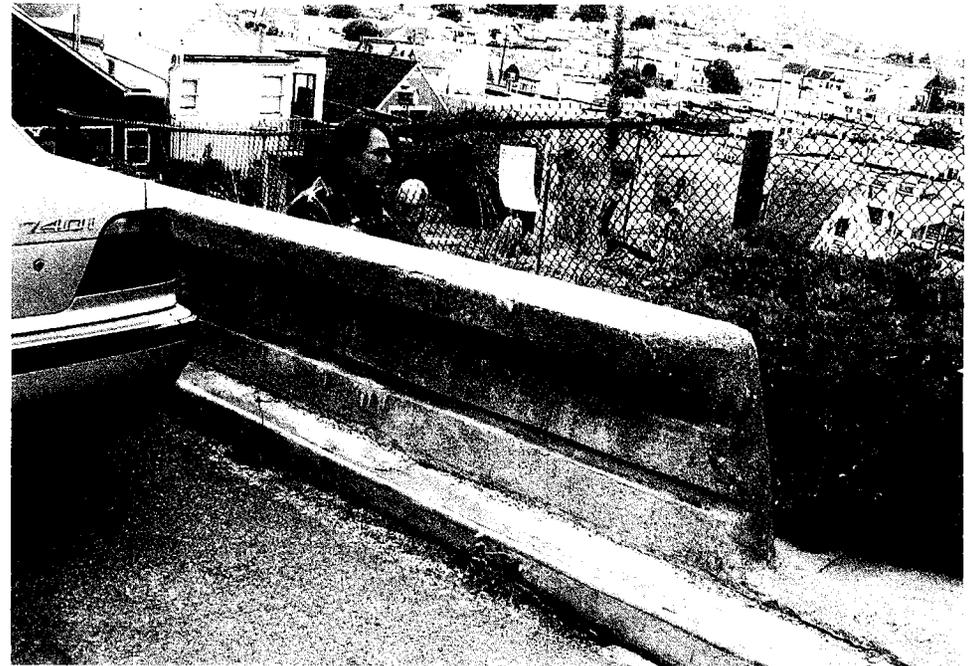
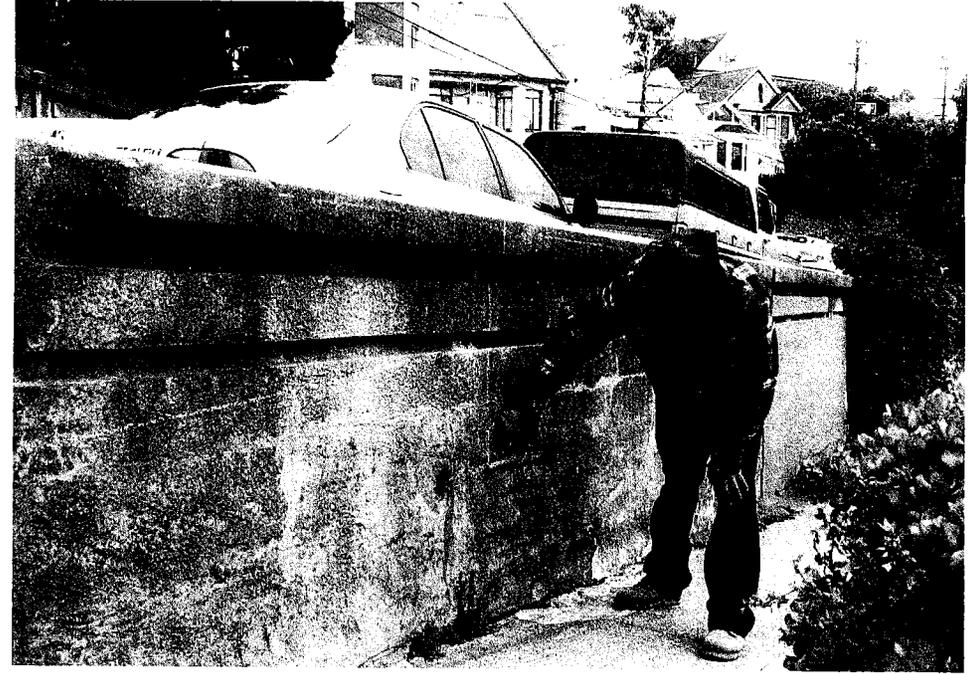
VIEW EAST TO WEST  
ON PERALTA AVENUE  
SHOWING CENTER RETAINING  
WALL AND STEPPED DOWN  
ROOF LINE ON NORTH SIDE.

VIEW NORTH TO SOUTH  
ON HAMPSHIRE STREET  
(WEST SIDE) FROM  
CESAR CHAVEZ TO PERALTA  
SHOWING STEPPED DOWN  
ROOF LINE.

VIEW SOUTH FROM POTRERO  
STREET SHOWING BACK  
OF PERALTA AVE. HOMES  
WITH STEPPED DOWN ROOF  
LINE.

VIEW NORTH TO SOUTH  
ON HAMPSHIRE STREET  
(EAST SIDE) FROM CESAR  
CHAVEZ TO PERALTA AVE.  
SHOWING STEPPED DOWN  
ROOF LINE.

54-56 Peralta Ave.  
26A



SIDEWALK IN FRONT OF  
54-62 PERALTA AVE.  
SHOWING SIDEWALK  
GRADE INCREASE AND  
RETAINING WALL/CURB CUT.

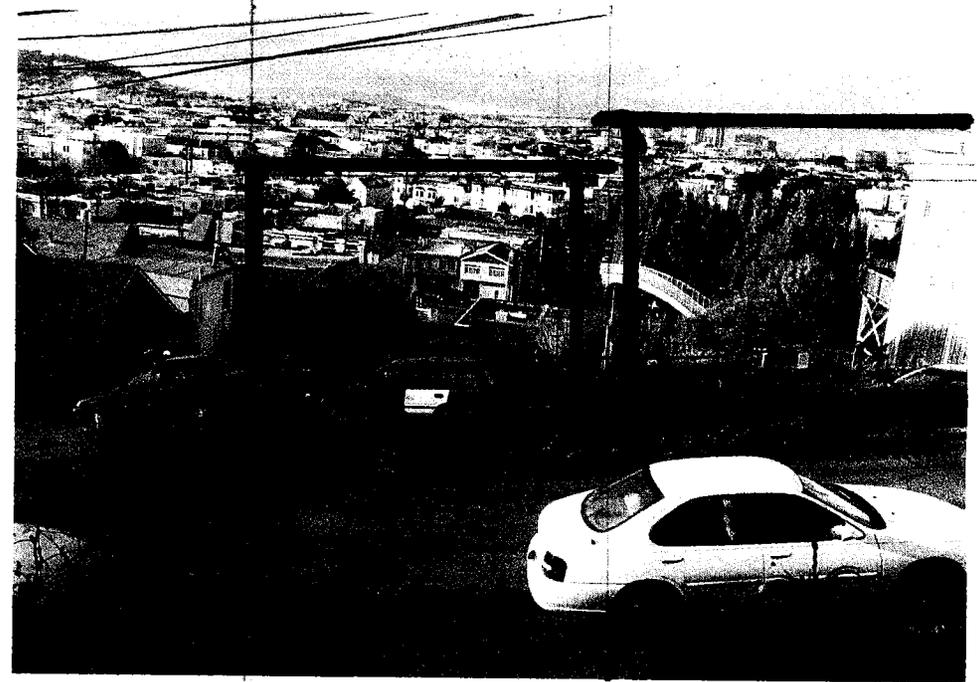
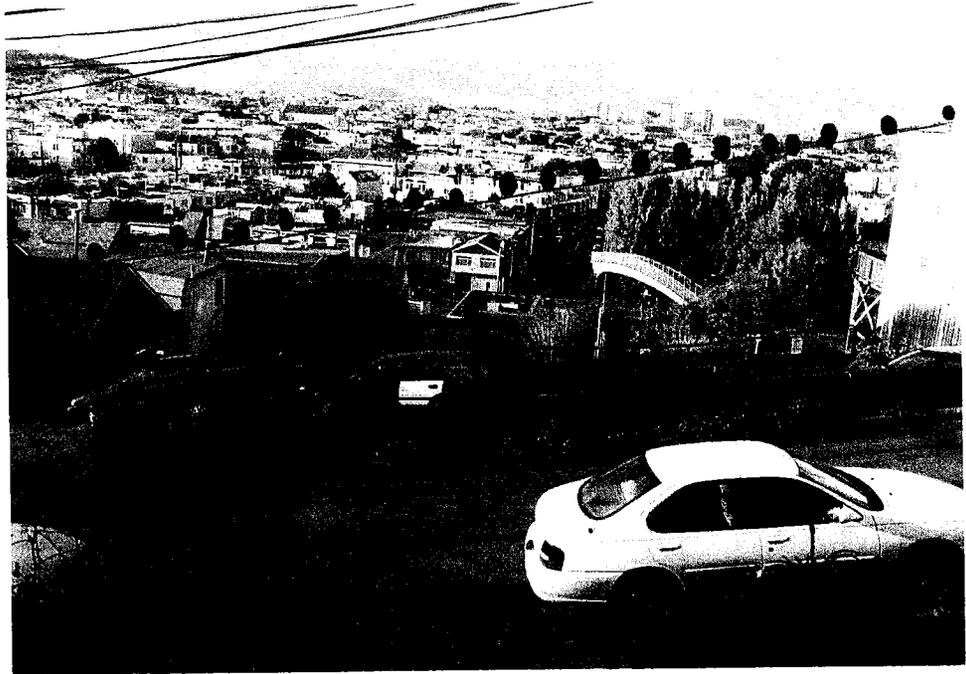
SIDEWALK IN FRONT OF  
54-62 PERALTA AVENUE  
SHOWING PROPOSED GRADE  
INCREASE AT PROPOSED  
RETAINING WALL AND CURB  
CUT + REMOVAL .

SIDEWALK IN FRONT OF  
54-62 PERALTA. PEDESTRIAN  
AT PROPOSED RETAINING WALL  
CUT, SHOWING PARTIAL  
OBSCURING OF PEDESTRIANS

SIDEWALK IN FRONT OF  
54-62 PERALTA AVENUE  
AT PROPOSED RETAINING  
WALL/CURB CUT.

54-56 Peracca Ave.

27A



VIEW FROM 23 PERALTA AVE.  
LOOKING NORTH WEST SHOWING  
48 PERALTA NEXT TO OPEN LOTS  
OF PROPOSED 54-62 PERALTA.  
PEAKS OF HAMPSHIRE STREET  
ROOFS VISABLE JUST ABOVE  
RETAINING WALL.

VIEW FROM 41 PERALTA  
LOOKING NORTH WEST, SHOWING  
STEEP DOWNHILL GRADE IN  
FRONT OF PROPOSED 54-62  
PERALTA.

VIEW FROM 41 PERALTA  
AVE LOOKING NORTH AT  
OPEN LOT OF PROPOSED 54-62  
PERALTA. PLEASE NOTE  
DOTTED LINE CONSISTENT  
WITH EXISTING STEPPED  
ROOF LINE.

VIEW FROM 41 PERALTA  
AVE. LOOKING NORTH AT  
OPEN LOTS. PLEASE NOTE  
APPROXIMATE FACADE OUTLINE  
OF PROPOSED 54-62 PERALTA  
AVE IN BLACK LINE.

CASE NUMBER:  
 For Staff Use only

# APPLICATION FOR Discretionary Review Application

## 1. Owner/Applicant Information

DR APPLICANT'S NAME: <b>Mr. Bob Besso</b>		
DR APPLICANT'S ADDRESS: <b>1569 Hampshire Street, San Francisco, California</b>	ZIP CODE: <b>94110</b>	TELEPHONE: <b>415 550-1985</b>
PROPERTY OWNER WHO IS DOING THE PROJECT ON WHICH YOU ARE REQUESTING DISCRETIONARY REVIEW NAME: <b>Tom Aquilina</b>		
ADDRESS: <b>1856 17th Avenue, San Francisco, CA</b>	ZIP CODE: <b>94122</b>	TELEPHONE: <b>(415) Unlisted</b> <b>415-640-0589</b>
CONTACT FOR DR APPLICATION: Same as Above <input checked="" type="checkbox"/>		
ADDRESS:	ZIP CODE:	TELEPHONE: ( )
E-MAIL ADDRESS:		

## 2. Location and Classification

STREET ADDRESS OF PROJECT: <b>60 - 62 Peralta Avenue, San Francisco, California</b>		ZIP CODE: <b>94110</b>
CROSS STREETS: <b>Holladay Avenue and Hampshire Street</b>		
ASSESSORS BLOCK/LOT: <b>5512 / 031</b>	LOT DIMENSIONS: <b>30' X 75'</b>	LOT AREA (SQ FT): <b>2220</b>
ZONING DISTRICT: <b>RH2</b>	HEIGHT/BULK DISTRICT: <b>40X</b>	

## 3. Project Description

Please check all that apply

Change of Use  Change of Hours  New Construction  Alterations  Demolition  Other

**Additions to Building:** Rear  Front  Height  Side Yard

Present or Previous Use: **Vacant - Existing neighborhood art installation.**

Proposed Use: **2 unit residential**

Building Permit Application No. **2009.12.31.4050**

Date Filed: **12-31-09**

**RECEIVED**

NOV 13 2010  
 CITY & COUNTY OF S.F.  
 DEPT. OF CITY PLANNING  
 PIC

10.03670  
~~10.10730~~

4. Actions Prior to a Discretionary Review Request

Prior Action	YES	NO
Have you discussed this project with the permit applicant?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Did you discuss the project with the Planning Department permit review planner?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Did you participate in outside mediation on this case?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

5. Changes Made to the Project as a Result of Mediation

If you have discussed the project with the applicant, planning staff or gone through mediation, please summarize the result, including any changes there were made to the proposed project.

Please note: We contacted Community Boards, however they replied we are not a candidate for mediation since the owner and developer dropped out of communication with the Bernal Heights East Slope Design Review Board upon receipt of their letter dated June 5, 2010. Kindly see Review Board letter in our Application documentation. Thank you.

~~10-10730~~

#1 What are the reasons for requesting Discretionary Review? The project meets the minimum standards of the Planning Code. What are the exceptional and extraordinary circumstances that justify Discretionary Review of the project? How does the project conflict with the City's General Plan or the Planning Code's Priority Policies or Residential Design Guidelines? Please be specific and site specific sections of the Residential Design Guidelines.

There are several exceptional and extraordinary circumstances that justify Discretionary Review of this project. These circumstances are as follows:

- Size and scope of project out of character with the neighborhood
- Insufficient design information for significant sidewalk grade change
- Safety and flooding concerns regarding removal of retaining wall for driveway/garage
- Inadequate and inoperable design for off street parking
- Suggested landscaping or set back to soften facade omitted from design
- Lack of cooperation or communications from owner and developer

#### **Size and Scope of Project out of Character with the Neighborhood**

The Bernal Heights Special Use District under Sec. 242 Letter (e) Number 6 under **Design** states: **"Requests for Planning Commission review shall be governed by Subsection 311(d) of this Code. In addition to applicable guidelines cited by Section 311, the Elsie Street Plan and the East Slope Building Guidelines shall be used as guidelines to determine neighborhood compatibility of new construction and alterations in the respective areas covered by those guidelines."**

As concerned and neighboring residents we have also found that the plan as proposed does not follow the BERNAL HEIGHTS SPECIAL USE DISTRICT Sec. 242 (b) under **Purposes** it states:

**"In order to reflect the special characteristics and hillside topography of an area of the City that has a collection of older buildings situated on lots generally smaller than the lot patterns in other low-density areas of the City, and to encourage development in context and scale with the established character, there shall be a Bernal Heights Special Use District."** The units as proposed are in reality a four unit apartment complex on two lots with less than required parking located in only one of the buildings.

Another set of specific areas of contention is contained within Section 4 of the East Slope Design Review Guidelines that, under the title **Building Bulk and Architectural Massing** states:

#### **INTENT**

**"Our objectives in the Building Bulk Guidelines are neatly summed up in already existing City Planning Policies. To quote from the Urban Design Element of the Master Plan, our intent is to:**

1. Minimize the blockage of sun from adjacent downhill properties;
2. Lower the first level of occupancy to a level enabling ready access to rear yard open space;
3. Deter the possibilities of visually dominant buildings with blank and uninteresting exteriors which do not relate well to surrounding development;
4. Promote harmony in the visual relationships and transitions between new and older buildings; and,
5. Encourage the Construction of buildings which meet the ground and reflect the slope of the hill."

The plan, as submitted by the Developer, fails to meet guideline intent items 1, 3, 4, & 5.

As members of the Far Northeast Bernal Heights Neighborhood Block Club, which represents affected neighbors, we feel this proposed development will irreparably upset the fabric & character of our neighborhood (Please see pages 6A-11A).

#### Insufficient Design Information for Significant Sidewalk Grade Change

The plans show a sidewalk grade increase of nearly 4% from 16.6% to 20% (Please see page 21A). This slope increase is to create a level entry into the proposed underground parking area. This increase in sidewalk slope creates a hardship for many of the older residents in our neighborhood and without adequate information or dialog with the Developer and Owner we are concerned about the feasibility and safety of this design.

#### Safety and Flooding Concerns Regarding Removal of Retaining Wall for Driveway/Garage

Of special concern is the proposed removal of the curb and retaining wall in front of this development, which currently channels rainwater down Peralta Avenue and prevents the flooding of residences on Hampshire Street. Peralta Avenue becomes Peralta Creek during a rainstorm. The placement and size of the street curbs and retaining walls is to accommodate ground stability which requires controlling water runoff (Please see historical photos page 24A). With the retaining wall breached at this point, and a ten foot section of it removed, rainwater will flow down the sidewalk, causing erosion and safety concerns for the residences downhill.

#### Inadequate and Inoperable Design for Off Street Parking

The Developer is seeking a Parking Variance. The parking plan as proposed doesn't meet the requirements of the Bernal Heights Special Use District under Sec. 242 Letter (e) Number 4 nor does it meet the minimum standards of the Planning Code. The square footage as submitted to the city requires 7 off street parking spaces. The sub-grade parking garage design shows four parking spaces facilitated by the use of an electric parking pallet. The plans shows the pallet accommodating four vehicles, shifting them to and from the steep driveway ramp. Upon consultation with the manufacturer, we found that the parking pallet shown in the submitted plans "Klaus PQ -AC/PQ-DC", will not accommodate vehicles as the plans suggests, due to the wheel safety stop, and non-beveled edge, at one end of the pallet (Please see pages 12A & 13A). As a result, this specific parking pallet, can only accommodate vehicles from one direction, rendering 2 of the 4 spaces unserviceable by the pallet. Even if the parking pallet was operable for 4 vehicles, the parking allocation would not be acceptable to the neighbors nor was it accepted by the East Slope Design Review Board in the letter dated June 5, 2010 (Please see pages 4A & 5A) that was sent to the Developer which stated: **The Board feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project's reliance on neighborhood parking. The Board recognizes that off-street parking cannot be provided at 60-62 Peralta and the difficulty in providing additional off-street parking within 54-56 Peralta, however, we feel that the parking deficit is more a reflection of the size of the buildings and not site access.**

The affected neighbors are completely against granting a parking variance for this project and are in agreement with the Design Review Board and its guidelines.

#### Suggested Landscaping or set back to Soften Façade Omitted from Design

Both buildings in this proposed project are set back the minimum amount allowed. This leaves little room for the landscaping needed to soften the edges of the building, add texture, and create intimacy. We have noted that there are trees placed on the plans for the front of these structures.

In actuality, there is insufficient room for such plantings to be installed and retain the required space for pedestrian foot traffic, rendering these plantings unacceptable (Please see page 17A).

**Lack of Cooperation or Communications from Developer and Owner**

The Project as submitted is not consistent with the guidelines developed by the Bernal Heights East Slope Design Review Board. This volunteer body functioning for more than 20 years is an important representative group that works to insure design & new home construction in Bernal Heights remains consistent with the character of the area of Bernal Heights under its purview as recognized by the Planning Code.

In a letter dated June 5, 2010 from the East Slope Design Review Board to the Developer of this project: SIA Consulting Corporation (Please see page 4A) it stated that: **"The Board regrets to inform you that we cannot recommend that the Department of City Planning approve this project as proposed. The Board also cannot recommend approval of the parking variances that are required to construct the project as currently planned. The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines."**

The East Slope Design Review Board presented this letter dated June 5, 2010 as part of their role to facilitate a continuing communication between the Developer and the Owner with the affected neighbors. The letter as noted cannot recommend the project to the Planning Department. When the Developer and Owner received this letter they chose to end the communication process with the affected neighbors and with the Design Review Board itself and go straight ahead to submitting for a Building Permit.

#2. The Residential Design Guidelines assume some impacts to be reasonable and expected as part of construction. Please explain how this project would cause unreasonable impacts. If you believe your property, the property of others or the neighborhood would be adversely affected, please state who would be affected, and how:

This project, as currently proposed, would cause unreasonable and adverse impacts to our neighborhood in several ways. These impacts are as follows:

- Dangerous vehicular entries and exits across the sidewalk and street.
- Towering buildings blocking light and eliminating privacy
- Removal of retaining wall and curb causing a water runoff hazard
- Unrealistic parking plan that will increase demand on street parking

**Dangerous Vehicular Entries and Exits across Existing Sidewalk**

One of the homes most directly affected by this portion of the proposed project is 48 Peralta, owned since 1969 by Lucy Gomez, who is 85 years old. The greatly increased vehicular traffic across the sidewalk (not more than 4 feet from her property line) presents an extraordinary public safety hazard, especially to people, like Lucy, who are advanced in years, and to children whom are not as aware of hazards. The vehicles making use of the subterranean parking spaces, as designed in the plan, will be accessing the driveway from an angle partially obscured by the sidewalk retaining wall. Vehicle drivers exiting the garage, backing up a 20 degree plus ramp (Please see page 14A), will be crossing the sidewalk and entering the street essentially blind to existing pedestrian and traffic conditions. Many neighborhood residents use this sidewalk to get to public transportation, walk their dogs, and transport their children in strollers. The crossing of this sidewalk by drivers backing up a steep ramp with obscured vision is a recipe for disaster. Additionally, the blind street access is further complicated by this block's high retaining wall that separates East and West bound traffic. This block of Peralta is used by

many Upper Bernal Heights neighbors coming down the hill to Cesar Chavez, and they notoriously take advantage of the downhill slope and one way traffic to "step on it" as they head west on Peralta.

### **Towering Buildings Blocking Light and Eliminating Privacy**

Lucy Gomez, at 48 Peralta will also lose all sunlight on the western side of her residence due to the exterior facades of the east side of the proposed project. The light from all seven windows of 48 Peralta will be blocked by the windowless, unarticulated and towering exterior wall of 54-56 Peralta (Please see pages 17A & 22A).

The second group of properties that are directly affected by the negative aspects of this proposed project are the homes on the East side of Hampshire Street, adjacent to the western side of the lots. The plans as proposed, do not include a adequate stepping down of the rooftops, which is inconsistent with the surrounding homes and hillside (Please see pages 25A & 27A). The overwhelming western facade of this towering project will obliterate nearly all sunlight and privacy for the abutting homes located on Hampshire Street. On it's western face, this inadequately stepped down project towers some 40 to 50 feet over the back yards of 1569, 1563, and 1557 Hampshire Street. These homes on Hampshire Street will have their daylight diminished to that of a narrow canyon, with neighbors peering down into their homes (Please see pages 18A, 19A, 20A).

### **Removal of Retaining Wall Causing Water Runoff into Adjacent Properties**

The retaining walls that line this portion of Peralta are there for structural and other reasons, and they also keep rain water flowing towards street drains and away from family residences. This proposed retaining wall cut (Please see page 26A), and removal of a ten foot section of curb and wall on the declining side of Peralta, would dramatically change the path and velocity of surface water runoff. Channeling surface water to the sidewalk at the proposed curb cut would create a torrent of water on the steep downhill, towards Hampshire Street homes during winter rain months. This cut would cause public access and safety concerns as the surface water would not find a street drain until the bottom of the hill at Cesar Chavez Street.

### **Unrealistic Parking Plan that will Increase "On Street" Parking**

The presented plan for parking vehicles (they are asking for variance from seven) in the 54-56 Peralta location is unrealistic. Entry into the property will involve several back and forth driving maneuvers due to the center street retaining wall (Please see page 23A), to line up with the garage opening. This retaining wall already causes reliance on street parking for those residents with restricted or no off street parking options. To gain access to either of the proposed projects' two lots, the developer will have to remove a minimum of one existing street parking space to accommodate any driveway. Furthermore, because of the unusual and complicated parking scheme as submitted by the developer, we believe that few of the proposed off-street parking places will be used for parking, but rather for storage or other purposes, further increasing the burden on the neighborhood. More significant, if this parking plan is approved and is attempted to be used as designed, we are seriously concerned for the neighborhood's health and safety. The planned garage is below street grade, inconsistent with the neighborhood buildings, and necessitates a steep driveway for access to and from the building. The plans show that all vehicles will back up this steep ramp, blindly crossing the sidewalk and into the street before the driver has any reasonable vision of existing pedestrian or vehicle circumstances. We see this as a significant safety concern for pedestrians, especially children and the elderly.

#3. What alternatives or changes to the proposed project, beyond the changes (if any) already made would respond to the exceptional and extraordinary circumstances and reduce the adverse effects noted above in question #1?

**The alternatives that we suggest to the project are as follows:**

**Neighborhood Context:**

**Reduce the height, mass & density of the proposed development**, to respect the neighborhood context (Please see page 25A), and preserve its character, as well as conform with the Bernal Heights East Slope Design Review Board Guidelines as outlined in its letter to the developer, dated June 5, 2010. The residents on Peralta, Hampshire and Holladay are against upsetting the character of our neighborhood with the construction of what is essentially a giant four-unit apartment building in our midst. Given solutions to other problems that we identified with this project, we find that the construction of a total of one home on each lot is in keeping with the letter and intent of the Special Use District as well as The Board's Guidelines.

If any structure is to be built on the property at 60-62 Peralta, which is directly adjacent to lots on Hampshire Street, it should be stepped down in conformity with the hill and set back from the sidewalk so as not to completely obliterate all light and privacy on the adjoining properties. Our homes are single-family residences, some having a small in-law unit at ground level behind their front stairs.

**Parking:**

**Provide off-street parking for all vehicles in compliance with Guidelines:**

In its letter to the developer dated June 5, 2010:

**“The Boards feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project’s reliance on neighborhood parking. The Board recognizes that off-street parking cannot be provided at 60-62 Peralta and the difficulty in providing additional off-street parking within 54-56 Peralta, however, we feel that the parking deficit is more a reflection of the size of the buildings and not site access.”**

Grade level parking (no steep driveway) for all vehicles in compliance with Bernal Heights Special Use District under Sec. 242 Letter (e) Number 4, in a building with a significant set back from the sidewalk, would greatly reduce our public safety concerns.

**Cooperation/Communication with Developer and Owner**

The Far North East Bernal Heights Block Club would welcome a continued dialog with the Developer and the Owner to create a realistic design for two single-family dwellings on these two adjacent lots.

~~10.10730~~

**REQUIRE A PUBLIC WORKS PUBLIC HEARING OF PROPOSED CHANGES TO THE PUBLIC RIGHT-OF-WAY ADJACENT TO THE PROPERTY**

A Public Works Hearing is mentioned in the Board's letter to the developer, dated June 5, 2010.

**Public right-of-way issues yet to be addressed:**

- **Steep Sidewalk:** The Bernal Heights Design Review Board suggests the developer provide stairs instead of a steeper slope to the sidewalk in front of this development. We still have many questions as we feel this would not accommodate either wheelchairs or strollers and would still make it more difficult for our many senior residents to transverse our neighborhood.
- **Retaining Wall and Curb Removal & Flooding:** As stated in question #1 we are especially concerned about the removal of the curb and retaining wall in front of this development's proposed driveway access. This curb and wall currently channels rainwater down Peralta Avenue. Peralta Avenue is banked to the North curbside as it starts a steep decent right in front of the planned development (see Historical photos attached). With the retaining wall and curb breached, rainwater coming down that entire block of Peralta would be channeled by the slope to the sidewalk in front of the development and onto the sidewalk toward Hampshire Street, causing health and safety concerns to the residences below.
- **Sidewalk Right of Way, Building Set-Back and Landscaping:** The proposed buildings are set back a minimal amount essentially leaving room for only the sidewalk. However, the plans as submitted, indicate trees in the sidewalk which would impede the public right of way. As stated in The Board's letter, "**Should this type of greenery not be provided then we request that the set-backs be increased to allow for front yards on both lots.**" Any structures on these lots should be set-back further from the sidewalk to assure public right of way and safety, as well as to accommodate landscaping.

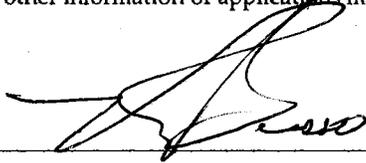
60-62 Penalta Ave. (9)

# Applicant's Affidavit

Under penalty of perjury the following declarations are made:

- a: The undersigned is the owner or authorized agent of the owner of this property.
- b: The information presented is true and correct to the best of my knowledge.
- c: The other information or applications may be required.

Signature: \_\_\_\_\_



Date: \_\_\_\_\_

11-11-10

Robert Besso owner

Print name, and indicate whether owner, or authorized agent:

\_\_\_\_\_  
Owner / Authorized Agent (circle one)

10.1073D

Application for Discretionary Review	
CASE NUMBER: For Staff Use only	

# Discretionary Review Application Submittal Checklist

Applications submitted to the Planning Department must be accompanied by this checklist and all required materials. The checklist is to be completed and signed by the applicant or authorized agent.

REQUIRED MATERIALS (please check correct column)	DR APPLICATION
Application, with all blanks completed	<input type="checkbox"/>
Address labels (original), if applicable	<input type="radio"/>
Address labels (copy of the above), if applicable	<input type="radio"/>
Photocopy of this completed application	<input type="checkbox"/>
Photographs that illustrate your concerns	<input type="checkbox"/>
Covenant or Deed Restrictions	<input type="checkbox"/>
Check payable to Planning Dept.	<input type="checkbox"/>
Letter of authorization for agent	<input type="checkbox"/>
Other: Section Plan, Detail drawings (i.e. windows, door entries, trim), Specifications (for cleaning, repair, etc.) and/or Product cut sheets for new elements (i.e. windows, doors)	<input type="checkbox"/>

- NOTES:
- Required Material.
  - Optional Material.
  - Two sets of original labels and one copy of addresses of adjacent property owners and owners of property across street.

For Department Use Only

Application received by Planning Department:

By: \_\_\_\_\_ Date: \_\_\_\_\_

10-10733

1A



Lewis Randal  
2 Peralta  
San Francisco, CA 94110  
#5512 / #3A

Paul Phillips  
3 Peralta  
San Francisco, CA 94110  
#5531 / #1

Chizuru Fujita  
5 Peralta  
San Francisco, CA 94110  
#5531 / #19

Jarl Passero  
Peralta  
San Francisco, CA 94110  
#5512 / #3B

Thomas Dorte  
14 Peralta  
San Francisco, CA 94110  
#5512 / #3C

Natasha Tuck  
15 Peralta  
San Francisco, CA 94110  
#5531 / #18

Chris Ho & Kirsten Irgens-Moller  
17 Peralta  
San Francisco, CA 94110  
#5531 / #17

Patrick Leonie  
20 Peralta  
San Francisco, CA 94110  
#5512 / #3D

Clare Wren  
23 Peralta  
San Francisco, CA 94110  
#5531 / #10

Ruth and Edmond Farias  
26 Peralta  
San Francisco, CA 94110  
#5512 / #3E

Philip and Mary Alotis  
27 Peralta  
San Francisco, CA 94110  
#5531 / #15

Philip and Mary Alotis  
29 Peralta  
San Francisco, CA 94110  
#5531 / #15

Seth Jacobson  
32 Peralta  
San Francisco, CA 94110  
#5512 / #3F

Seth Jacobson  
401 Roland Way #220  
Oakland, CA 94621  
#5512 / #3F

Michael Keeth  
35 Peralta  
San Francisco, CA 94110  
#5531 / #14

Frank and Monica Wallace  
38 Peralta  
San Francisco, CA 94110  
#5512 / #3G

Vivian Najarro  
41 Peralta  
San Francisco, CA 94110  
#5531 / #13

Vivian Najarro  
1235 Mc Alister St #112  
San Francisco, CA 94115  
#5531 / #13

Max Kirkberg  
44 Peralta  
San Francisco, CA 94110  
#5512 / #4

Lucy Gomez  
48 Peralta  
San Francisco, CA 94110  
#5512 / #5A

Rochelle Kimball  
51 Peralta  
San Francisco, CA 94110  
#5530 / #1

Jose Gutierrez  
53 Peralta  
San Francisco, CA 94110  
#5530 / #10

Harry Lampiris  
59 Peralta  
San Francisco, CA 94110  
#5530 / #15

Stephan Tyler  
247 28th Street  
San Francisco, CA 94131  
#5530 / #15

Christa Dibiase  
65 Peralta  
San Francisco, CA 94110  
#5530 / #14

Amy Darius  
71 Peralta  
San Francisco, CA 94110  
#5530 / #13

Elizabeth Pittinos  
75 Peralta  
San Francisco, CA 94110  
#5530 / #12

Teresa Kennett  
76 Peralta  
San Francisco, CA 94110  
#5513 / #10

Jason Porter  
77 Peralta  
San Francisco, CA 94110  
#5530 / #12

Suzanne Skull  
78 Peralta  
San Francisco, CA 94110  
#5513 / #11A



~~10-10730~~

(2A)



Saeed Mirfattah  
30 Peralta  
San Francisco, CA 94110  
#5513 / #11B

Michael Perry  
82 Peralta  
San Francisco, CA 94110  
#5513 / #11C

Richard and Kathleen Fine  
84 Peralta  
San Francisco, CA 94110  
#5513 / #12A

David Krakower  
86 Peralta  
San Francisco, CA 94110  
#5513 / #12B

Charles Turner  
88 Peralta  
San Francisco, CA 94110  
#5513 / #13

Douglas Haijsman  
90 Peralta  
San Francisco, CA 94110  
#5513 / #14

Frank Camp  
92 Peralta  
San Francisco, CA 94110  
#5513 / #15

Robert Besso  
709 York Street  
San Francisco, CA 94110  
#5512 / #8

Yuko Murakami  
1528 Hampshire  
San Francisco, CA 94110  
#5513 / #3

Baily Smith  
1529 Hampshire  
San Francisco, CA 94110  
#5512 / #5

Fred Hampton  
1534 Hampshire  
San Francisco, CA 94110  
#5513 / #4

Frank Scrivini  
1535 Hampshire  
San Francisco, CA 94110  
#5512 / #14

Margo Freistadt  
1540 Hampshire  
San Francisco, CA 94110  
#5513 / #5

Rudy Concepcion  
1541 Hampshire  
San Francisco, CA 94110  
#5512 / #13

Brad Drain  
1545 Hampshire  
San Francisco, CA 94110  
#5512 / #30

Yukiko Tominaga  
1546 Hampshire  
San Francisco, CA 94110  
#5513 / #6

R Hernandez  
1550 Hampshire  
San Francisco, CA 94110  
#5513 / #7

Keith Willson  
1552 Hampshire  
San Francisco, CA 94110  
#5513 / #7

Paul Espanoza  
1555 Hampshire  
San Francisco, CA 94110  
#5512 / #32

Erik Bourget  
1556 Hampshire  
San Francisco, CA 94110  
#5513 / #8

Norman and Sharon Kaman  
1563 Hampshire  
San Francisco, CA 94110  
#5512 / #9

Mark Lowe  
1566 Hampshire  
San Francisco, CA 94110  
#5513 / #9

Mark Lowe  
30 Grandview Ave. #303  
San Francisco, CA 94114  
#5513 / #9

Mike McCabe  
1569 Hampshire  
San Francisco, CA 94110  
#5512 / #8

Teresa Kennett  
1570 Hampshire  
San Francisco, CA 94110  
#5513 / #10

Reza Khoshenvisan  
1256 Howard Street  
San Francisco, CA 94103  
#5512 / #29 #31

10.0367D  
10.1073D



(3A)



# SAN FRANCISCO PLANNING DEPARTMENT

1650 Mission Street Suite 400 San Francisco, CA 94103

## NOTICE OF BUILDING PERMIT APPLICATION (SECTION 311)

On December 31, 2009, the Applicant named below filed Building Permit Application No. 2009.12.31.4050 and 2009.12.31.4052 (New Construction of Two Buildings) with the City and County of San Francisco.

CONTACT INFORMATION		PROJECT SITE INFORMATION	
Applicant:	Reza Khoshnevisan	Project Addresses:	54-56 and 60-62 Peralta Ave
Address:	1256 Howard Street	Cross Streets:	Holladay Ave and Hampshire St
City, State:	San Francisco, CA 94103	Assessor's Block /Lot No.:	5512/029 and 031
Telephone:	(415) 922-0200	Zoning Districts:	RH-2 / 40-X

Under San Francisco Planning Code Section 311, you, as a property owner or resident within 150 feet of this proposed project, are being notified of this Building Permit Application. You are not obligated to take any action. For more information regarding the proposed work, or to express concerns about the project, please contact the Applicant above or the Planner named below as soon as possible. If your concerns are unresolved, you can request the Planning Commission to use its discretionary powers to review this application at a public hearing. Applications requesting a Discretionary Review hearing must be filed during the 30-day review period, prior to the close of business on the Expiration Date shown below, or the next business day if that date is on a week-end or a legal holiday. If no Requests for Discretionary Review are filed, this project will be approved by the Planning Department after the Expiration Date.

PROJECT SCOPE			
<input type="checkbox"/> DEMOLITION	and/or	<input checked="" type="checkbox"/> NEW CONSTRUCTION	or <input type="checkbox"/> ALTERATION
<input type="checkbox"/> VERTICAL EXTENSION		<input type="checkbox"/> CHANGE # OF DWELLING UNITS	<input type="checkbox"/> FACADE ALTERATION(S)
<input type="checkbox"/> HORIZ. EXTENSION (FRONT)		<input type="checkbox"/> HORIZ. EXTENSION (SIDE)	<input type="checkbox"/> HORIZ. EXTENSION (REAR)

PROJECT FEATURES (FOR EACH BLDG)	PROPOSED CONDITIONS	
	54-56 Peralta Ave	60-62 Peralta Ave
FRONT SETBACK.....	+/- 2 feet	+/- 2 feet
FRONT SETBACK (at Top Floor).....	+/- 7 feet	+/- 7 feet
SIDE SETBACK.....	+/- 4 feet (southern side)	+/- 4 feet (southern side)
BUILDING DEPTH (Lowest Floor).....	+/- 54 feet 6 inches	+/- 41 feet
REAR YARD.....	+/- 44 feet 7 inches	+/- 33 feet 4 inches
HEIGHT OF BUILDING (from front curb).....	+/- 29 feet	+/- 29 feet
NUMBER OF STORIES (front).....	3	3
NUMBER OF STORIES (rear).....	3 plus Crawlspace	4
NUMBER OF PARKING SPACES.....	4 (2 provided for 60-62 Peralta Ave)	0 (2 provided at 60-62 Peralta Ave)
NUMBER OF DWELLING UNITS.....	2	2

### PROJECT DESCRIPTION

The proposal is to construct two new two-family homes on down-sloping adjacent lots (currently vacant), resulting in a total of four new dwelling units. The 52-54 Peralta Ave building will contain four off-street parking spaces, of which two will be reserved for the units in the proposed adjacent building at 60-62 Peralta Ave, resulting in one off-street parking space for each of the four new units. Providing only four parking spaces for the two buildings requires a variance from Planning Code Section 242(e). The parking variance for this project is *tentatively* scheduled to be heard by the Zoning Administrator on **September 22, 2010**. Separate notice will be issued for the variance hearing.

PLANNER'S NAME: Corey Teague  
 PHONE NUMBER: (415) 575-9081  
 EMAIL: corey.teague@sfgov.org

DATE OF THIS NOTICE: 10-13-10

EXPIRATION DATE: 10-12-10



**Bernal Heights**  
**East Slope Design Review Board**

Terry Milne, external secretary • 321 Rutledge • San Francisco 94110 • [285-8978]

June 5, 2010

SIA Consulting Corporation  
1256 Howard Street  
San Francisco, CA 94103

Re: 54 – 62 Peralta Avenue

Dear Applicant,

The Bernal Heights East Slope Design Review Board held a neighborhood meeting on Monday April 19, 2010 to review your revised plans for a project that proposes to construct a two-unit residential building on each of two adjoining lots that are currently vacant. The April meeting was a follow-up to comments from the Board that was discussed at our previous meetings on July 28 and August 11, 2009.

The Board regrets to inform you that we cannot recommend that the Department of City Planning approve this project as proposed. The Board also cannot recommend approval of the parking variances that are required to construct the project as currently planned. The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines.

The Board's primary concern about your project is your request for a variance for three parking spaces. The drawings indicate that the total area of 54 – 56 Peralta Avenue will be 2,342 s.f., which requires three off-street parking spaces pursuant to Section 241 of the Planning Code. The drawings also indicate that the total area of 60 – 62 Peralta Avenue will be 3,245 s.f., which requires four off-street parking spaces. You have proposed a total of four off-street parking spaces, with two of the spaces deeded to 60 – 62 Peralta, where a total of seven spaces are required. The Board feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project's reliance on neighborhood parking. The Board recognizes that off-street parking cannot be provided at 60 – 62 Peralta and the difficulty in providing additional off-street parking within 54 – 56 Peralta, however, we feel that the parking deficit is more a reflection of the size of the buildings and not site access.

The Board also has concerns about the scale of 60 – 62 Peralta at the street wall. The top floor of 60 – 62 Peralta is set back 7' from the front wall and the building looms over the rear yard of the adjacent building to the west on Hampshire Street. With the minimal front setback the top floor would be clearly visible from the street thus interrupting the stepping of the rooflines at the street wall. To address this situation the Board suggests setting the top floor back a minimum of 15' from the front wall and setting it back 5' from the west side property line.

You indicated that your project would necessitate changes to the public right-of-way adjacent to the property. Your plans indicate that the sidewalk slope would become steeper but the full scope of the proposed changes is unclear. We suggest providing a call-out detail of the sidewalk changes with an existing and proposed transverse section because it is an important element of your project that needs to be accurately understood by all who

5A

review your project. The sidewalk at this location is fairly steep and we would prefer that stairs be added instead of a steeper slope. Furthermore, we want to know if a Public Works hearing will be required for the changes.

Both buildings are set back a minimal amount leaving little room for landscaping. The Board suggests adding planter/window boxes to the façade where appropriate to make up for the lack of landscaping. This type of greenery will help soften the edges of the building, add texture, and create intimacy.

Neighbors in attendance at the meeting expressed similar concerns to those of the Board and have drafted a letter describing their concerns (see attached). The Board strongly urges you to reconsider these aspects of your project and we welcome another meeting if you are interested in resolving these issues.

The Board wishes to thank you for presenting the plans to the neighborhood. Since the Board is not a City agency, it does not have the power to either approve or disapprove the permit application.

Cordially,

Jeff Saydah, Chair, Bernal ESDRB



May 18, 2010

Dear members of the Bernal Heights East Slope Design Review Board,

This is a letter from the neighbors most directly affected by the proposed project at 56-60 Peralta Ave.

We appreciate the time that the Bernal Heights Design Review Board has taken with this proposed development. As you know, this project has caused quite a bit of concern in the neighborhood given the scale of the development proposed & the severe, negative impact it will cause to many residents living on Peralta, Hampshire, Holladay and other adjacent streets. This particular neighborhood & surrounding blocks have a long history of single family homes & we find it entirely out of character to be faced with a 4-unit development on such precarious lots with so many negative impacts to the surrounding neighbors.

Our main areas of concern:

**Parking:** The design for the 4-car garage is completely unrealistic as much as the developer would have us believe otherwise. They are asking for a 3-car variance in an area without a lot of options for parking. They will be removing a street parking space with the construction of the garage. This is potentially putting 8 or more cars on our street & that is just too much given the already overburdened vehicle density homeowners are experiencing. Also, we are completely against granting any sort of parking variance for any number of vehicles. They need to come up with a realistic plan for how they will deal with 7 parking spaces without the use of any kind of parking variance.

**Roof Heights:** The planset presented at the latest meeting shows building heights that are not consistent with the existing slope of the neighboring houses.

**Sidewalk Slope:** Several neighbors have expressed concern that the developer is proposing to create a steeper sloped sidewalk in order to accommodate an already suspicious looking garage design. This is a serious issue as we have many longtime aging residents in the neighborhood who walk & this would put them at risk. As well, this may be against ADA requirements as regards new construction.

**Backyard & Daylight Plane:** Adjacent neighbor's backyard & daylight plane are being completely obscured by the height of the proposed building.

This is just too much development for such small lots. These lots if built upon should not be overbuilt just so the current owner & developer can make large profits at the expense of the neighborhood, area homeowners & local building regulations. Why not build a total of two houses on the two lots? - that would be more in following with the existing houses in the neighborhood.

We respect your commitment and experience in helping to represent Bernal Residents deal with new construction. In this case we see a developer driving the process to build

with only profits in mind without regard for the current architecture or residents. Their plans & presentations have tried to paper over the fact that they are trying to build too much on too small of a space. We are all completely against this proposal as it has been presented & would like to see your support in our efforts to block this proposal.

We are not however completely against building on this space. A lot owner should be allowed to build if they follow city ordinances & neighborhood guidelines. Two lots, two houses. Any more than that given the constraints of the lots & parking is just unrealistic & would burden this neighborhood significantly.

We would like to have this letter submitted to the city as part of the Bernal Heights East Slope Design Review Board materials. Thank you for your attention to this matter.

Below are the signatures we have collected from neighbors most directly affected by this project & who agree with the contents of this letter.

CC: SF Planning Department

Signed,

Address:

CLARE H. WREN

23 PERALTA AVE SF CA 94110

Raul A. Mickelson

29 Peralta S.F. CA.

Philip A. G. [unclear]

27 PERALTA

Carol [unclear]

41 PERALTA

Margie [unclear]

41 Peralta SF, CA 94110

Ang Pader

41 Peralta SF, CA 94110

[unclear]

65 PERALTA SF-CA 94110

Wim [unclear]

35 Peralta S.F. CA

[unclear]

35 Peralta Ave.

Sudhi Sunil

18 Peralta St. SF. CA

2 w/ signatures

SA

\_\_\_\_\_

\_\_\_\_\_

Abriel Pro

44 Peralta

Joan Kearney

1569 Hampshire St.

Rochelle Kimball

51 PERALTA AVE.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Signed,

Address:

K. Inge-Moller

17 Peralta Ave SF 94110

Lewis C. Randall

2 Peralta Ave SF 94110

Chujik

5 PERALTA AVE 94110

April O

109 HOLLADAY AVE APT B

Carl M. Passer

8 PERALTA AVE.

Jim Pat

20 Peralta Ave

Max C Kirkley

44 Peralta Ave

Robert Brine

44 Peralta Ave

Ranelli Maddy

44 PERALTA

Lucy Gomez

48 Peralta

Dorian Gomez

48 Peralta

Sara Gilroy

1569 Hampshire St

Diane Barton. Pin

1563 Hampshire St.

Leah Sheldon

1557 Hampshire St.

Saeed Mirfattel

80 Peralta Ave.

Dick Fine

84 Peralta ! !

[Signature]

17 Peralta N.

[Signature] zia  
DeSantis

76 Peralta Ave.

[Signature] Nick  
DeSantis

76 Peralta Ave

[Signature] LUCINDA CAKE

120 Holladay Ave.

Signature:

Address:

Thomas Duarte

14 Peralta Ave SF

[Signature]

14 Peralta Ave SF

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

10-21-2010

## Far Northeast Bernal Heights Neighborhood Block Club

Re: Proposed Development at 54-62 Peralta Ave

Our neighborhood and Block Club met with the developer SIA Consulting Corporation, and the property owner, through a series of meetings convened by the Bernal Heights East Slope Design Review Board. Following our last meeting with the developer, the Review Board issued letter to the developer, dated June, 5, 2010, listing a number of issues that needed to be addressed prior to a recommendation of the project to the planning department. Among other things, the Review Board letter states "The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines". And, after identifying similar neighborhood concerns, "The Board strongly urges you to reconsider these aspects of your project and we welcome another meeting if you are interested in resolving these issues".

Unfortunately rather than coming back to the neighborhood Review Board with revised plans for a follow-up neighborhood meeting, the developer ignored the recommendations of the Board and proceeded to file for a parking variance. This variance filling by the developer and owner shows complete disregard of the neighborhoods concerns and the Bernal Heights East Slope Design Review Board.

### Far Northeast Bernal Heights Neighborhood Context:

**Neighborhood Consistency:** Both the East Slope Design Review Board and our Block Club asked this developer to scale back the size of this multi-unit development to bring it in line with the fabric and character of our neighborhood. Many of our homes are Victorian in age, single family residences, some of which have a small, in-law rental unit at ground level. The Review Board letter describes the top floor of 60 – 62 as looming over the rear yard of the adjacent building to the west on Hampshire Street and interrupting the stepping of the rooflines at the street wall. "To address this situation the Board suggests setting the top floor back a minimum of 15' from the front wall and setting it back 5' from the west side of the property line." We've all seen & experienced the egregious consequences of large scale developments amid a fabric of single-family homes in other parts of our city and we value the architectural treasure of our Victorian middle class neighborhoods.

**Transportation:** Our neighborhood is on a very steep hillside, making access to what little public transportation that is available, difficult at best. We are not served directly by public transportation, and our nearest BART station is a half hour walk for a young person in good health. Owning a vehicle in this neighborhood is therefore a necessity for most, and parking is already difficult.

**Parking:** The developer is requesting a parking variance from the seven spaces required by our neighborhood guidelines and has provided only 4 spaces underground in one of the two buildings. Parking in 3 of these 4 spaces requires use of a sliding parking pallet. The pallet however will not do the job for these 3 spaces as it can be loaded/unloaded from only one direction. In addition, to quote the Review's letter, "The Board feels strongly that variances for new construction not be viewed lightly. The off-street parking requirement is an important provision for reducing your project's reliance on neighborhood parking." The Board further states, "...we feel that the parking deficit is more a reflection of the size of the buildings and not site access." In addition the development will remove a minimum of 10' of existing street parking with the addition of their garage driveway.

**Sidewalk Slope/Changes to Public Right-of-Way:** Our neighborhood's sidewalks already have a steep grade and this project proposes to create an even steeper slope on Peralta Avenue to build a level driveway access to the proposed underground garage. This is a change to the public right-of-way and a serious issue of great concern to our many longtime, aging residents in the neighborhood, some living here for more than forty years. A steeper sidewalk puts them at risk of injury. The Review Board states in their June 5<sup>th</sup> letter, "...the full scope of the proposed changes in unclear. We suggest providing a call-out detail of the sidewalk changes with an existing and proposed transverse section because it is an important element of your project that needs to be understood by all who review the project. The sidewalk at this location is very steep and we would prefer that stairs be added instead of a steeper slope. Furthermore, we want to know if a Public Works hearing will be required for the changes." Stairs, however



**Northeast Bernal Heights Neighborhood Block Club****2**

**Sidewalk Slope/Changes to Public Right-of-Way, (cont.):** are not an acceptable solution to the neighbors as these prevent public access by wheel chairs, baby strollers, etc..

**Flooding:** There are unresolved drainage issues for downslope neighbors that relate to removing a ten foot section of the retaining wall and curb along Peralta Avenue to accommodate the garage driveway of this proposed development. When it rains Peralta Avenue becomes Peralta Creek with a downhill torrent of rain water roaring down our steep street. The retaining wall on Peralta Avenue, in front of this proposed development, holds the water in the street and prevents it from flooding the downhill houses on Hampshire Street. It is of great concern that removing a ten foot section of this retaining wall and curb will result in flooding our downhill Victorians along the east side of Hampshire.

**Roof Heights & Stepping Rooflines:** The development's design as presented shows proposed building heights that are not consistent with the neighborhood, (they are higher than the uphill neighbor's house), with the existing slope & the elevations of the neighboring houses. The development is a monolithic structure not respecting the hillside with its stepping rooflines at both the street wall on Peralta Avenue and the Victorian houses stepping down Hampshire.

**Daylight Access:** The adjacent houses on both Hampshire and Peralta have their access to daylight over shadowed by the immensity of this development. The adjacent homes on Hampshire will have all morning light completely obliterated by building 60-62, which would loom over them by nearly 20 feet above roof peaks and 40 feet above back yards!

**Landscape:** The Review Board states, "Both buildings are set back a minimal amount leaving little room for landscaping." They recommend landscaping to, "...soften the edges of the buildings, add texture and create intimacy." Their suggestion is the addition of planter/window boxes to the facade where appropriate to make-up for the lack of landscaping. The developer's plans show trees planted in front of the development between the sidewalk and the street that are impossible. There is no space between the already narrow sidewalk and the street.

### **We are not opposed to any construction.**

Our main objection to this multi-family development is its size, and variance to parking requirements. More appropriate for the neighborhood context is a single family residences.

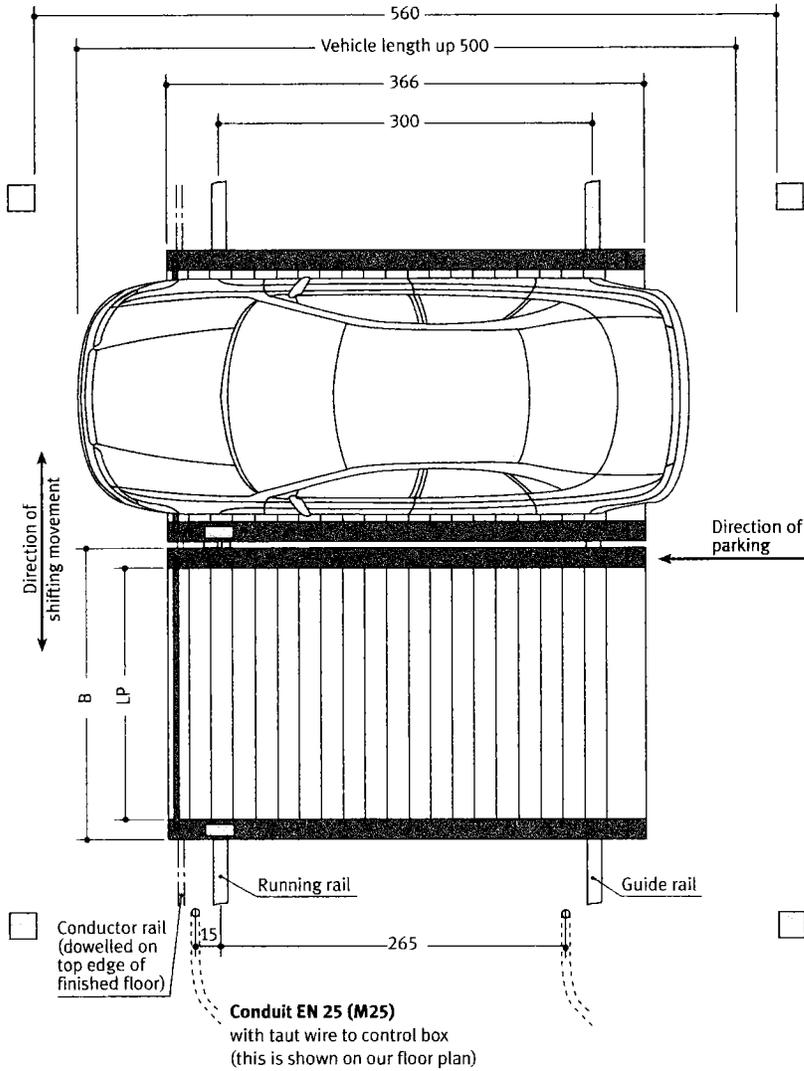
- The developer should be required to respect the neighborhood's character of single family homes with stepped rooflines. The installation of story poles would greatly help in our review of this proposal.
- The Developer should not be allowed to rely on neighborhood street parking, but provide off street parking that meets the Bernal Heights East Slope Design Review Board Guidelines. We believe that the proposed parking garage is inadequate, impractical and dangerous to pedestrians, as vehicle access and egress will be blind to the garage user. If a motorized pallet is allowed, the developer should be required to specify on his plans a pallet that actually performs in the space specified in the plans.
- The Developer needs to come up with a drainage plan that will address the runoff water that will impact the downhill neighbors due to the removal of a section of the retaining wall and curb. A Public Works hearing should be a requirement for this development to proceed.
- The Developer should be required to address a call-out detail of the sidewalk changes as discussed so these can be accurately understood. Neither stairs nor a steeper slope are acceptable to the neighbors.
- The Developer needs to amend their design so that the backyard and daylight access of the existing adjacent residences are not completely obscured with a huge building looming over them.



12A

- Page 1  
Dimensions  
Car data
- Page 2  
Dimensions  
Recess/  
Rail system
- Page 3  
Eveness/  
Tolerances  
Function
- Page 4  
Electrical  
installation  
Technical  
data
- Page 5  
To be performed by the customer  
Description

**Parking Pallet PQ-DC**  
(with current supply from rail)

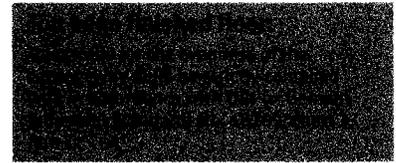


**Product Data**  
**Parking Pallet**  
**PQ-DC/PQ-AC**  
crosswise shifting



Parking pallet PQ-DC  
with current supply from rail  
Parking pallet PQ-AC  
with current supply over trailing  
cable

**Dimensions:**  
All space requirements are minimum  
finished dimensions. Tolerances for space  
requirements  $^{+3}_0$ . Dimensions in cm.



Type	LP	L	B
PQ-210	182	366	210
PQ-220	192	366	220
PQ-236*			

\* = Standard Type  
We generally recommend to use type  
PQ-DC-236 where possible.

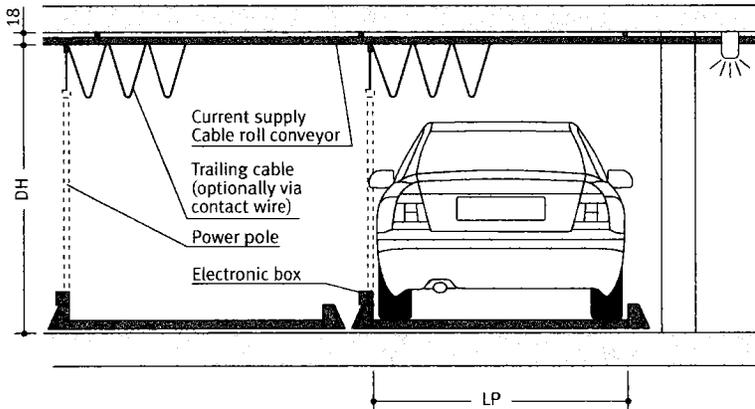
**Suitable for:**  
Standard passenger car and station wagon.

**Car dimensions**

<b>length</b>	<b>max. 5.00 m</b>
<b>width</b>	<b>max. 1.80 (PQ-210)</b> <b>max. 1.90 (PQ-220)</b> <b>max. 2.08 (PQ-236)</b>
<b>weight</b>	<b>max. 2000 kg</b>
<b>wheel load</b>	<b>max. 500 kg</b>

Standard passenger cars are vehicles  
without any sports options such as  
spoilers, low-profile tyres etc.

**Parking Pallet PQ-AC**  
(with current supply over trailing cable/optionally via contact wire)



**Notes**

A safety clearance of 30 cm must be maintained between the front or rear bumpers of vehicles on parking pallets and any fixed parts of the surroundings or other vehicles in accordance with DIN EN 14 010. At a max. vehicle length of 500 cm, this means a length dimension of 560 cm between the columns. The length dimension of 560 cm can only then be shortened if the max. vehicle or parking place length is reduced or light barriers are used.

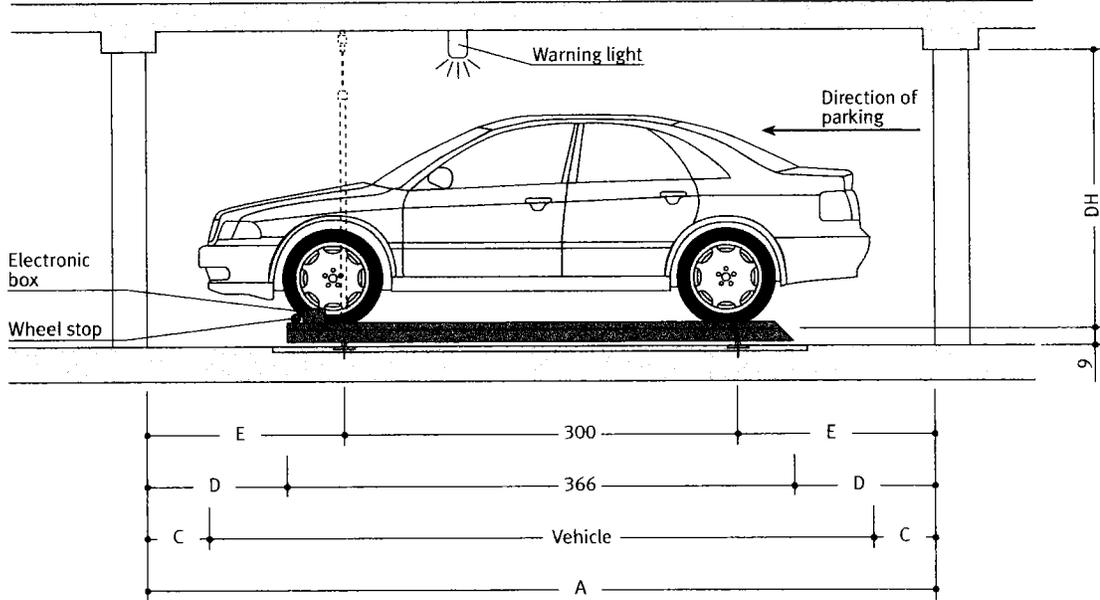
The operating console must be mounted in such a way that the operator can see the entire system during operation and the motion sequences can be observed and monitored.



**Klaus Multiparking GmbH**  
Hermann-Krum-Straße 2  
D-88319 Aitrach  
Phone **+49-75 65-5 08-0**  
Fax **+49-75 65-5 08-88**  
E-Mail **info@multiparking.com**  
Internet **www.multiparking.com**

13A

- Page 1  
Dimensions  
Car data
- Page 2  
Dimensions  
Recess/  
Rail system
- Page 3  
Eveness/  
Tolerances  
Function
- Page 4  
Electrical  
installation  
Technical  
data
- Page 5  
To be per-  
formed by the  
customer  
Description



A	Vehicle	C	D	E	Please note the following on parking space	DH
560	500	30	97	130	Parking space and pallet conform to German regulations and DIN EN 14010	acc. to local requirements
530	500	15	82	115	Where the unit is equipped with lighth barriers, parking space and pallet also conform to the above	acc. to local requirements
<530	<500	15	<82	<115	Light barrier is essential. Parking space does not conform to German regulations. <b>Note that length of vehicle is restricted!</b>	acc. to local requirements

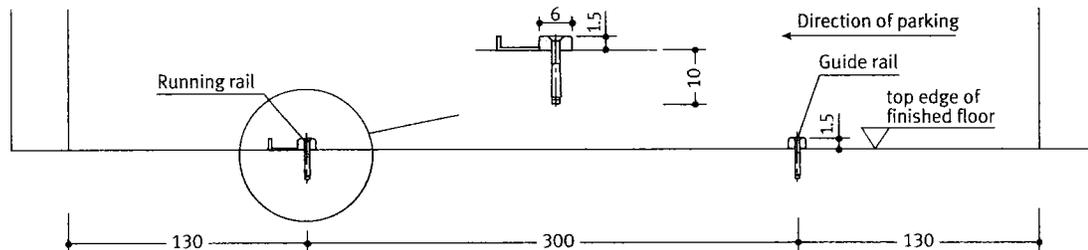
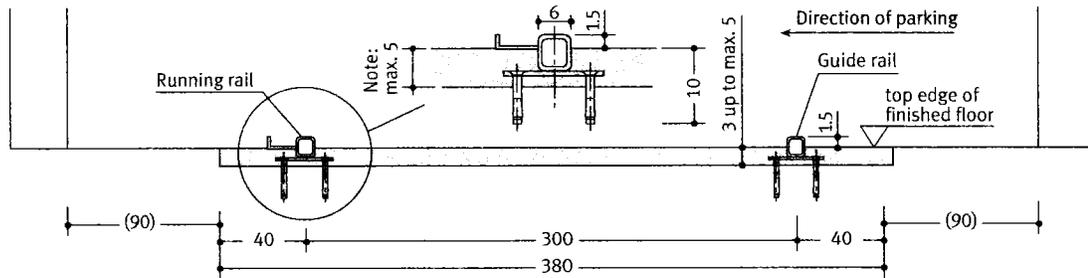
Dependent upon the structural conditions of the garage, several different options are available for installation of the rails.

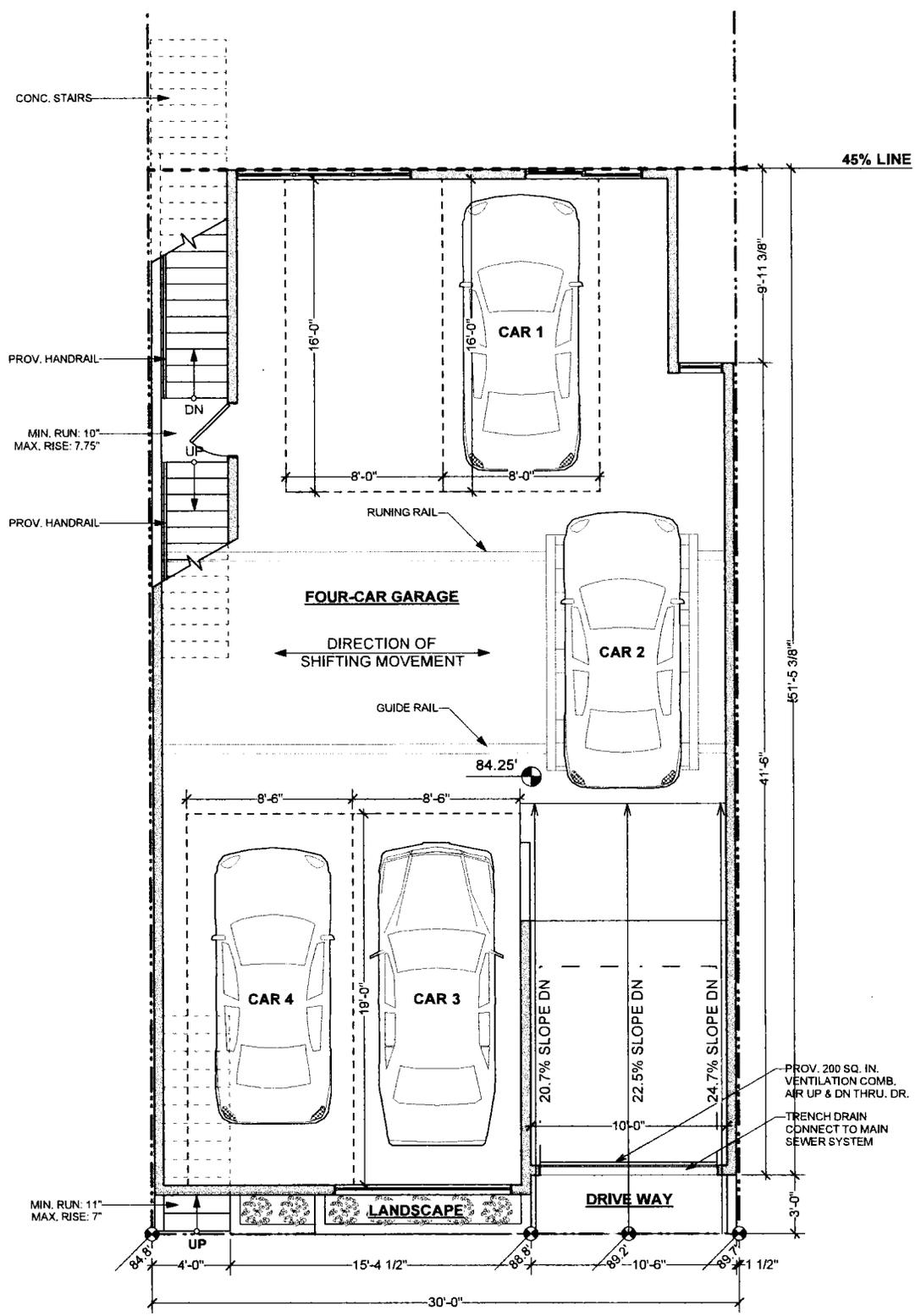
When executing the carriageway, according to the raw bottom floor combined with a cement screed, attention must be paid to the regulation that the thickness of the floor materials is determined by the structurally admissible tolerances. The set-up of the rails amounts to 3 cm (height of floor screed 4 cm).

Another variant consists in that recesses in the finished carriageway for the rails are provided by customer. After the rails have eventually been laid, the area under the rails, as well as the recesses must be topped up with concrete by the customer. When exact evenness of the carriageway has successfully been accomplished, the rails may subsequently also be dowelled onto it.

Detailed plans for correct recessing of rails are available from the local agency of Klaus.

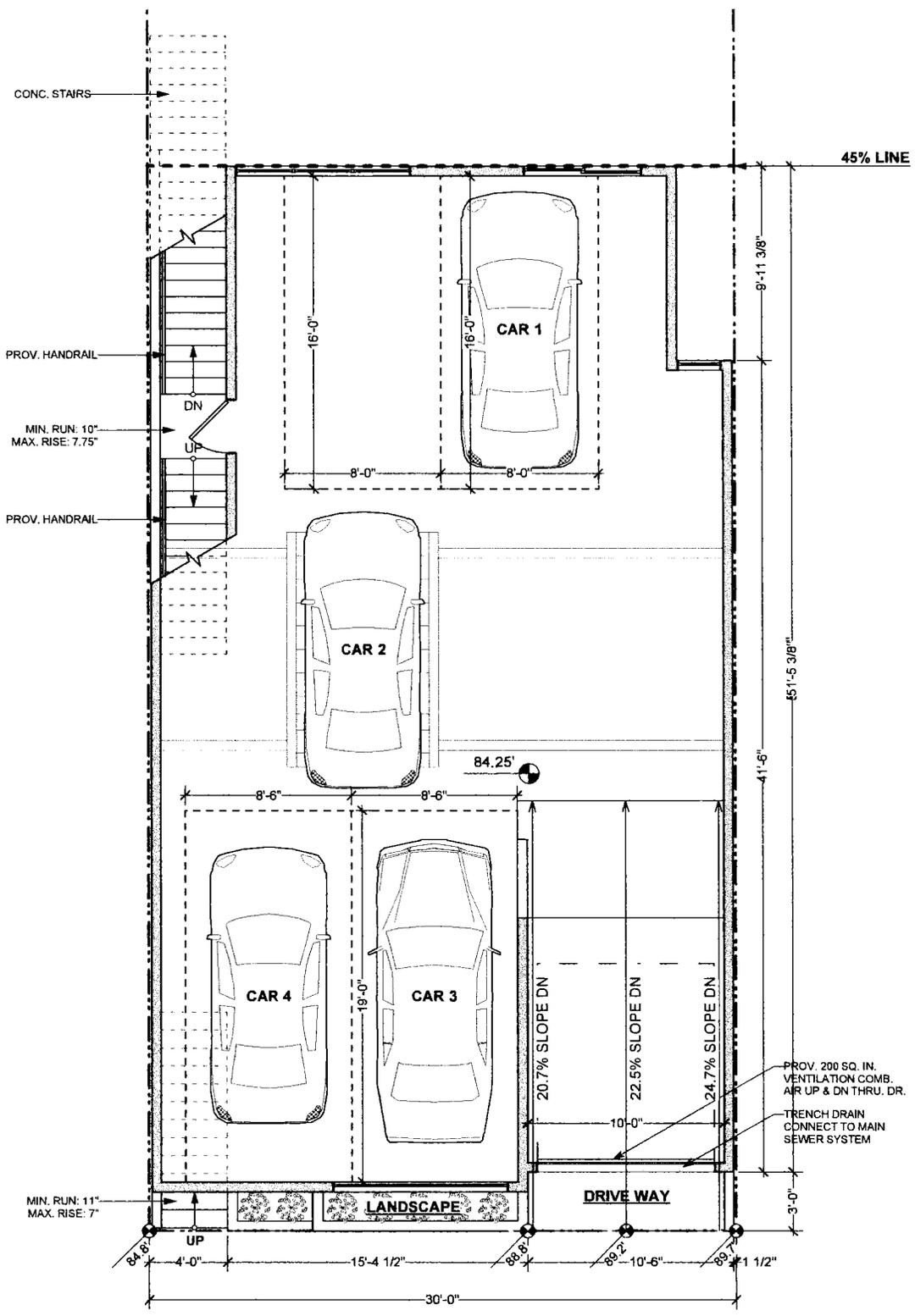
**Note:** Tolerances for the evenness of the carriageway must be strictly complied with in accordance with DIN (= German Industrial Standard) No. 18202, chart 3, line 3.





54-62 PERALTA AVE.  
SAN FRANCISCO, CA

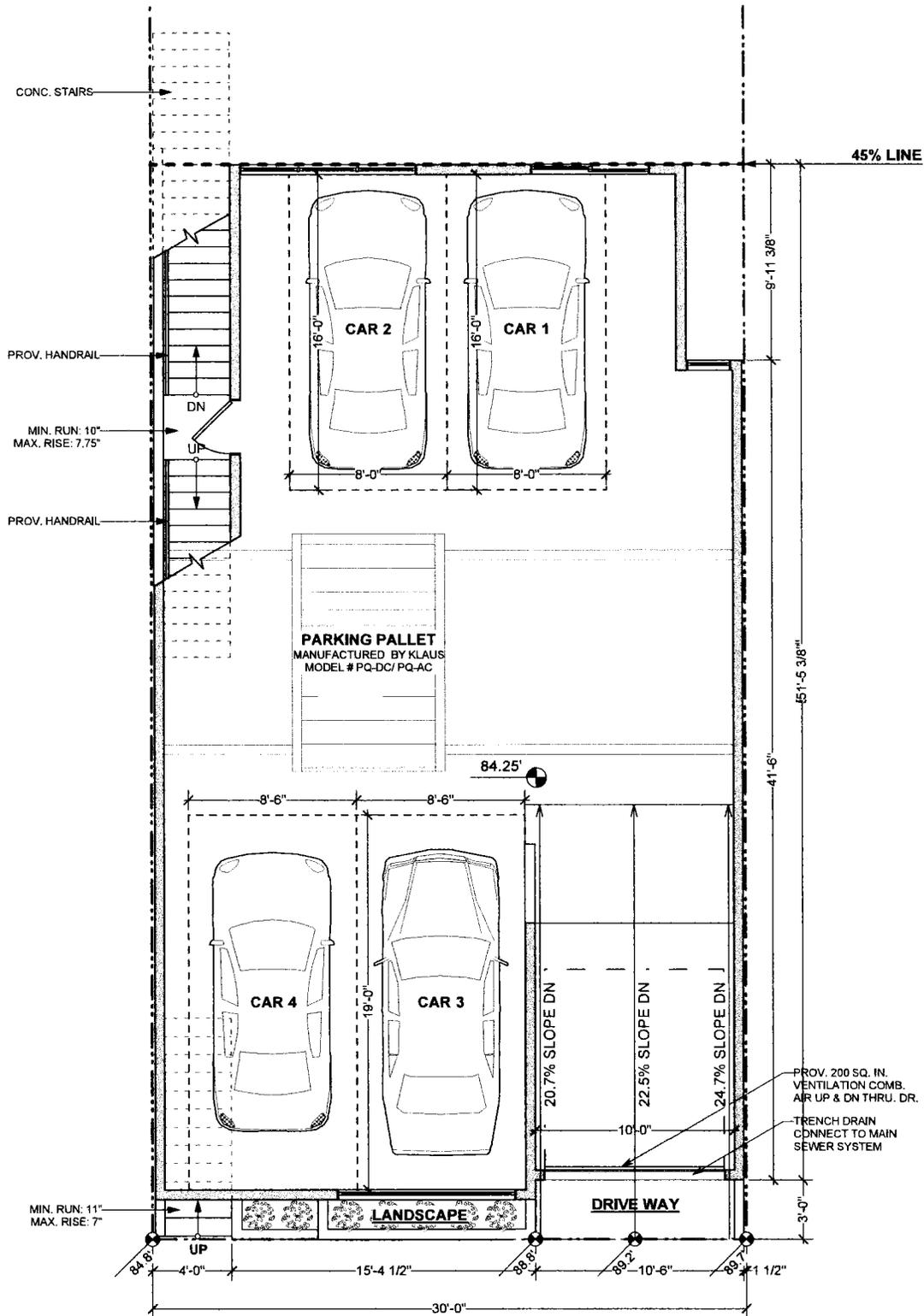




~~10.1073D~~

54-62 PERALTA AVE.  
 SAN FRANCISCO, CA





54-62 PERALTA AVE.  
SAN FRANCISCO, CA



60-62 Peralta Ave.  
 (17A)

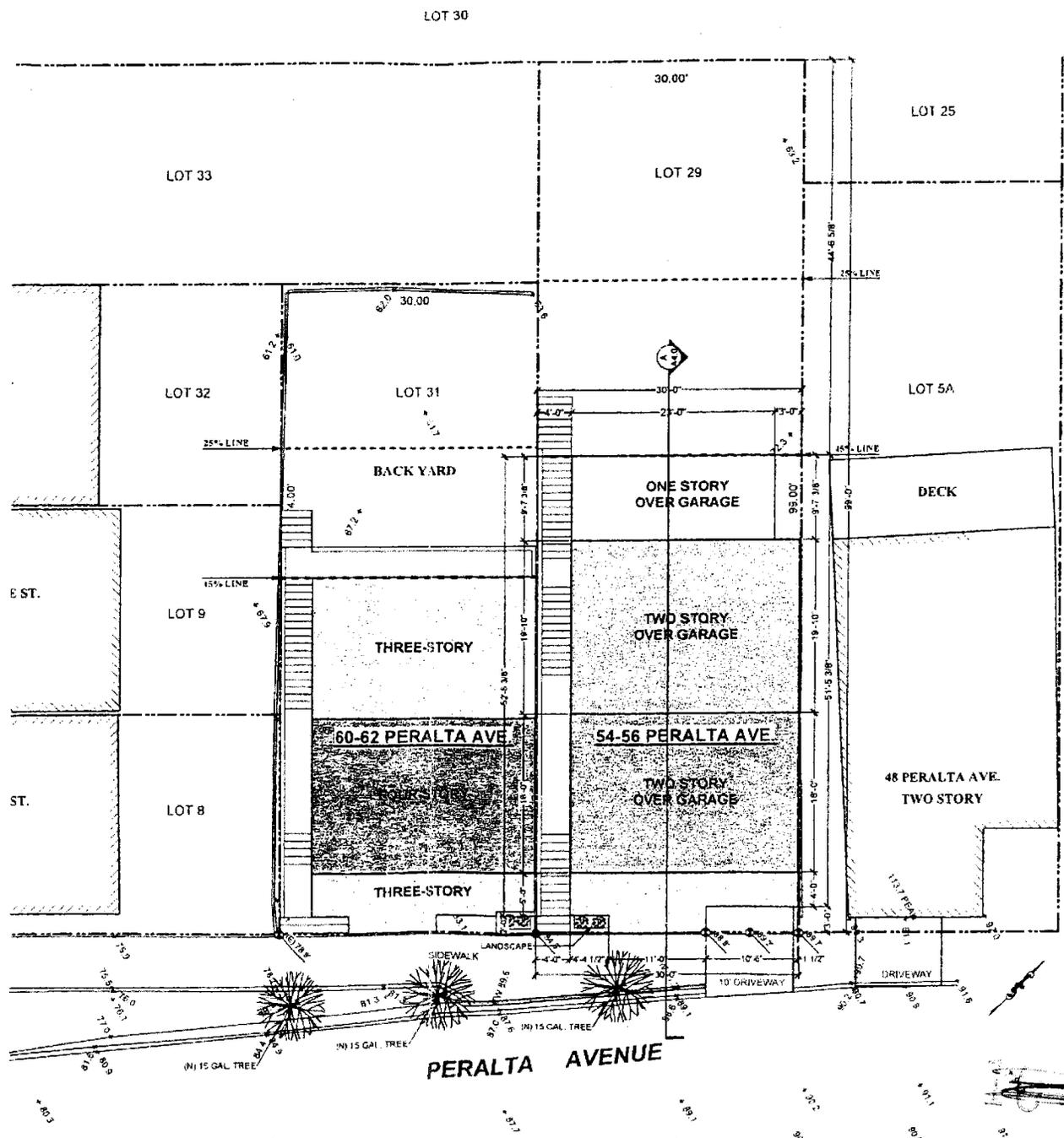
PROJECT NAME  
**54-56 PERALTA AVE**  
 SAN FRANCISCO, CA



SIA CONSULTING CORPORATION  
 1256 HOWARD STREET  
 SAN FRANCISCO CA 94103  
 TEL: (415) 922-0200  
 FAX: (415) 922-0203  
 WEBSITE: WWW.SIACONSULT.COM

SHEET TITLE

**SITE PLAN & NOTES**



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ISSUES / REVISIONS		
NO.	DATE	DESCRIPTION

Sheet A-1.0  
 Job No. 08-1364A  
 SIA Consulting Corporation  
 Revised 8-26-10

60-62 Peralta Ave.

18A

A  
B  
C  
D  
E

1569 HAMPSHIRE ST.

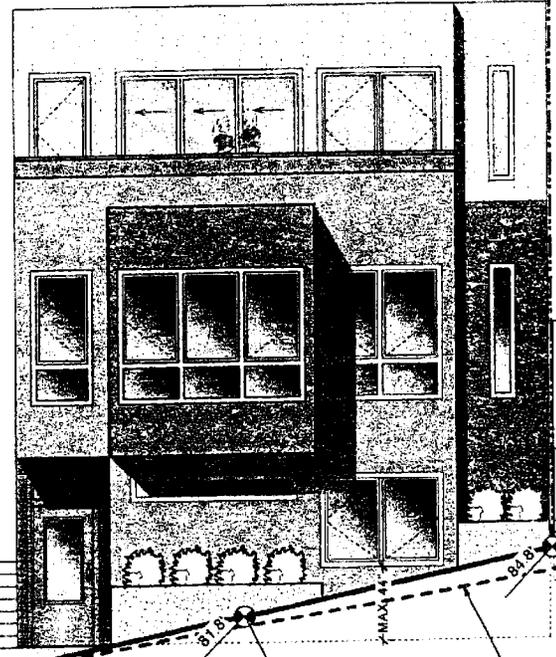
60 PERALTA AVE.

ROOF. FINISH ELEV.  
111.55'

FOURTH FLR. FINISH ELEV.  
100.55'

THIRD FLR. FINISH ELEV.  
90.55'

SECOND FLR. FINISH ELEV.  
80.55'



Sheet A-3.0  
 Job No. 08-1364A  
 SIA Consulting  
 Corporation  
 Revised 8-26-10  
 Front Elevation  
 60 - 62 Peralta Ave.

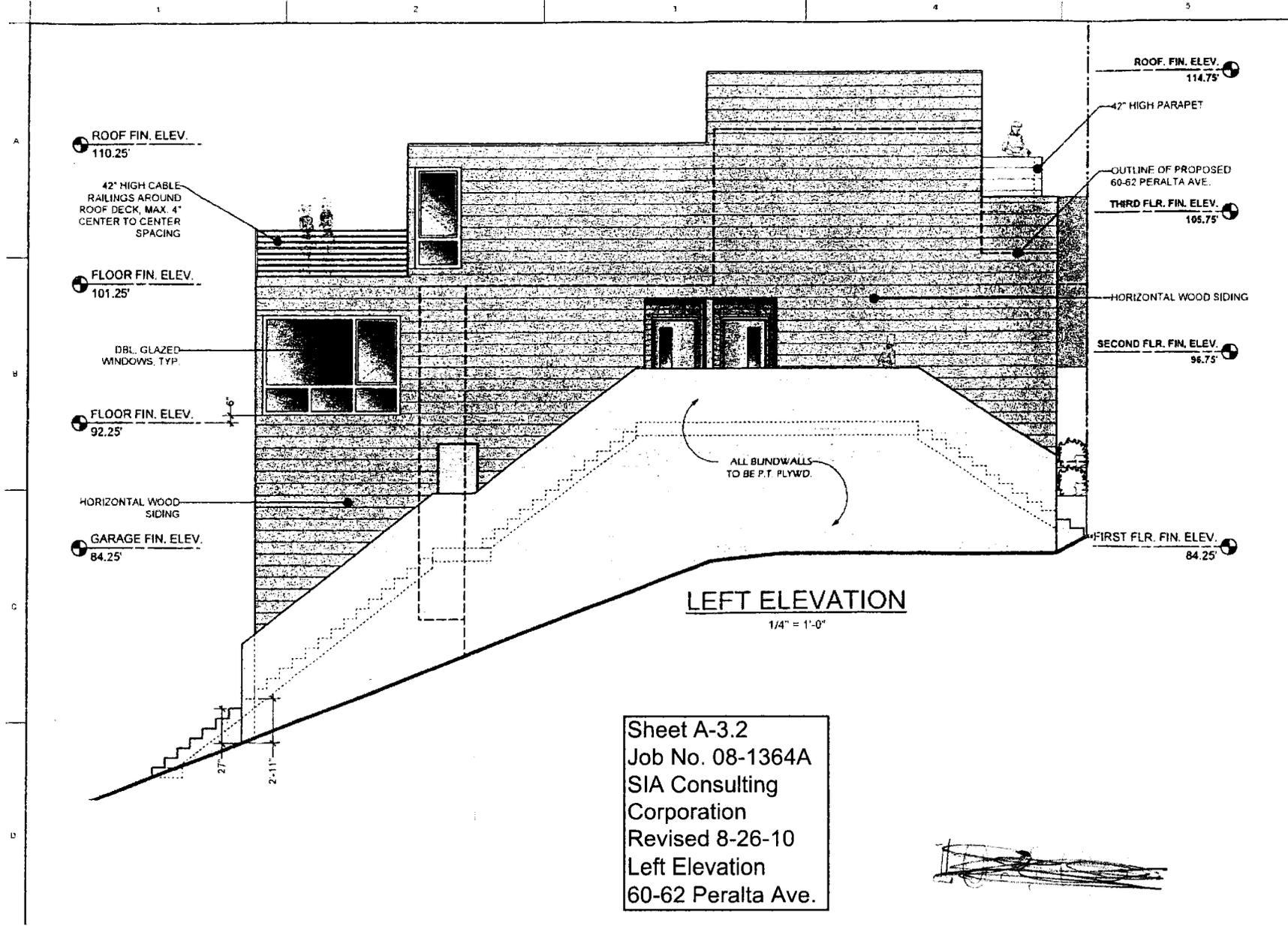
FRONT ELEVATION

1/4" = 1'-0"



60-62 Peralta Ave.

19A

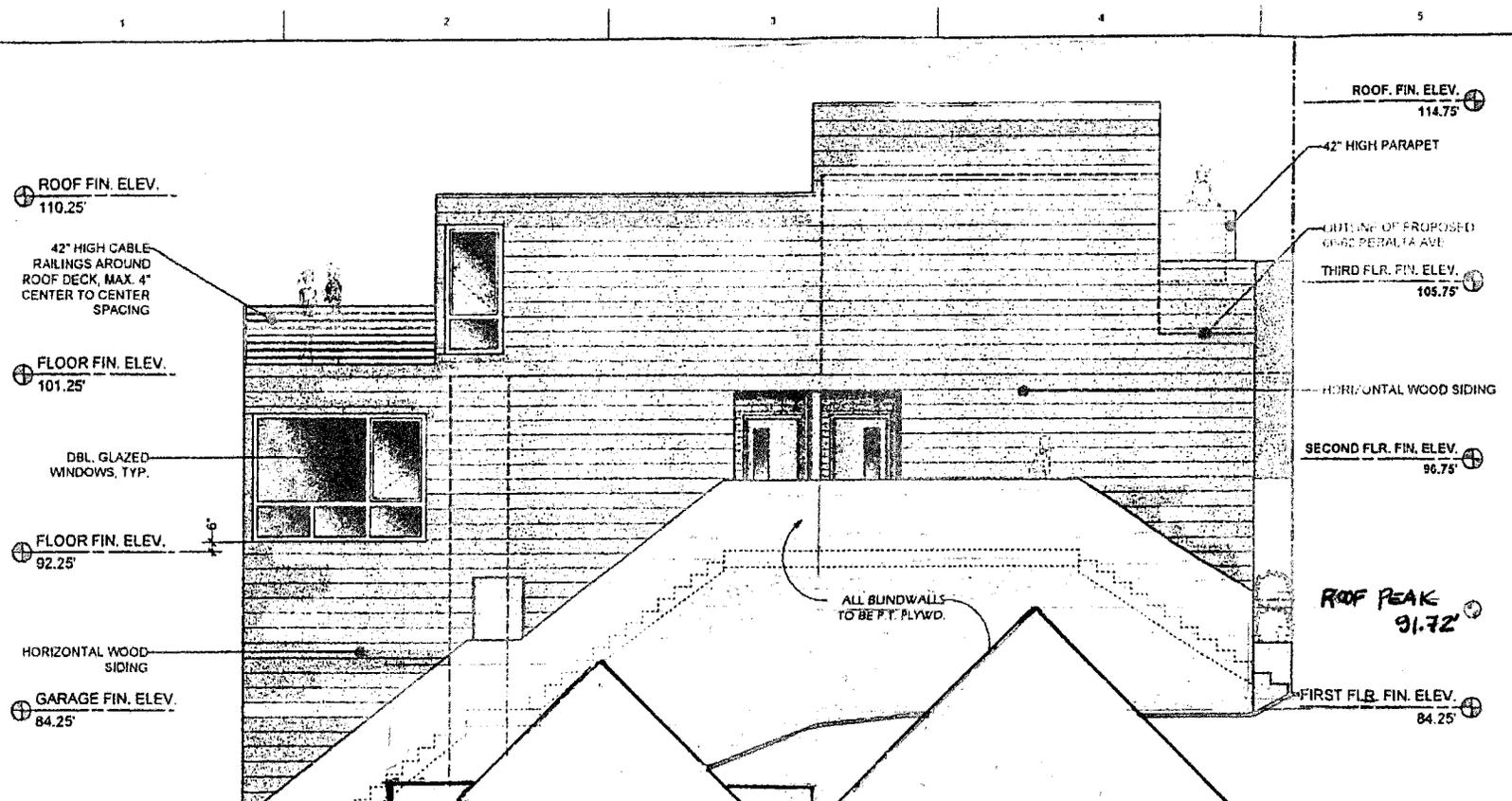


Sheet A-3.2  
 Job No. 08-1364A  
 SIA Consulting  
 Corporation  
 Revised 8-26-10  
 Left Elevation  
 60-62 Peralta Ave.



Hampshire Street Roof Lines in Elevation  
 Overlaid on Left Elevation

60-62 Peralta Ave  
 20A



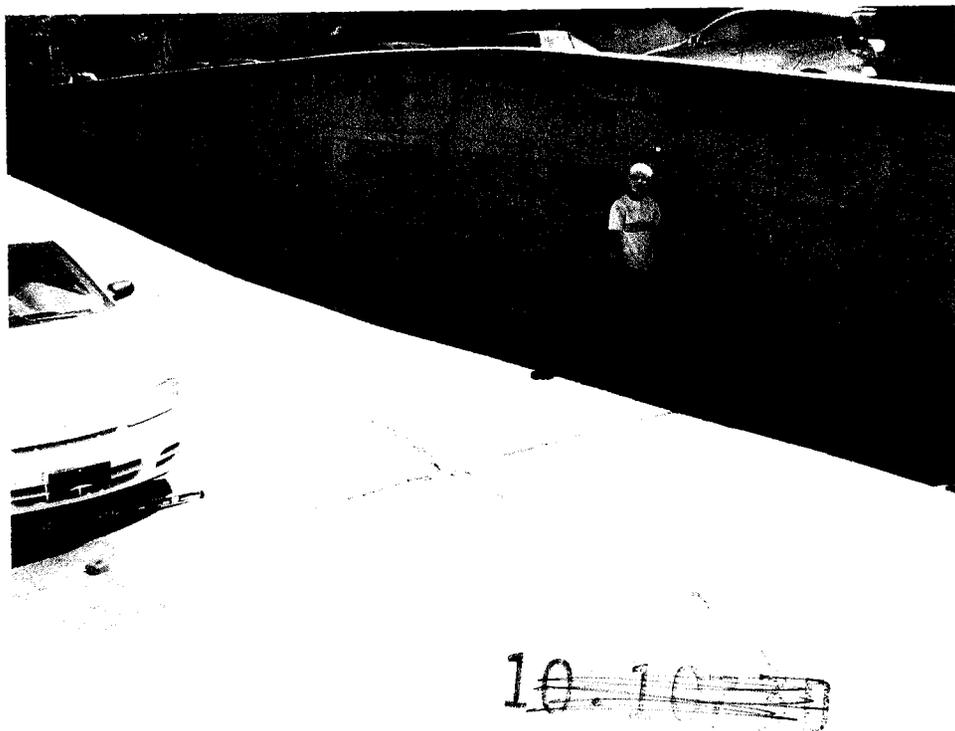
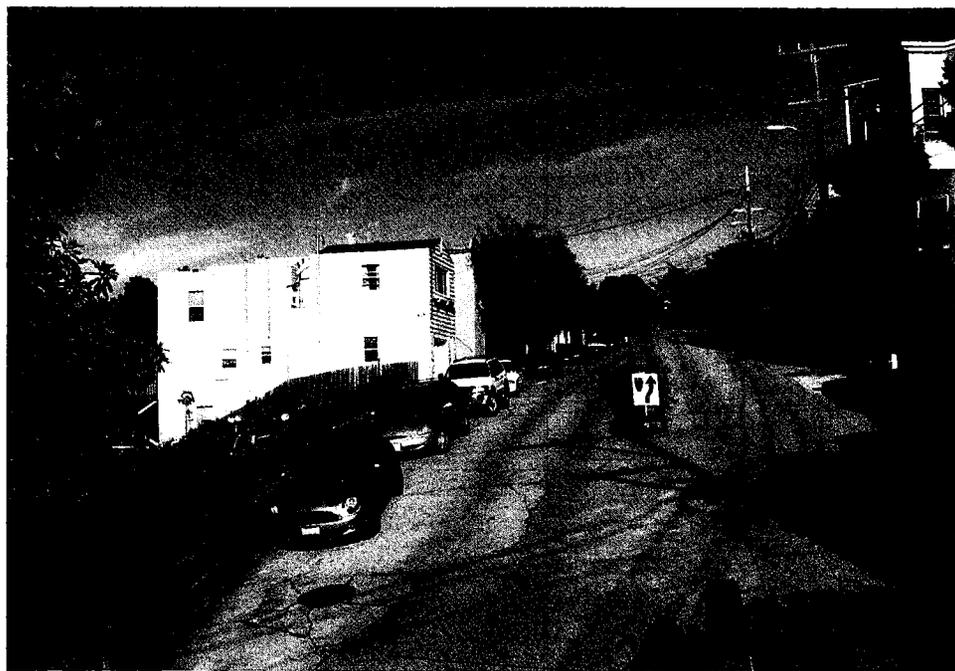
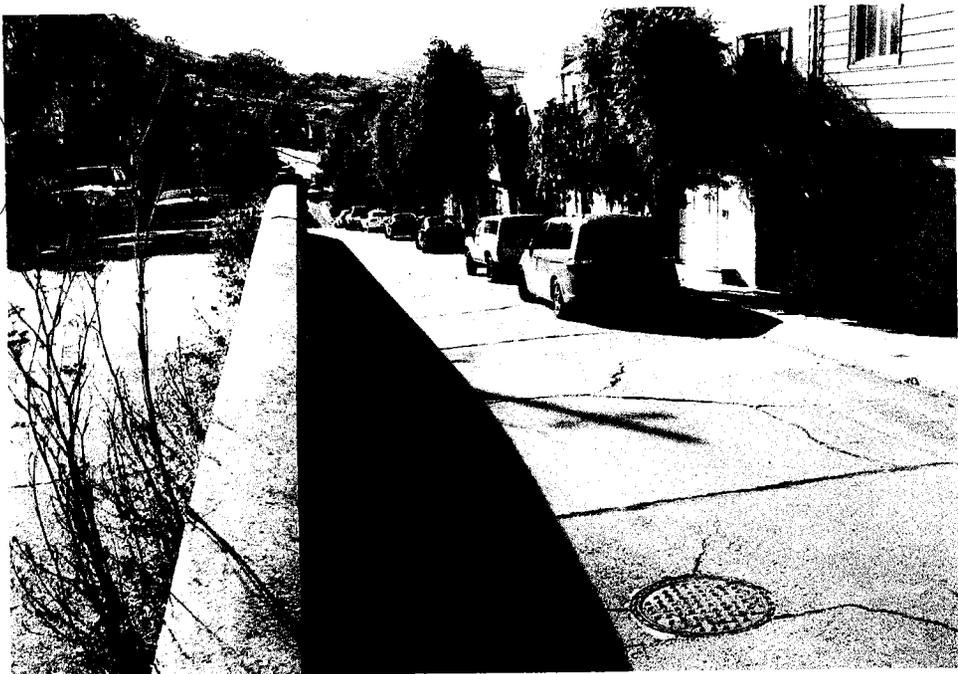
1569  
 HAMPSHIRE

MODIFIED  
 RDKimball  
 11/11/10





60-62 Peralta Ave  
(23A)



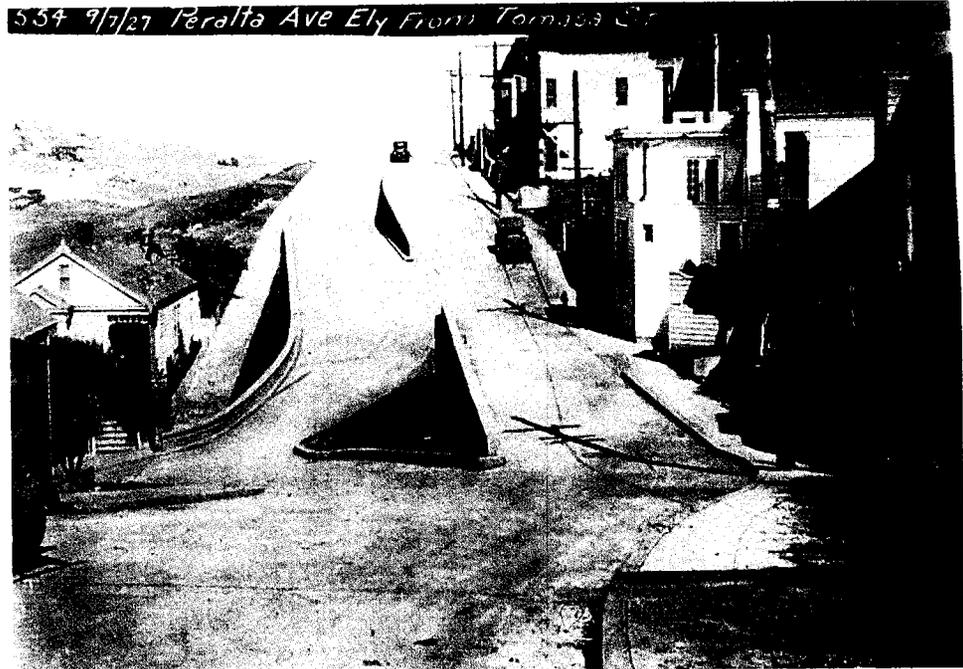
VIEW WEST TO EAST  
ON PERALTA AVENUE  
FROM HAMPSHIRE TO HOLLADAY

VIEW EAST TO WEST  
ON PERALTA AVENUE  
FROM HOLLADAY TO HAMPSHIRE

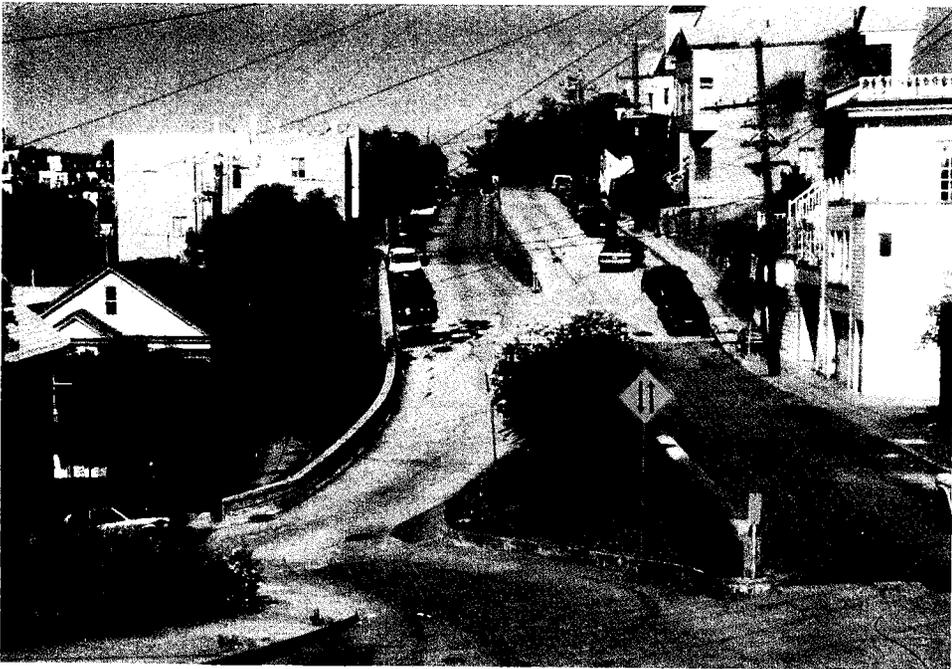
PERALTA AVENUE  
RETAINING WALL AT  
STREET CENTER IN FRONT  
OF 54-62 PERALTA

PERALTA RETAINING WALLS  
IN FRONT OF 54-62  
PERALTA AVENUE.

60-62 Peralta Ave  
24A



554 9/7/27 Peralta Ave Ely From Tomasa St



Peralta Ave Ely from Hampshire

12-1-27

HISTORICAL  
VIEW WEST TO EAST  
ON PERALTA FROM HAMPSHIRE  
TO HOLLADAY SHOWING NEW  
RETAINING WALLS.

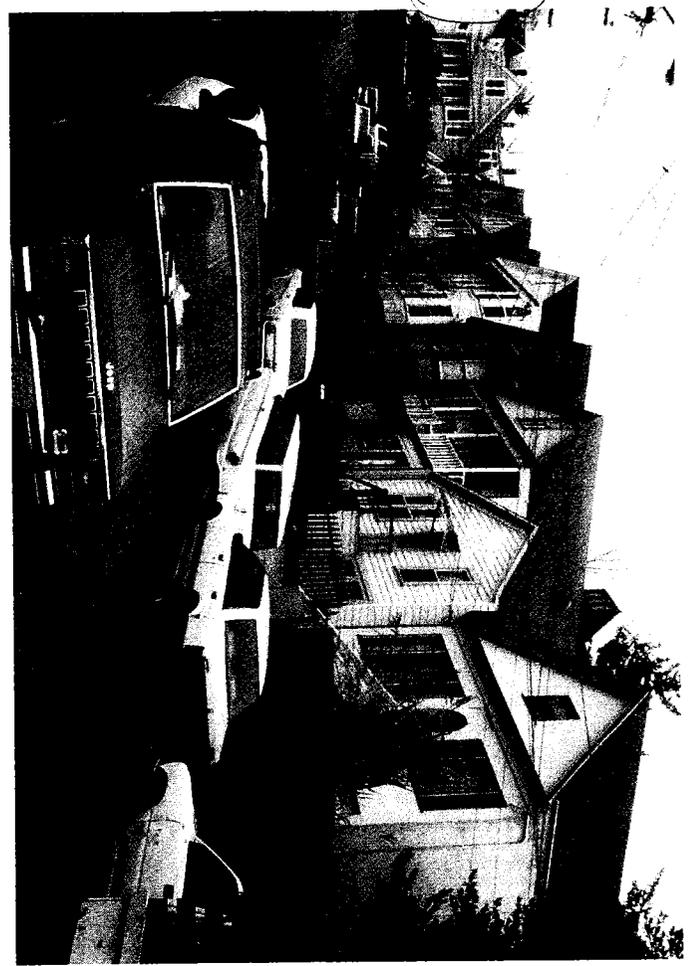
VIEW EAST TO WEST  
ON PERALTA SHOWING  
CENTER RETAINING WALL  
+ FACADES ON SOUTH SIDE

HISTORICAL  
VIEW WEST TO EAST  
ON PERALTA AVE  
FROM HAMPSHIRE TO  
HOLLADAY

CURRENT VIEW WEST TO EAST  
ON PERALTA AVENUE  
FROM HAMPSHIRE TO HOLLADAY

60-62 Peralta Ave.

25A



## HISTORICAL

VIEW EAST TO WEST  
ON PERALTA SHOWING  
CENTER RETAINING WALL  
AND STEPPED DOWN ROOF LINE  
OF NORTH SIDE.

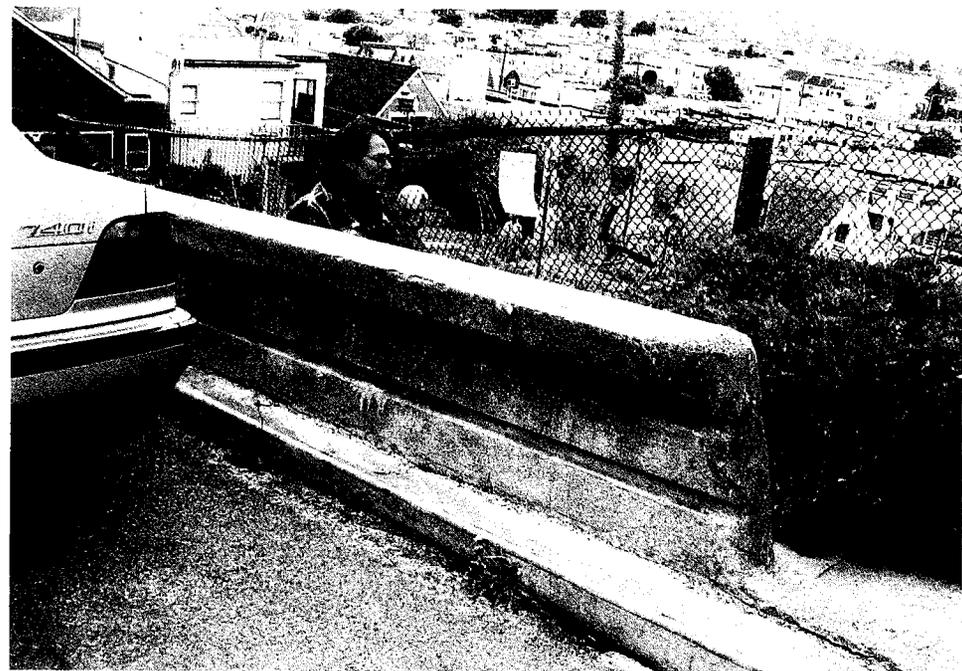
VIEW NORTH TO SOUTH  
ON HAMPSHIRE STREET  
(WEST SIDE) FROM  
CESAR CHAVEZ TO PERALTA  
SHOWING STEPPED DOWN  
ROOF LINE.

VIEW SOUTH FROM POTRERO  
STREET SHOWING BACK OF  
PERALTA AVE. HOMES  
WITH STEPPED DOWN ROOF  
LINES.

VIEW NORTH TO SOUTH  
ON HAMPSHIRE STREET  
(EAST SIDE) FROM CESAR  
CHAVEZ TO PERALTA SHOWING  
STEPPED DOWN ROOF LINE.

60-62 Peralta Ave.  
(26A)

60-62 Peralta Ave.



SIDEWALK IN FRONT OF  
54-62 PERALTA AVE.  
SHOWING SIDEWALK GRADE  
INCREASE AND RETAINING  
WALL / CURB CUT.

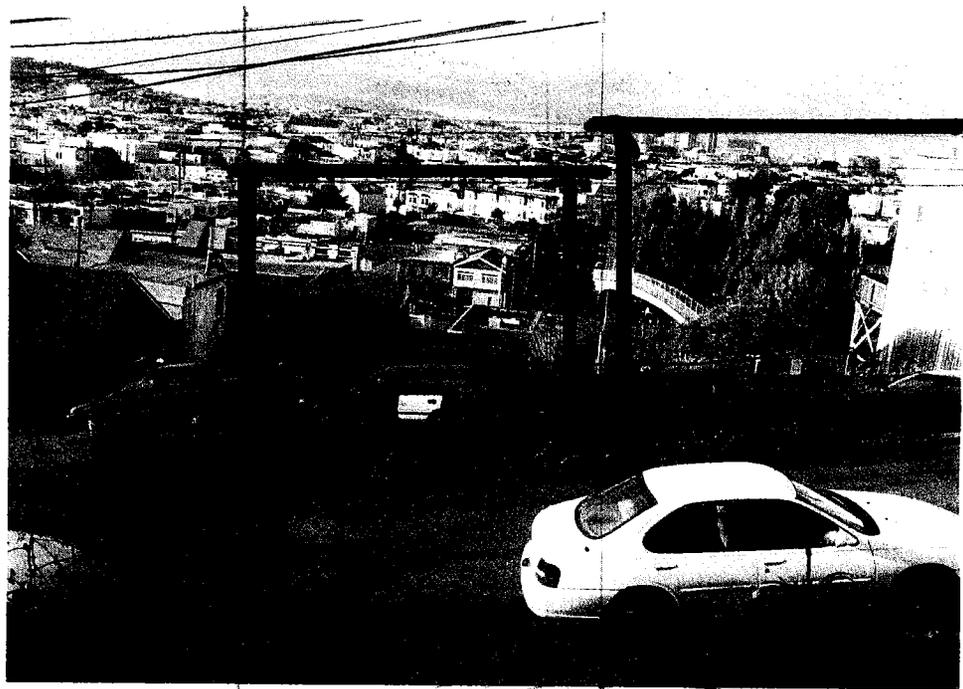
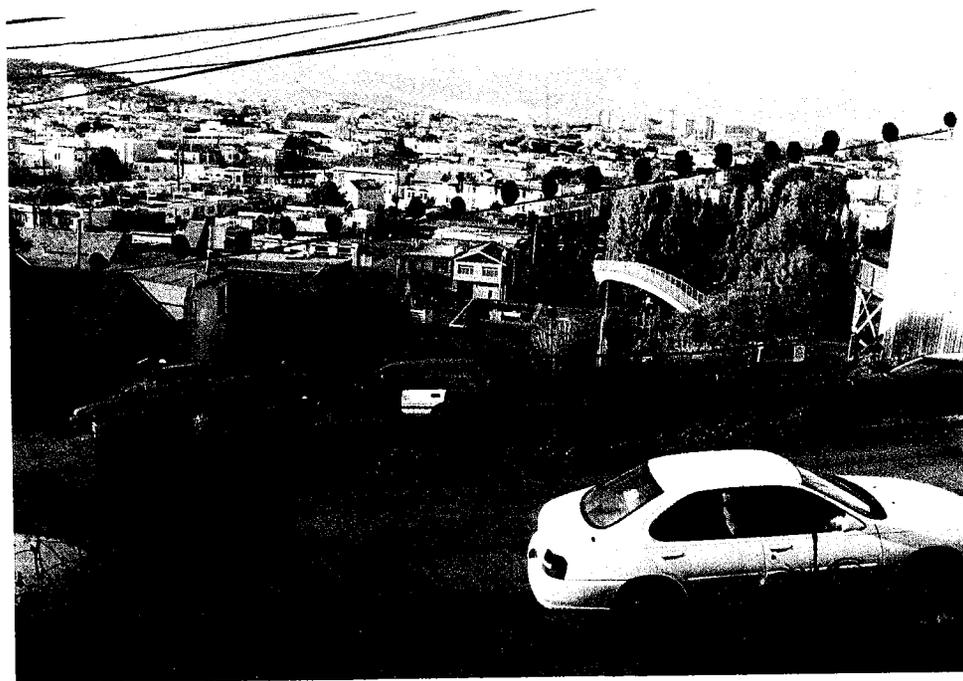
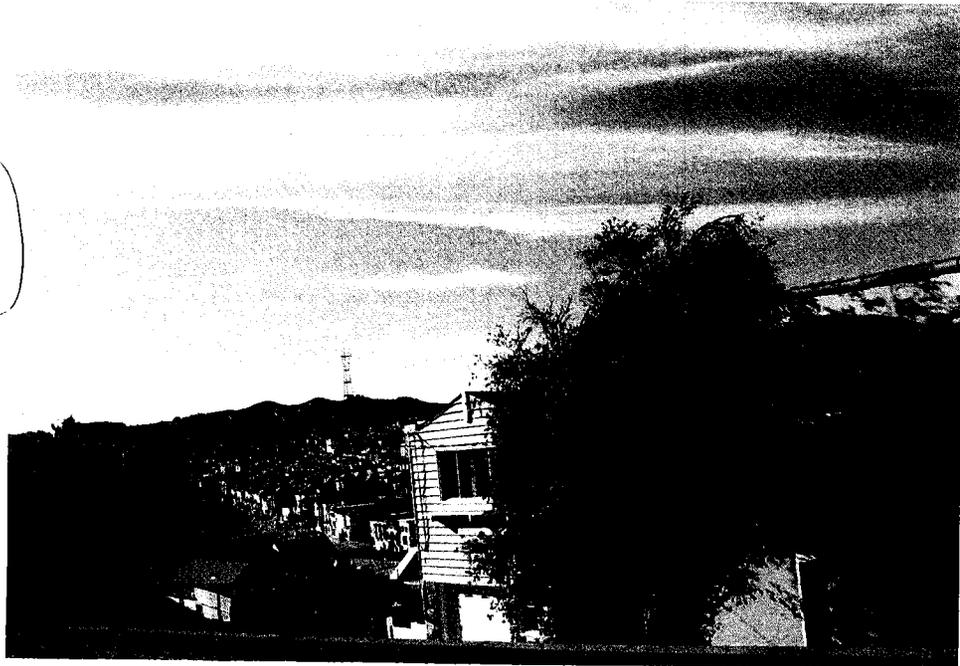
SIDEWALK IN FRONT OF  
54-56 PERALTA AVENUE  
SHOWING PROPOSED GRADE  
INCREASE AT PROPOSED RETAINING  
WALL AND CURB CUT AND REMOVAL

SIDEWALK IN FRONT OF  
54-62 PERALTA. PEDESTRIAN  
AT PROPOSED RETAINING WALL  
CUT, SHOWING PARTIAL  
OBSCURING OF PEDESTRIANS.

SIDEWALK IN FRONT OF 54-62  
PERALTA AVENUE AT PROPOSED  
RETAINING WALL / CURB CUT.

60-62 Peralta Ave.

(27A)



60-62 Peralta Ave.

(27A)

View from 41 Peralta Ave  
Looking North at open lots  
of proposed 54-62 Peralta  
Ave. Please note dotted  
line consistent with  
stepped roof line.

View from 23 Peralta Ave.  
Looking North West, showing  
48 Peralta Ave next to open  
lots of proposed 54-62 Peralta.  
Peaks of Hampshire St. roofs  
visable just above retaining  
wall.

View from 41 Peralta Ave.  
Looking North at open  
lots.  
Please note approximate  
facade outline of  
Proposed 54-62 Peralta  
in Black line.

View from 41 Peralta Ave.  
Looking North West, showing  
steep downhill grade in  
front of 54-62 Peralta Ave.

M. BRETT GLADSTONE

GLADSTONE & ASSOCIATES  
ATTORNEYS AT LAW  
PENTHOUSE, 177 POST STREET  
SAN FRANCISCO, CALIFORNIA 94108

TELEPHONE (415) 434-9500  
FACSIMILE (415) 394-5188  
admin@gladstoneassociates.com

February 17, 2011

President Ron Miguel  
Planning Commission  
1650 Mission Street, Suite 400  
San Francisco, CA 94103

Re: 54-56 and 60-62 Peralta Avenue  
Response to Discretionary Review Request

Dear President Miguel and Planning Commissioners:

We represent Thomas Aquilina, the owner of the properties located at 54-56 and 60-62 Peralta Avenue. The owner proposes to construct two new buildings, one on each lot, each containing two units. The zoning calls for two units per lot, and this is encouraged by Planning Staff since two Muni lines are located very close. The owner has made many compromises but was unable to satisfy all of the neighbors.

**I. PROJECT DESCRIPTION.**

The project involves two properties adjoining each other. Each property is vacant except for some landscape art objects placed on the lots by a neighbor without authorization from the property owner. The properties slope steeply downhill away from the street. A concrete wall barrier created by the City lies between cars and the street sidewalk, the latter of which is at a lower elevation than the street. (See Exhibit A.)

The project would construct a new building on each property, each containing two small dwellings, the largest of which is a 3 bedroom unit at 1728 square feet (at 60 Peralta). (The plans

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February 17, 2011  
Page Two

are attached to a separate letter from us to you). The units are designed for families with each unit containing three bedrooms, except 54 Peralta which contains two bedrooms.

The buildings' height is mitigated by the severely down-sloping lot. In this 40 foot height district, the buildings' height is only 29 feet from the front curb.<sup>1</sup> Further, the top floors are set back 7 feet at the front. The top floors also are set back at the rear: 9' 11" on the uphill building and 16' 2" on the downhill building. (See plans.) As a result, the top floor on the downhill building is reduced to the minimum needed for one bedroom and bathroom at the top floor. All building statistics are shown at Exhibit H. Renderings are shown on Exhibit I.

The lower building at 60-62 Peralta would have no garage, for reasons discussed below. The new building at 54-56 Peralta Avenue would provide five parking spaces to serve both buildings. One two unit building may have three spaces, and the other may have two spaces. The property is located within the Bernal Heights Special Use District, which has a rather archaic rule which requires *seven* spaces. As a result, the project requires a parking variance for two spaces even though it is one block from public transportation. MUNI Line 27 stops within one block and Line 9 (and 9 Limited) stops within 1 - 1½ blocks from the project site.

Some party or event has previously demolished the first 3 to 4 linear feet of the concrete barrier which prevents vehicles from veering off the street into the sidewalk at a lower elevation. An additional 6 feet in length of concrete barrier at the street would be removed to accommodate the driveway to the upper building, and to avoid creating a bridge over the sidewalk from the

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<sup>1</sup> The uphill building is only 27 feet tall as measured from the front curb. As to the lower building, a typo on page A-3.0 shows the roof elevation above sea level incorrectly.

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street to the new building on the downhill side. (See Exhibit B also contains a letter from client's consulting engineer concerning the feasibility of the retaining wall and driveway).

The Aquilina family has created over a dozen setbacks, greater landscaping, and façade changes to (1) accommodate all the Planning Department's concerns (2) accommodate all but two of the concerns of the official neighborhood Design Review Board (See Exhibit G); and (3) accommodate the reasonable concerns of neighbors.

## **II. BACKGROUND.**

Contrary to the DR Requestor's allegations, *Mr. Aquilina is not a builder of properties professionally*. He was raised nearby in San Francisco's Mission District, only a few blocks from the project site. His mother still lives in that house. Mr. Aquilina is a MUNI truck driver. Mr. Aquilina purchased the property from a friend as an investment for his family. He is building the units so that his daughter will reside in one unit and his son in another. Mr. Aquilina's plan is to retire with the help of rental income from the other two units. As a result, this project is providing two new homeownership opportunities to help keep young families in the City (particularly younger members of an existing San Francisco family). And the project will provide new rental units that are relatively affordable since they are small.

## **III. THE REQUEST FOR DISCRETIONARY REVIEW IS WITHOUT MERIT.**

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Page Four

Mr. Aquilina or his representatives have had three meetings with the Bernal Heights East Slope Design Review Board and separate meetings with neighbors. **As a result of these meetings, Mr. Aquilina agreed to several compromises including the following:**

1. Reduced the width of each building by four feet to provide additional light and minimize the street impact. This has created a sideyard set back for (downhill) DR Requestor's benefit. (Mr. Besso lives at the corner of Hampshire and Peralta);
2. Eliminated the car lift and a steep ramp to it, replacing it with the one sliding pallet, thus allowing the residents to access their space without turning the steering wheel. This also minimized the amount of excavation, which was a concern to neighbors;
3. On 54-56 Peralta (the upper building), provided a three foot set back at the rear corner of the building to allow for additional light and ventilation for 48 Peralta;
4. On 60-62 Peralta, eliminated 400 square feet to accommodate a side set back for the benefit of DR Requestor.
5. On 54-56 Peralta, provided a rear building notch along the property line for the benefit of the uphill neighbor at 48 Peralta.

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Page Five

6. On 54-56 Peralta, created two setbacks (at different elevations) on the top floor rear for the benefit of adjacent neighbor at 48 Peralta: 19' 6" at the higher building level and 9' 11" at the next level, for a total of 29' 5".
7. Enhanced the sidewalk lighting for better security;
8. Added landscaping at the front and rear at the request of neighbors.

Despite these efforts, this DR Request was filed claiming the following:

A. ***The project's size and scope is out of scale with the neighborhood character.*** In reality, the surrounding block (facing the project and on the same side of the project) is characterized by two-story and three-story buildings containing single family and multi-unit buildings. DR Requestor has told our client's architect that the surrounding lots are (with very few exceptions) all single family homes. Our survey of the number of mailboxes on the nearest lots shows more of a mixture.<sup>2</sup> (See Exhibit C). Neighbors have also stated the units are greatly oversized. Page 2 of Exhibit C indicates differently. The buildings' height are mitigated by the severe down sloping lot and significant set backs of the top floors at both the front and rear.

B. ***The plans do not contain sufficient information about the repairs to sidewalk grade and removal of a portion of the retaining wall to accommodate the driveway and garage.*** That information has been shown to neighbors on page A-3.0 of the plans. In order to provide the parking, a driveway is necessary. The driveway's location was chosen because it would

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<sup>2</sup> The mailbox survey doesn't determine the legality of the units. The number of legal units has not been surveyed.

President Ron Miguel and Planning Commissioners  
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Page Six

require the least amount of grade change and least amount of additional removal of the street's concrete barrier; and avoid a bridge over sidewalk. This bridge would be required if the driveway were located any more downhill toward Hampshire Street. The East Slope Design Review Board recognized that a driveway cannot be created for 60-62 Peralta Avenue. The new driveway across the uphill property sidewalk will not be steep at the entrance to the street. (See A-3.0 of the plans.)

C. There is a very abrupt change in elevation on the sidewalk along the joint property line between the proposed uphill property and the existing property next door at 48 Peralta. This was created when an adjacent neighbor (or her predecessor) at 48 Peralta added a garage and driveway to the property and the sidewalk change was not approved according to our research. This owner or a previous owner created a bump due to a newly created slope (about five to seven feet in length) with a grade of approximately 35 percent, much greater than the amount allowed by City code. (See Exhibit D). Such a grade change and the bump it creates makes it difficult for the disabled and elderly and creates a trip hazard for any resident at night. My client's project would remove the bump illegally created. The result is that to make a safe grade change across the 70 feet width of my client's two properties, my client has to change an existing slope (on the lower sidewalk) from 16.6 percent to 20 percent (a percent allowed by Department of Public Works and accessibility laws). (See A-3.0 of the plans.) In response to neighbors concerns regarding safety and aesthetics, Mr. Aquilina also has agreed to do additional work on the public right-of-way, including installing landscaping just below the concrete barrier and lighting.

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Although these issues will be addressed during the review by the Building Department and Department of Public Works (“DPW”), we met with DPW to be sure the plans for the sidewalk and driveway are feasible. DPW did not raise any objections to the plans. We are certain that if the Building Department or DPW has concerns about the feasibility and safety of the design, the Departments will require them to be appropriately addressed when they finish plan review. DPW has mentioned that it and Planning approved a very similar project nearby on Elsie Street. Information on the project (and how it resembles this project) is shown at Exhibit E.

Appellants seem to be asking that your Department and Commission make the determination normally made by DPW and/or DBI, as to safety of work on public property and whether a car palette design follows the Codes applied by these two other City Departments. This has several problems. First, it is economically risky for an owner to create the very detailed construction documents to provide to DBI and DPW before it knows from your Commission that there is an approved building envelope design. Second, it would require the Planning Staff to have the expertise of experienced members of other City Departments.

*Four parking spaces are inadequate and cannot be created using the mechanical parking pallet.*<sup>3</sup> The project would provide five spaces. Five spaces are sufficient given the sites’ close proximity to at least two MUNI lines within two blocks. The parking pallet is shown at Exhibit F (and has been done elsewhere in the City). The zoning, which bases number of parking spaces required on a size of a building (no matter how close to MUNI), is out of date.

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<sup>3</sup> The neighbors are concerned that the parking pallet cannot function and provide parking as shown. However, our client’s architect has confirmed with the parking pallet company that such a parking pallet is feasible.

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*The project is out of scale with the neighborhood and would block light and air to the buildings facing Hampshire.* As shown on Exhibit C there are at least eight buildings containing two units (see attached chart on neighboring lots). The new buildings contain design features to mitigate any impact.

As stated in Sections I and III above, set backs for the benefit of neighbors have been made on the building's sides, front, rear and other places. Even so, the Bernal Heights Design Review Board requested an additional set back of the lower building at the front of 8 feet. This would effectively eliminate the top floor and one bedroom making the top floor unit at 62 Peralta only a unit of 2 bedrooms not conducive to families. A shift of the floor could be done so as to have more front setback and less rear setback. (See page 2, Exhibit G).

We believe some of the opponents have other concerns that underlie their opposition. Several neighbors located across Peralta have views that would be blocked by the top floors of the new buildings. A view is not a protected property right.

The property owner has complied with the wishes of the neighborhood's official City-recognized Design Review Board, with two exceptions discussed on Exhibit G.

There are several City policies we feel are not taken into account by neighbors in opposition:

1. Where one can build two units in a two unit zoning, particularly when the lots are so close to MUNI, the City encourages this.

GLADSTONE & ASSOCIATES  
ATTORNEYS AT LAW

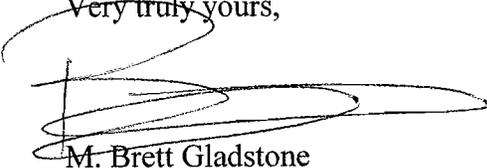
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Page Nine

2. When a project can build rental housing and ownership housing, particularly when the latter is for existing younger City residents to who wish to stay in the City, the City encourages such construction.

3. When a project is less than two blocks from several MUNI lines, the City should promote no more than 1:1 parking and, in the case of new zoning plans throughout the City (not yet reaching Bernal Heights), the City recently has discouraged 1:1 parking. The neighbors here wish to see seven spaces for four units.

This project fulfills these policies while minimizing any impact to the neighborhood. The main impact we have heard about is the potential loss of views of neighbors across the street. Therefore, we respectfully request that the Planning Commission deny the DR Request and approve the project as proposed.

Very truly yours,

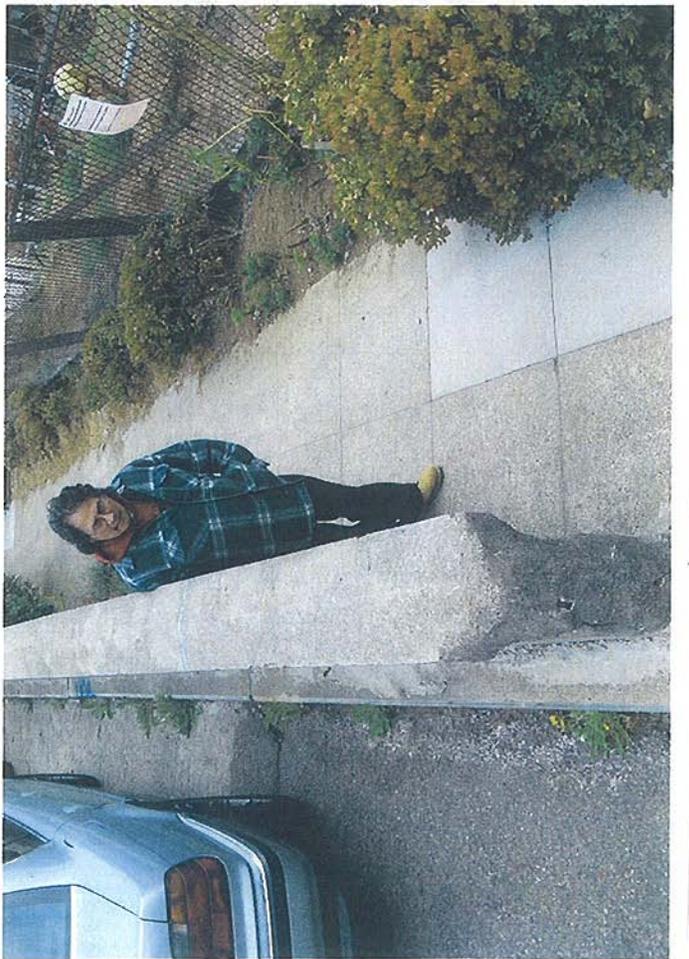
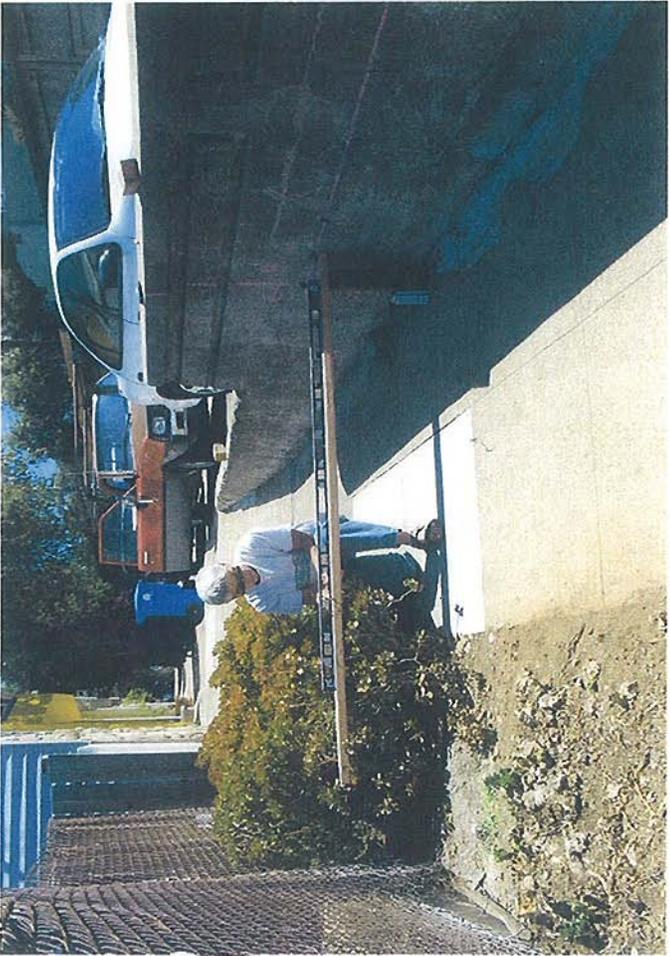
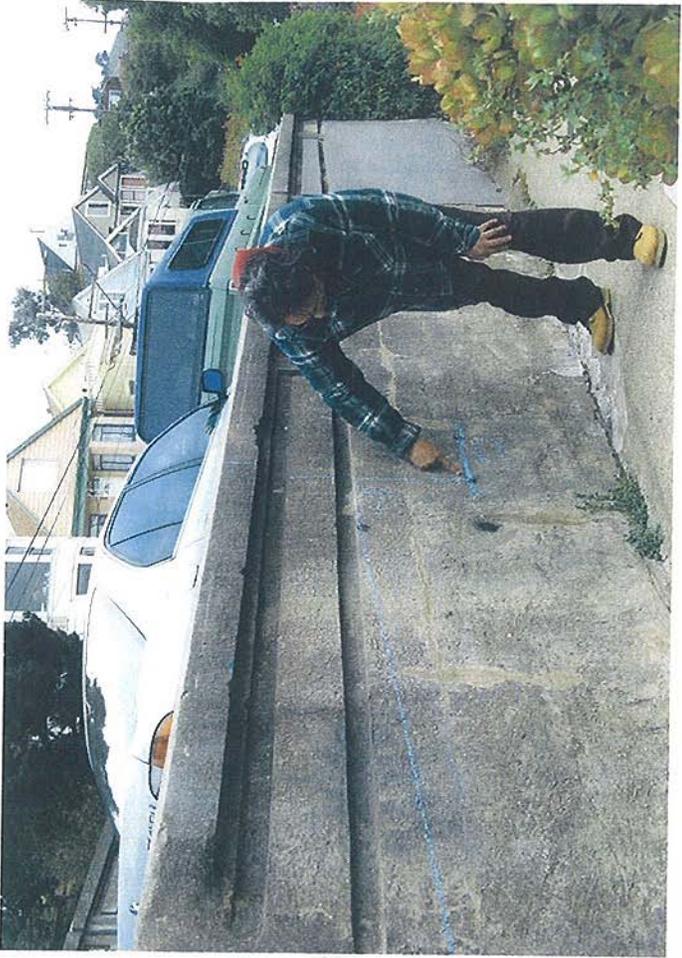


M. Brett Gladstone

Enclosures

cc: Tom Aquilina, owner  
Reza Khoshnevisan, architect  
Bob Besso, Appellant  
Design Review Board  
Zoning Administrator  
Director John Rahaim









PORTION OF THE CONCRETE WALL TO BE REMOVED IN ORDER TO ACCOMODATE THE NEW DRIVEWAY

54 PERALTA AVE.  
SAN FRANCISCO, CA



# BGA CONSULTING ENGINEERS

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February 16, 2011

President Miguel and Planning Commissioners  
San Francisco Planning Commission  
1650 Mission Street, Suite 400  
San Francisco, Ca 94103-2479

Re: 54-62 Peralta Avenue  
Proposed Parking Layout and Retaining Wall Modification

Dear President Miguel and Planning Commissioners:

Our firm has been retained by Mr. Tom Aquilina to review SIA Consulting Corporation's proposed parking approach and layout for 54-62 Peralta (SIA Project No. 08-1364A). In addition, we have reviewed the modifications to the city owned retaining wall at the city right of way in front of the 2 subject lots.

The arrangement of the proposed parking and driveway approach is comfortable and in conformance with typical San Francisco parking garages. Furthermore in our opinion, once at the back of the sidewalk, the car would be at a gentle slope and therefore there exists no sight obstruction.

In addition to the review of the parking layout and design, we analyzed the modifications to the city retaining wall between Peralta Avenue and the sidewalk as proposed by the project sponsor. The design reduces the unsupported height of the concrete retaining wall and will not cause any additional loading to the wall or the sidewalk, and therefore will not undermine any city owned structure.

Respectfully Submitted,

  
**Bahman Ghassemzadeh, P.E.**  
LEED Accredited Professional



Cc: Tom Aquilana  
Reza Khoshnevisan  
M. Brett Gladstone

4335 Lincoln Way, San Francisco, Ca 94122  
(650) 678-0234  
bghassemzadeh@gmail.com

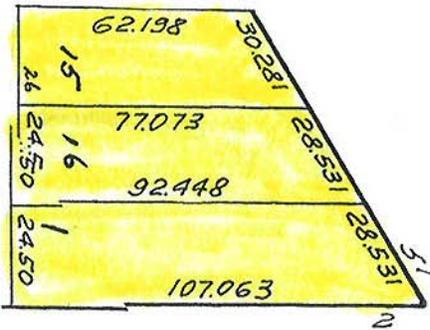




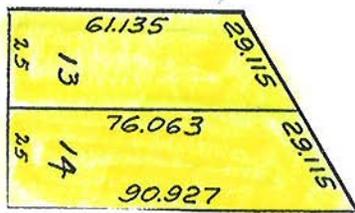
Survey to Determine Average Square Footage of All Units on Nearest Lots

AVERAGE UNIT SIZE = 1,971 SQUARE FEET

- Highlighted lots are the lots surveyed
- 2 of these lots had no official record of unit square footages, thus these lots were not included in the average

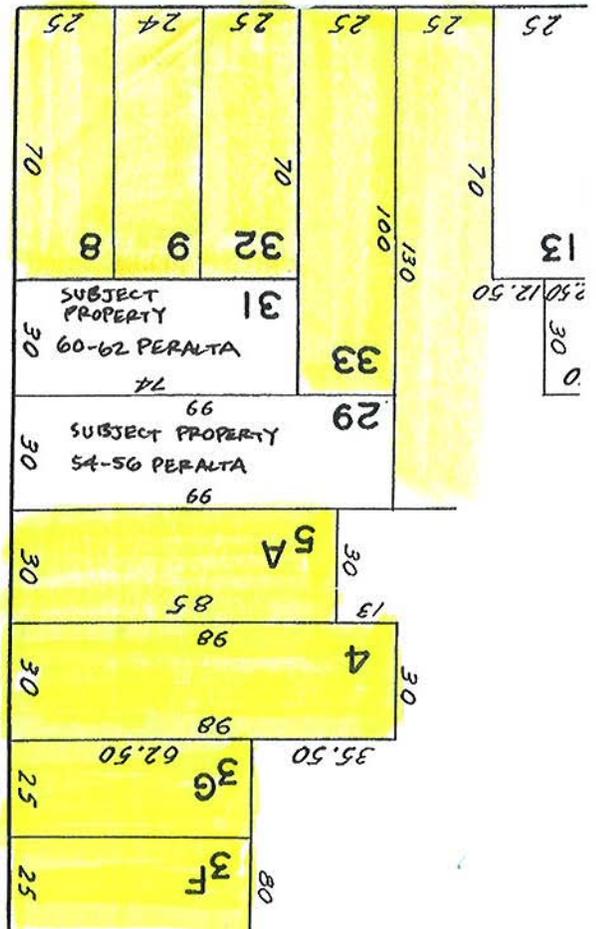


MONTCALM



PERALTA AVE.

HAMPSHIRE ST.





## Proposed Slope of Sidewalk

- **The proposal improves on the existing conditions.** The proposal takes what was done without permits and makes it better and safer.
- **Existing conditions:** A steep slope was created in the sidewalk adjacent to 48 Peralta Avenue when the neighbor leveled the sidewalk area in front of her driveway (no permits or City approval has been found). See the red arrow in the photo below; it points to the non-code compliant portion of steep sidewalk, which is currently approximately 35% slope.

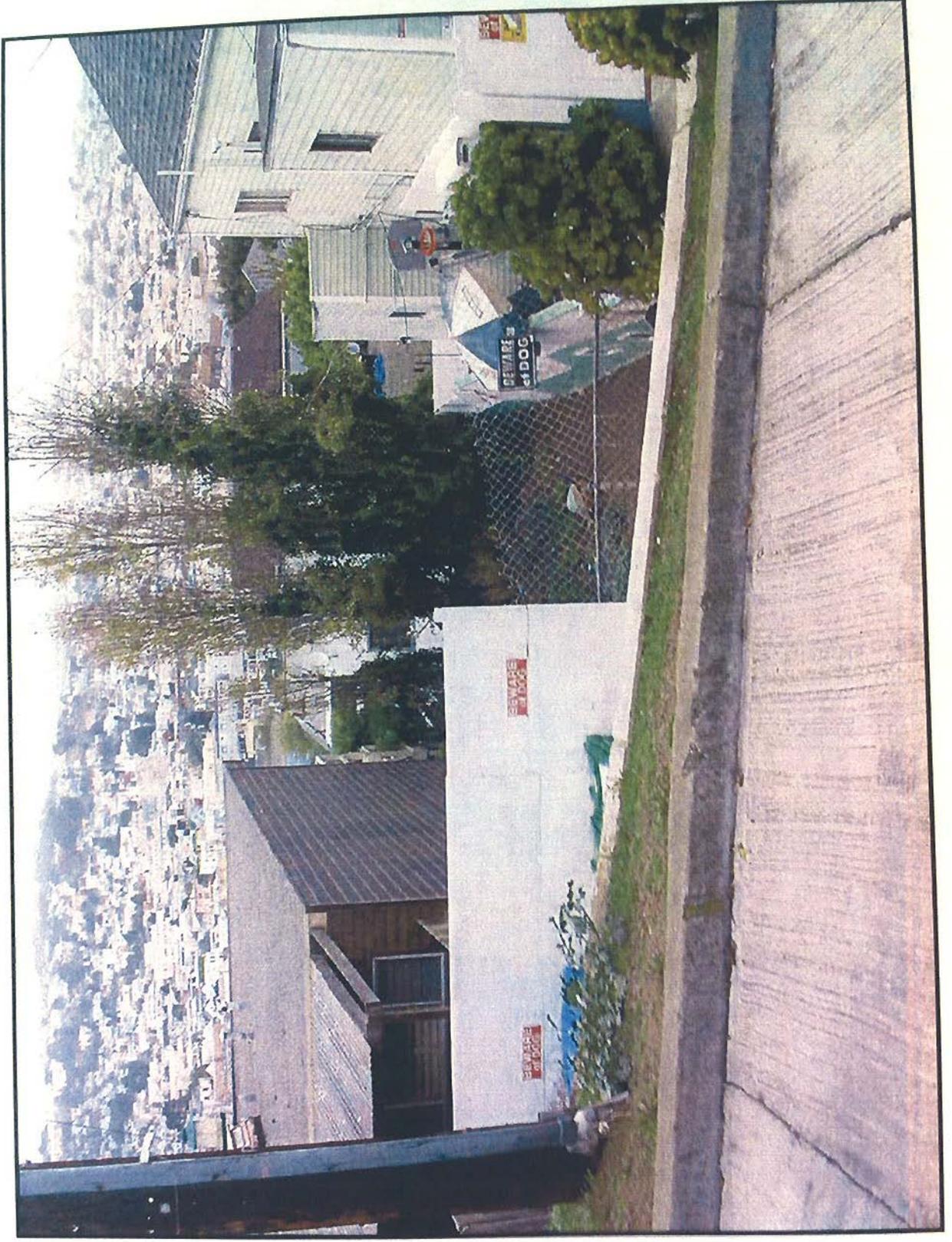


- **Existing Conditions Photo**
- **Proposed Conditions:** The proposed change in sidewalk makes the abrupt change (shown above) more gradual. In addition, the proposed sidewalk slope in front of 54-56 Peralta Avenue and 60-62 Peralta Avenue will go from approximately 16.6% slope to 20% slope. The increase in slope is only 3.4% which is very minor and the change is code compliant.
- **DPW says the proposed slope meets code:** The Department of Public Works has informally told us that the proposed sidewalk slope meets code, but their approval is pending (and premature at this time).

**EXHIBIT E**

The nearby Elsie Street project, now built, presented the same issue as this project: *how to create adequate parking, and how to locate a garage entrance, when a City street lies several feet above a sidewalk which is adjacent to a new building or buildings.* The following pages indicate how the Planning Staff and DPW and DBI accommodated this on Elsie Street. Our client's design is even simpler, as it does not involving ending the sidewalk before it reaches the front of the subject lots, and avoids forcing pedestrians to walk up ramps to reach a sidewalk newly installed next to the street.

# 152 Elsie Street as it is today.



April 6<sup>th</sup>, 2006

Nick Elsner  
Department of Public Works  
Bureau of Street Use and Mapping,  
875 Stevenson Street, Room 460  
San Francisco, CA 94107

**Re: Case No. 2006.0390R  
152 Elsie Street Major Encroachment  
Assessor's Block No 5618, Lots 020, 021, 019, 018**

Dear Mr. Elsner:

Pursuant to Section 4.105 of the City Charter and Section 2A.53 of the Administrative Code, on March 2<sup>nd</sup>, 2006, the San Francisco Planning Department received the above referenced General Plan Referral from the Department of Public Works (DPW) for a Major Encroachment Permit in the public right-of-way to build a private driveway in front of 152 Elsie Street, Assessor's Block 5618, Lot 020 and to improve the sidewalk in front of adjacent lots 021, 019, and 018. The project is located on Bernal Heights.

This is a revised submission for a proposal submitted to construct a driveway at the same location which was reviewed under Planning Case No. 2003.1233R. In April 2004, the Planning Department informed the applicant that the Department would have to bring the project to the Planning Commission for its consideration, because the Department determined that the proposal was controversial and staff could not recommend finding the project, as then designed, in conformity with the General Plan.

Currently, the project sponsor has submitted the subject General Plan referral application (Case No. 2006.0390R), and is seeking to install a driveway in the public right-of-way to provide vehicular access to 152 Elsie Street (Lot 020 in Assessor's Block 5618). It also proposes to improve and create a new sidewalk in the public right-of way fronting lots 021, 019, and 018 on this portion of Elsie Street. The new sidewalk would connect the existing split-level sidewalk to the rest of Elsie street sidewalk and create a continuous travel path, which currently does not exist. The project includes the following components:

- Constructing a driveway in the public right-of-way to provide vehicular access to 152 Elsie Street (Assessor's Block 5618, Lot 020);
- The new driveway would be elevated. Sidewalk ramps constructed on each side of the driveway, not exceed a 1:12 slope, conforming with ADA access standards (fronting lots 021 and 019).

- The project would remove a 13'-6" long portion of an existing retaining wall in a portion of the public right-of-way fronting lot 021. The sidewalk would therefore be elevated in the public right-of-way fronting lots 021, 020 and 019.
- The project would improve the sidewalk fronting lots 018 and 019, building a new set of new concrete stairs to allow for the grade differences in front of lots 018 and 019
- The project would build a new sidewalk on the upper level of the split-level sidewalk, fronting lot 019. This would result in a continuous travel path from lot 017 through lot 021, which currently does not exist.
- All proposed improvements are shown in *Attachment 1*.

Although the lower portion of the split-level sidewalk fronting lots 018 and 019 would remain inaccessible to wheelchair use, the new elevated-sidewalk and improved upper level (fronting lot 019) would enhance and extend pedestrian access to the public right-of-way and adjacent private homes on Elsie Street.

The project would not only provide vehicular access to AB 5618, lot 020, but also improve pedestrian access in the public right-of-way on this portion of Elsie Street. The revised design helps the proposed driveway blend into the street pattern and shows the property owner's willingness to consider the impact of the proposed project in the neighborhood and on the community at large. The Department is pleased to see several improvements to the current design.

Based on the information provided by the project sponsor and by DPW, the Planning Department determines that the attached modified Major Encroachment Permit proposal would be, on balance in-conformity with the General Plan, as described in the attached Case Report. *Attachment 2*.

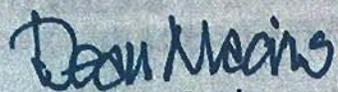
#### Environmental Review

On March 16, 2006, the Major Environmental Analysis Division of the Planning Department determined that the proposed project is categorically exempt from CEQA review under Class 1(c) and 4(b) of State Environmental Review Guidelines.

#### Planning Code Section 101.1 Policies

The proposal has been found consistent with the Eight Priority Policies of Section 101.1 of the Planning Code Section 101.1 and the findings are included as *Attachment 3*

Sincerely,

  
 Dean L. Macris  
 Director of Planning

Attachments: 1. DPW drawings  
 2. Case Report  
 3. Planning Code Section 101.1 Findings

cc: J.Banales, Planning Department  
 S.Soto-Grondona, Planning Department  
 S.Shatar, Project Architect/Contact



RESIDENCE

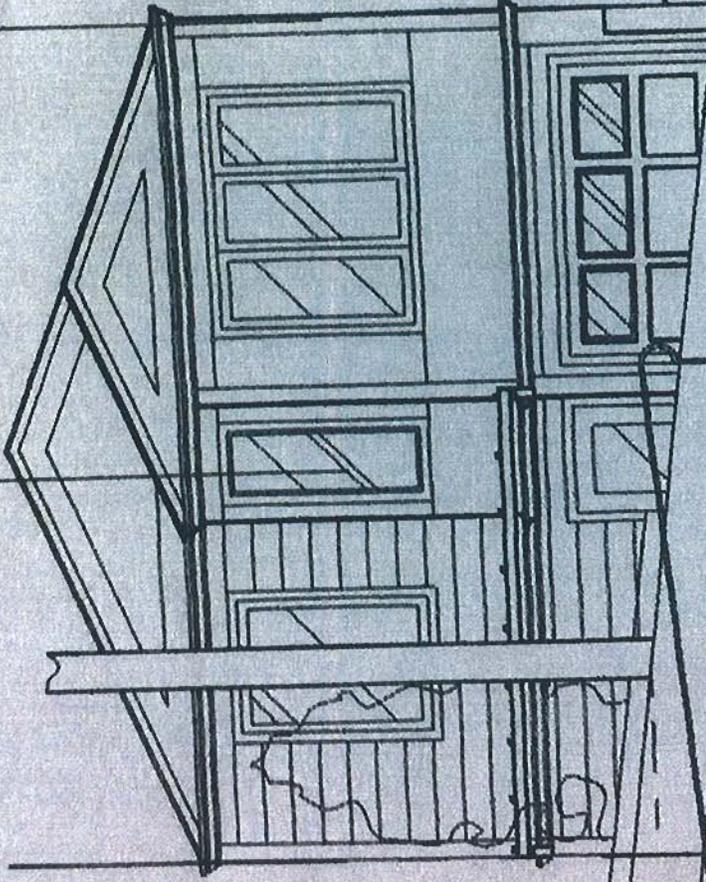
RESIDENCE

LOT 021  
RESIDENCE

AREA OF "PATH OF TRAVEL" LEVEL  
ACROSS DRIVEWAY W/2" PITCH

R

R



(E) SIDEWALK  
TO REMAIN

(E) SIDEWALK  
BELOW (N)  
RAMP AND  
DRIVEWAY

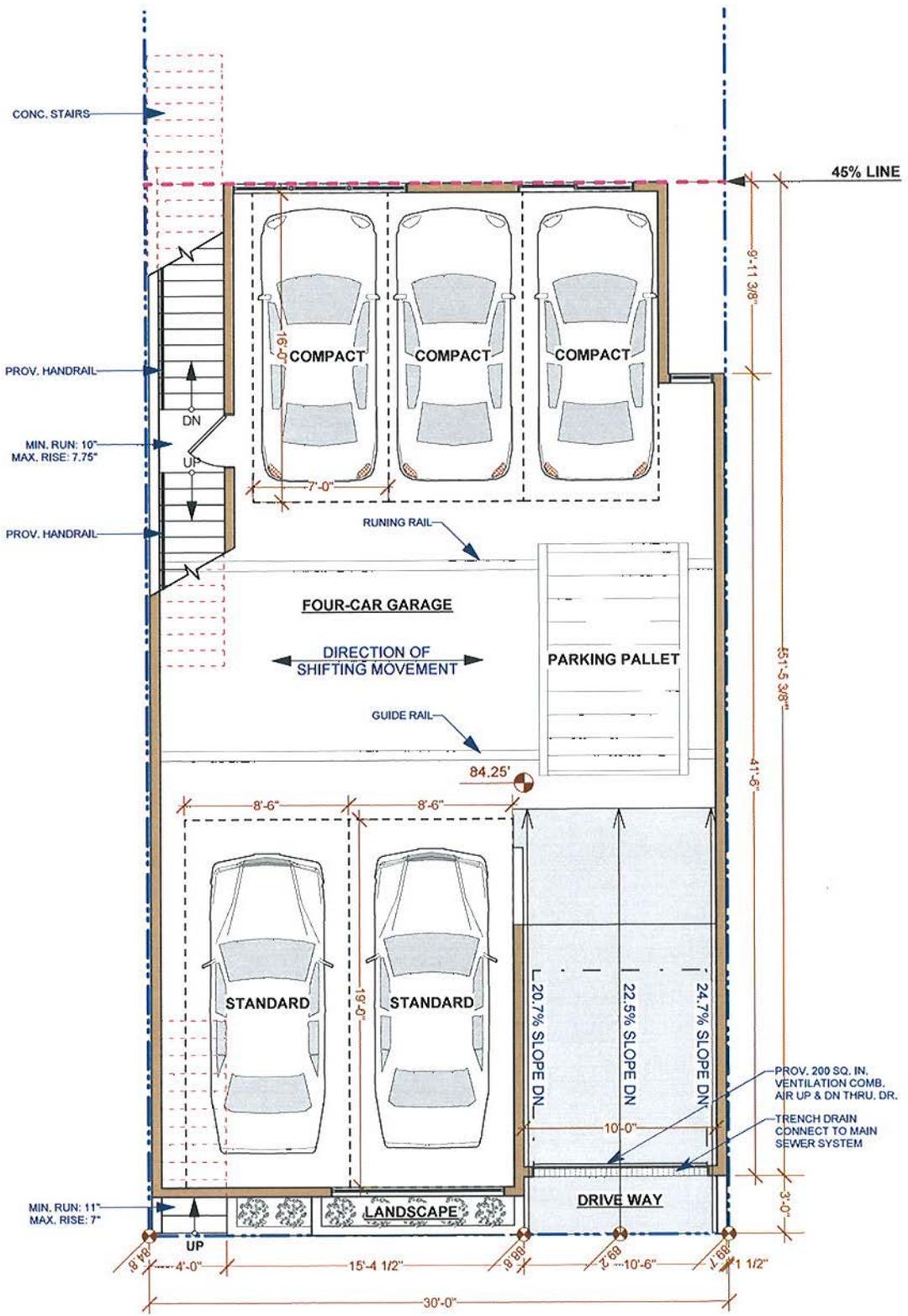
PROPOSED  
DRIVEWAY  
RAMP 1:50

(N) RAMP 1:12  
TYP.

(E) DRAIN  
W/ACCESS  
PANEL &  
(N) RAMP

(N)







## **TWO REMAINING DIFFERENCES BETWEEN DESIGN PROPOSED BY THE NEIGHBORHOOD'S DESIGN REVIEW BOARD, AND PROPERTY OWNER**

Although you may have seen a letter from the applicable Bernal Heights Review Board with a number of great concerns, that letter was prepared well before a number of recent client concessions. *As a result, our client believes that you will receive an undated letter before the hearing, one which will show only the following additional changes to the project being sought by the Design Review Board:*

- (1) increase the setback on the front of the lower building at 60-62 Peralta from 7 feet to roughly 15 feet

OUR RESPONSE: **a.** eliminating the third bedroom and reducing the building to about 19 feet in height at the front, in a forty foot height district, is quite significant; **b.** this will accommodate an unprotected potential view loss for neighbors across the street, but will not give significantly less shadow to neighbors on Hampshire Street. Those neighbors properties buildings have approximately 20 feet in separation from our client's proposed lower building, a setback much greater than most San Franciscan's have. **c.** our client could accommodate the request if the top floor is shifted to the rear of the lot so that there is more setback at the front and less than the current setback of 16' 2" at the rear. See Attachment To This Page.

- (2) Increase the number of parking spaces from four to seven, the code-required amount.

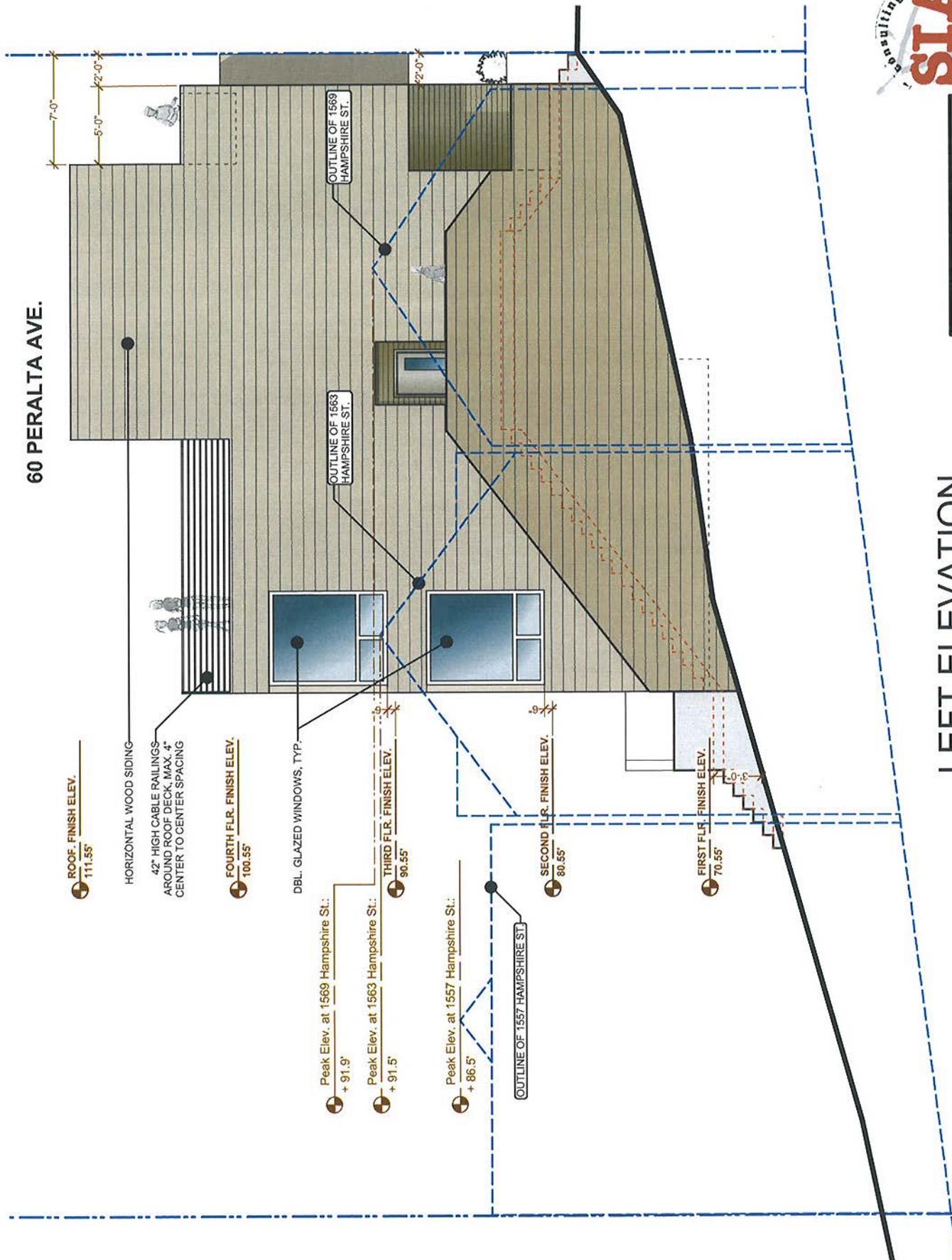
OUR RESPONSE: Our client has responded to this concern by recently making the project one that has five independently accessible parking spaces, but my client cannot accommodate seven without creating the major hillside excavation that neighbors want my client to avoid, and without adding very significant expense to the cost of construction and reduce the units' affordability.



60 PERALTA AVE.  
SAN FRANCISCO, CA

# LEFT ELEVATION

1/8" = 1'-0"



60 PERALTA AVE.



**Downhill:**

**60-62 Peralta Ave (Block 5512 / Lot 031)**

- Lot Dimensions: 30' wide x 74' deep (2,220 square feet)
- RH-2 zoning
- 40-X height district
- 45% rear yard provided
- No garage
- Building Dimensions
  - o 3-stories at front
  - o 7-foot setback at top floor at front of building and 16' 2" at rear
- 60 Peralta (lower unit)
  - o 3 bedrooms; 3 baths
  - o 1,728 square feet
- 62 Peralta (upper unit)
  - o 3 bedrooms; 2 baths
  - o 1,538 square feet

**Uphill:**

**54-56 Peralta Ave (Block 5512 / Lot 029)**

- Lot Dimensions: 30' wide x 99' deep (2,970 square feet)
- RH-2 zoning
- 40-X height district
- 45% rear yard provided
- 5 car garage (3 compact & 2 standard)
- Building Dimensions
  - o 2-stories over garage at front
  - o 7-foot setback at top floor at front of building and 9' 11" and 19' 6" at rear levels
- 54 Peralta (front unit facing street)
  - o 2 bedrooms; 2.5 baths
  - o 1,057 square feet
- 56 Peralta (rear unit facing yard)
  - o 3 bedrooms; 3 baths
  - o 1,249 square feet



Neighbors building at 48 Peralta Ave

Proposed building at 54-56 Peralta Ave

Proposed building at 60-62 Peralta Ave

DR applicant at 1569 Hampshire St



Neighbors building at 48 Peralta Ave

Proposed building at 54-56 Peralta Ave

Proposed building at 60-62 Peralta Ave



Neighbors building  
at 48 Peralta Ave

Proposed building at  
54-56 Peralta Ave

Proposed building at  
60-62 Peralta Ave



Neighbors building  
at 48 Peralta Ave

Proposed building at  
54-56 Peralta Ave

Proposed building at  
60-62 Peralta Ave

DR Applicant at  
1569 Hampshire St

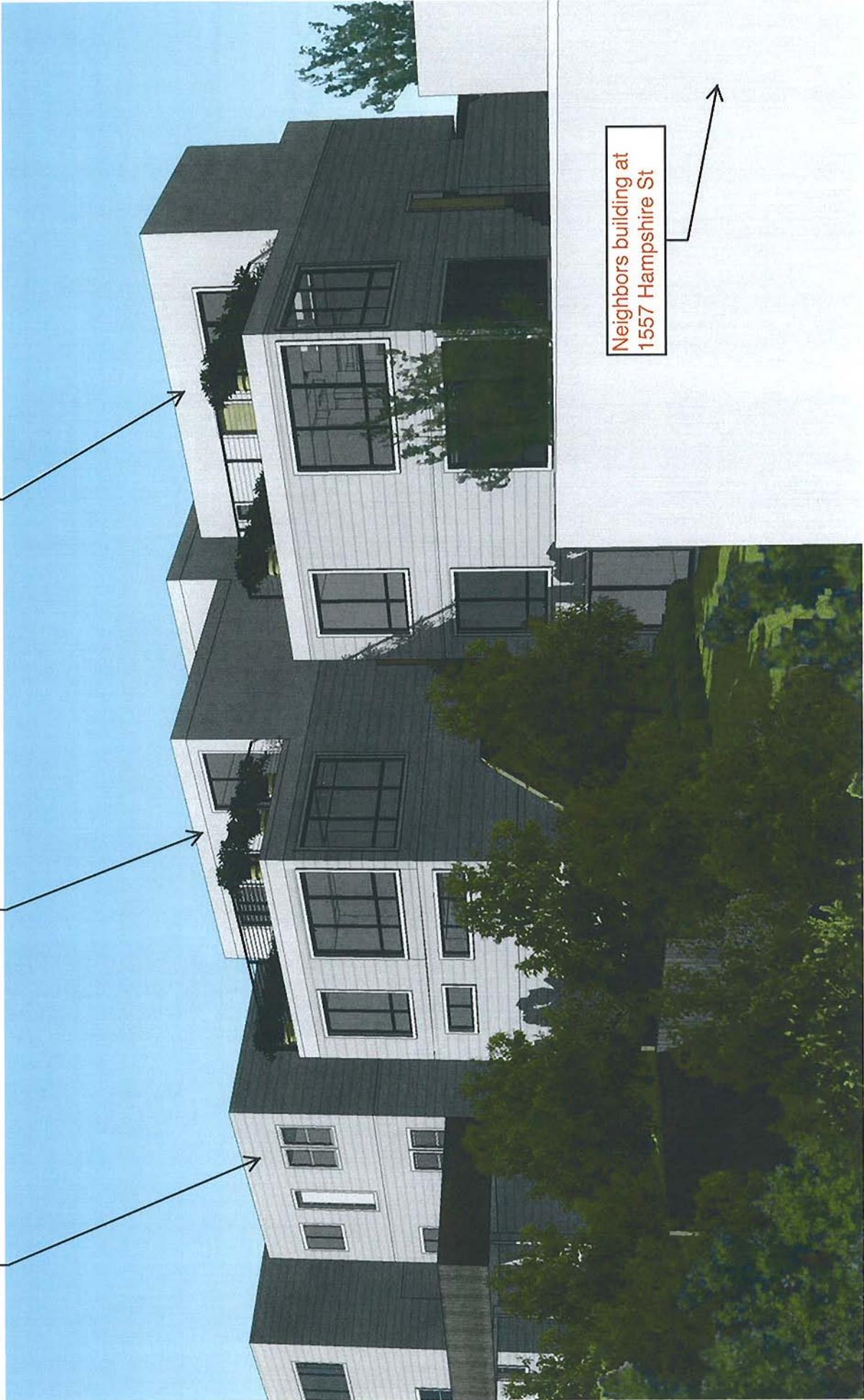


Proposed building at  
60-62 Peralta Ave

Proposed building at  
54-56 Peralta Ave

Neighbors building  
at 48 Peralta Ave

Neighbors building at  
1557 Hampshire St



Neighbors building  
at 48 Peralta Ave

Proposed building at  
54-56 Peralta Ave

Proposed building at  
60-62 Peralta Ave

DR Applicant at  
1569 Hampshire St



DR Applicant at  
1569 Hampshire St.

Proposed building at  
60-62 Peralta Ave

Proposed building at  
54-56 Peralta Ave

Neighbors building  
at 48 Peralta Ave



Neighbors building  
at 48 Peralta Ave

Proposed building at  
54-56 Peralta Ave

Proposed building at  
60-62 Peralta Ave

DR Applicant at  
1569 Hampshire St



Neighbors building at  
1557 Hampshire St

View of proposed buildings near sidewalk across street  
(they appear like 2 story buildings)

60-62 Peralta Ave

54-56 Peralta Ave





# SAN FRANCISCO PLANNING DEPARTMENT

## RESPONSE TO DISCRETIONARY REVIEW

Case No.: \_\_\_\_\_  
Building Permit No.: 2009.12.31.4050  
Address: 54-56 Peralta Ave

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
415.558.6378

Fax:  
415.558.6409

Planning  
Information:  
415.558.6377

Project Sponsor's Name: Thomas Aquilina c/o Brett Gladstone, Gladstone & Associates  
Telephone No.: 415.434.9500 (Brett Gladstone) (for Planning Department to contact)

1. Given the concerns of the DR requester and other concerned parties, why do you feel your proposed project should be approved? (If you are not aware of the issues of concern to the DR requester, please meet the DR requester in addition to reviewing the attached DR application.

Please see letter to Planning Commission dated February 17, 2011

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2. What alternatives or changes to the proposed project are you willing to make in order to address the concerns of the DR requester and other concerned parties? If you have already changed the project to meet neighborhood concerns, please explain those changes. Indicate whether the changes were made before filing your application with the City or after filing the application.

Please see letter to Planning Commission dated February 17, 2011

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3. If you are not willing to change the proposed project or pursue other alternatives, please state why you feel that your project would not have any adverse effect on the surrounding properties. Please explain your needs for space or other personal requirements that prevent you from making the changes requested by the DR requester.

Please see letter to Planning Commission dated February 17, 2011

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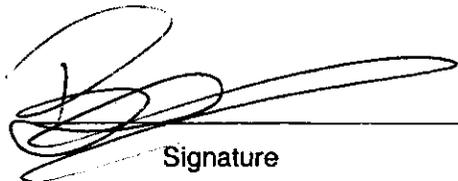
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If you have any additional information that is not covered by this application, please feel free to attach additional sheets to this form.

4. Please supply the following information about the proposed project and the existing improvements on the property.

<u>Number of</u>	<u>Existing</u>	<u>Proposed</u>
Dwelling units (only one kitchen per unit –additional kitchens count as additional units) .....	<u>vacant</u>	<u>2 units</u>
Occupied stories (all levels with habitable rooms) ...	<u>vacant</u>	<u>2 stories over garage</u>
Basement levels (may include garage or windowless storage rooms) .....	<u>vacant</u>	<u>unoccupied crawl space</u>
Parking spaces (Off-Street) .....	<u>vacant</u>	<u>5 spaces total</u> <small>(2 or 3 dedicated to 60-62 Peralta; to be determined)</small>
Bedrooms .....	<u>vacant</u>	<u>54 Peralta: 2 bedrooms</u> <u>56 Peralta: 3 bedrooms</u>
Gross square footage (floor area from exterior wall to exterior wall), not including basement and parking areas....	<u>vacant</u>	<u>approx. 2,306 sq. ft.</u>
Height .....	<u>vacant</u>	<u>approx. 27'</u>
Building Depth .....	<u>vacant</u>	<u>approx. 54'-6"</u>
Most recent rent received (if any) .....	<u>none</u>	_____
Projected rents after completion of project .....	<u>unknown</u>	<u>unknown</u>
Current value of property .....	<u>unknown</u>	<u>unknown</u>
Projected value (sale price) after completion of project (if known) .....	<u>unknown</u>	<u>unknown</u>

I attest that the above information is true to the best of my knowledge.

 _____ Signature	2.18.11 _____ Date	Brett Gladstone _____ Name (please print)
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# SAN FRANCISCO PLANNING DEPARTMENT

## RESPONSE TO DISCRETIONARY REVIEW

Case No.: \_\_\_\_\_

Building Permit No.: 2009.12.31.4052

Address: 60-62 Peralta Ave

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
415.558.6378

Fax:  
415.558.6409

Planning  
Information:  
415.558.6377

Project Sponsor's Name: Thomas Aquilina c/o Brett Gladstone, Gladstone & Associates

Telephone No.: 415.434.9500 (Brett Gladstone) (for Planning Department to contact)

- Given the concerns of the DR requester and other concerned parties, why do you feel your proposed project should be approved? (If you are not aware of the issues of concern to the DR requester, please meet the DR requester in addition to reviewing the attached DR application.

Please see letter to Planning Commission dated February 17, 2011

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- What alternatives or changes to the proposed project are you willing to make in order to address the concerns of the DR requester and other concerned parties? If you have already changed the project to meet neighborhood concerns, please explain those changes. Indicate whether the changes were made before filing your application with the City or after filing the application.

Please see letter to Planning Commission dated February 17, 2011

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- If you are not willing to change the proposed project or pursue other alternatives, please state why you feel that your project would not have any adverse effect on the surrounding properties. Please explain your needs for space or other personal requirements that prevent you from making the changes requested by the DR requester.

Please see letter to Planning Commission dated February 17, 2011

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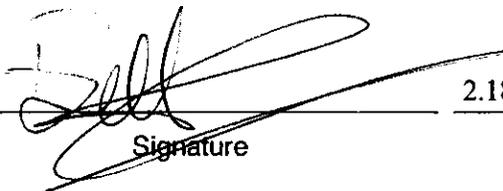
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If you have any additional information that is not covered by this application, please feel free to attach additional sheets to this form.

4. Please supply the following information about the proposed project and the existing improvements on the property.

<u>Number of</u>	<u>Existing</u>	<u>Proposed</u>
Dwelling units (only one kitchen per unit –additional kitchens count as additional units) .....	<u>vacant</u>	<u>2 units</u>
Occupied stories (all levels with habitable rooms) ...	<u>vacant</u>	<u>4 stories total</u> <u>(3 stories at street)</u>
Basement levels (may include garage or windowless storage rooms) .....	<u>vacant</u>	<u>none</u>
Parking spaces (Off-Street) .....	<u>vacant</u> 2 or 3	<u>spaces provided off-site at 54-56 Peralta</u> <u>(none provided on-site)</u>
Bedrooms .....	<u>vacant</u>	<u>60 Peralta: 3 bedrooms</u> <u>62 Peralta: 3 bedrooms</u>
Gross square footage (floor area from exterior wall to exterior wall), not including basement and parking areas....	<u>vacant</u>	<u>approx. 3,273 sq. ft.</u>
Height .....	<u>vacant</u>	<u>approx. 29'</u>
Building Depth .....	<u>vacant</u>	<u>approx. 41'</u>
Most recent rent received (if any) .....	<u>none</u>	_____
Projected rents after completion of project .....	<u>unknown</u>	<u>unknown</u>
Current value of property .....	<u>unknown</u>	<u>unknown</u>
Projected value (sale price) after completion of project (if known) .....	<u>unknown</u>	<u>unknown</u>

I attest that the above information is true to the best of my knowledge.

	2.18.11	Brett Gladstone
Signature	Date	Name (please print)

M. BRETT GLADSTONE

GLADSTONE & ASSOCIATES  
ATTORNEYS AT LAW  
PENTHOUSE, 177 POST STREET  
SAN FRANCISCO, CALIFORNIA 94108

TELEPHONE (415) 434-9500  
FACSIMILE (415) 394-5188  
admin@gladstoneassociates.com

February 17, 2011

**BY HAND DELIVERY**

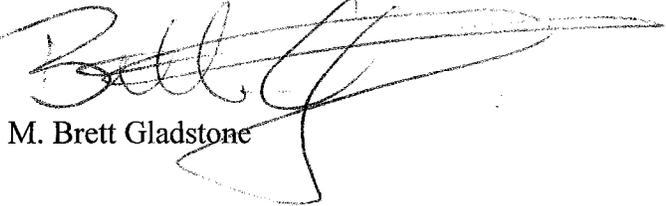
President Ron Miguel  
Planning Commission  
1650 Mission Street, Suite 400  
San Francisco, CA 94103

Re: 54-56 and 60-62 Peralta Avenue  
Response to Discretionary Review Request

Dear President Miguel:

Attached are the plans for each of the two proposed new buildings.

Very truly yours,



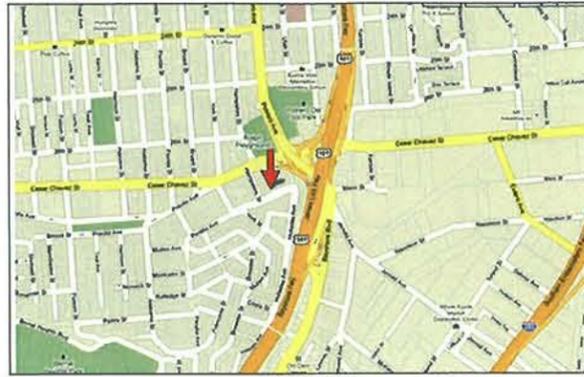
M. Brett Gladstone

Enclosure

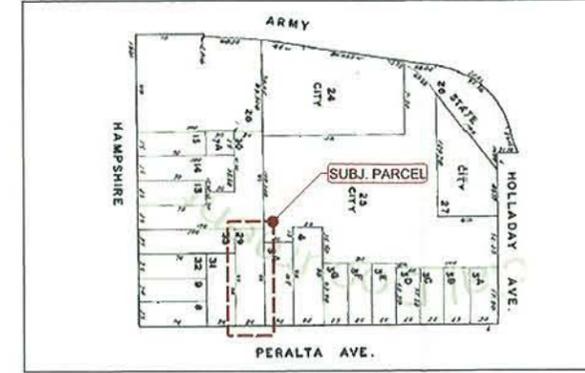
cc: Tom Aquilina, owner  
Reza Khoshnevisan, architect  
Bob Besso, Appellant  
Design Review Board  
Zoning Administrator  
Director John Rahaim

**SCOPE OF WORK:**

PROPOSED NEW TWO-UNIT BUILDING WITH FOUR-CAR GARAGE  
@ 54-56 PERALTA AVE., SAN FRANCISCO, CA



AREA MAP



ASSESSOR'S MAP

PROJECT NAME  
**54-56 PERALTA AVE**  
SAN FRANCISCO, CA



SIA CONSULTING CORPORATION  
1256 HOWARD STREET  
SAN FRANCISCO CA 94103  
TEL: (415) 922.0200  
FAX: (415) 922.0203  
WEBSITE: WWW.SIACONSULT.COM

**GENERAL NOTES:**

1. ALL WORK SHALL BE PERFORMED IN COMPLETE COMPLIANCE WITH ALL APPLICABLE CODES, LAWS, ORDINANCES AND REGULATIONS OF ALL AUTHORITIES HAVING JURISDICTION OVER THE WORK. ALL CONTRACTORS SHALL HOLD HARMLESS THE ARCHITECT/ENGINEER AND THE OWNER FROM ALL DAMAGES AND/OR PENALTY ARISING OUT OF VIOLATION THEREOF.
2. ALL ATTACHMENTS, CONNECTIONS OR FASTENING OF ANY NATURE ARE TO BE PROPERLY AND PERMANENTLY SECURED IN CONFORMANCE WITH THE BEST PRACTICE OF THE BUILDING INDUSTRY. DRAWINGS SHOWS ONLY SPECIAL REQUIREMENTS TO ASSIST THE CONTRACTOR AND DO NOT ILLUSTRATE EVERY DETAIL.
3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, MEASUREMENTS AND CONDITIONS IN THE FIELD BEFORE BEGINNING WORK. ANY DISCREPANCIES, ERROR OR OMISSIONS SHALL BE BROUGHT TO THE ARCHITECT/ENGINEER'S ATTENTION IMMEDIATELY.
4. UNLESS OTHERWISE NOTED, ALL ANGLES SHALL BE RIGHT ANGLES, ALL LINES WHICH APPEAR PARALLEL SHALL BE PARALLEL, AND ALL ITEMS WHICH APPEAR CENTERED SHALL BE CENTERED. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL LINES TRUE LEVEL, PLUMB AND SQUARE.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SHORING AND PROTECTION DURING CONSTRUCTION. ALL EXISTING IMPROVEMENTS TO REMAIN SHALL BE PROTECTED. ALL MATERIALS DELIVERED TO THE SITE SHALL BE PROPERLY STORED AND PROTECTED UNTIL INSTALLATION. ALL LUMBER SHALL BE PROTECTED FROM MOISTURE AND STORED ABOVE GROUND.
6. DETAILED AND/OR LARGER SCALE DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL AND SMALLER SCALE DRAWINGS. FIGURED DIMENSIONS SHALL TAKE PRECEDENCE OVER SCALED DIMENSIONS. ALL SCALED DIMENSIONS SHALL BE VERIFIED.
7. ALL WORK SHALL BE DONE UNDER PERMIT. PLANS AND CALCULATIONS, IF REQUIRED, SHALL BE SUBMITTED TO AND APPROVED BY THE BUILDING DEPARTMENT. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS.

**ABBREVIATION**

&	AND	F.D.	FLOOR DRAIN
@	A T	FDN	FOUNDATION
A.B.	ANCHOR BOLT	FL	FLOOR
ABV	ABOVE	FLUOR	FLUORESCENT
A.F.F.	ABOVE FINISHED FLOOR	F.O.F.	FACE OF FINISH
ALT	ALTERNATE	FURR	FURRING
ALUM	ALUMINUM	G.B.	GRAB BAR
APPROX	APPROXIMATE	GND	GROUND
ASPH	ASPHALT	GRP	GROUP
BD	BOARD	G YP	GYP SUM
BLDG	BUILDING	H.C.	HANDICAPPED
BLKG	BLOCKING	HR	HOUR
BOT	BOTTOM	MAX	MAXIMUM
CLG	CEILING	MIN.	MINIMUM
CLO	CLOSET	(N)	NEW
CLR	COUNTER	N.T.S.	NOT TO SCALE
CNTR	CLEAR	O.C.	ON CENTER
CONC	CONCRETE	OFF	OFFICE
CORR	CORRIDOR	P.L.	PROPERTY LINE
CTR	CENTER	PLYWD	PLYWOOD
DBL	DOUBLE	RDWD	REDWOOD
DET	DETAIL	REQD	REQUIRED
D.F.	DRINKING FOUNTAIN	SF	SQUARE FOOT
DIA	DIAMETER	STD	STANDARD
DN	DOWN	STRUCT	STRUCTURAL
DR	DOOR	SQ	SQUARE
DWR	DRAWING	TC	TOP OF CURB
(E)	EXISTING	TYP	TYPICAL
EA	EACH	U.N.O	UNLESS NOTED OTHERWISE
EL	ELEVATION	V.I.F.	VERIFY IN FIELD
ELEC	ELECTRICAL	W.H.	WATER HEATER
EQ	EQUAL	W/	WITH
EXT.	EXTERIOR	WD	WOOD

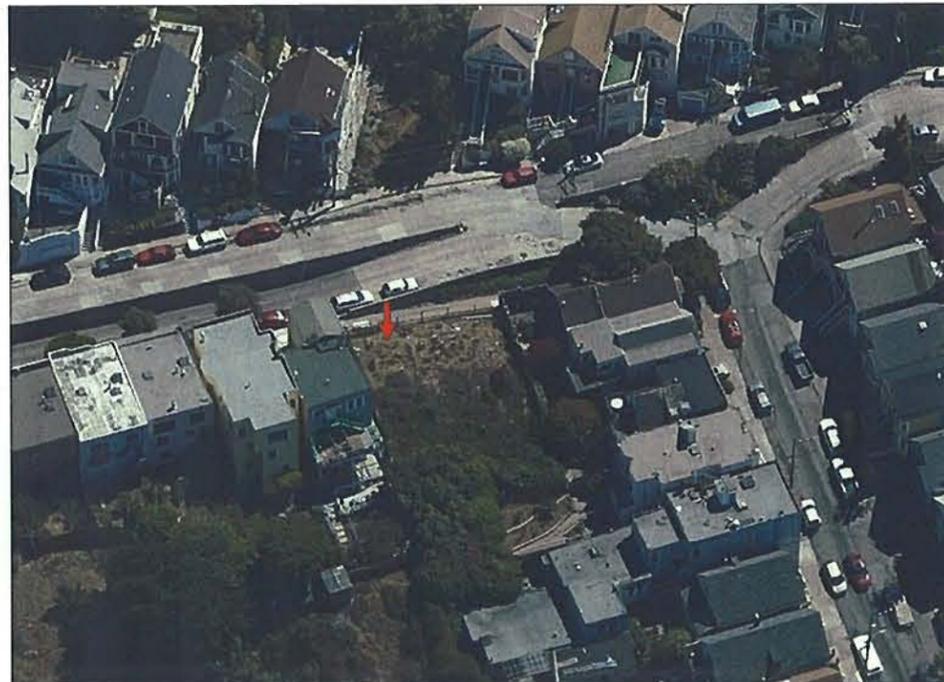
**PROJECT DATA**

LOT AREA :	2,970 S.F.
FIRST FLOOR AREA (GARAGE):	1,438 S.F.
SECOND FLOOR AREA:	1,336 S.F.
THIRD FLOOR AREA:	970 S.F.
TOTAL FLOOR AREA (INCL. GARAGE):	3,744 S.F.
54 PERALTA AVE:	
FLOOR AREA:	1,057 S.F.
OPEN SPACE:	85 S.F.
56 PERALTA AVE:	
FLOOR AREA:	1,249 S.F.
OPEN SPACE:	189 S.F.
COMMON OPEN SPACE(BACK YARD):	1,336 S.F.
NUMBER OF STORIES:	2 OVER GARAGE
NUMBER OF UNITS:	2
NUMBER OF COVERED PARKING:	4
BUILDING HEIGHT:	28'-9"
ZONING DISTRICT:	RH-2
OCCUPANCY GROUP:	R-3
TYPE OF CONSTRUCTION:	V A
A.P.N. :	5512-029
APPLICABLE CODES:	2007 CALIFORNIA CODE EDITIONS W/ SAN FRANCISCO AMENDMENTS

**SHEET TITLE**

**COVER SHEET**

**AERIAL PHOTO**



**APPENDIX:**

A-0.0	COVER SHEET
A-1.0	SITE PLAN & NOTES
A-2.0	FIRST & SECOND FLOOR PLANS
A-2.1	THIRD FLOOR PLAN, & ROOF PLAN
A-3.0	FRONT ELEVATION
A-3.1	REAR ELEVATION, & WINDOW DETAILS
A-3.2	LEFT & RIGHT ELEVATIONA
A-4.0	SECTION A-A
C-1	SURVEY

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ISSUES / REVISIONS		
NO.	DATE	DESCRIPTION

DRAWN	R.L.
CHECKED	R.K.
DATE	05/29/08
REVISED DATE	08/26/10
JOB NO.	08-1364A
SHEET NO.	<b>A-0.0</b>

**ELECTRICAL NOTES:**

ELECTRICAL SUBPANEL(S) ON FLOOR PLAN(S). PANELS SHALL NOT BE LOCATED IN THE VICINITY OF EASILY IGNITABLE MATERIAL(S) SUCH AS CLOTHES CLOSETS. PANELS IN FIREWALL SHALL BE RELOCATED OR PROPERLY PROTECTED TO MAINTAIN FIREWALL SEPARATION.

GFCI PROTECTED OUTLETS AT THE FOLLOWING LOCATIONS.  
 (A) GARAGE  
 (B) UNFINISHED BASEMENT, CRAWL AND STORAGE SPACES.  
 (C) WITHIN 6' OF SINK OR BASIN  
 (D) EXTERIOR (WATERPROOF)

RECEPTACLE OUTLETS AT THE FOLLOWING LOCATIONS.  
 (A) 12" O.C. MAX, AND WITHIN 6' OF THE END OF WALLS.  
 (B) ANY WALL SPACE 2 OR MORE FEET WIDE.  
 (C) AT EACH KITCHEN AND DINING AREA COUNTER SPACE WIDER THAN 12", SO THAT NO POINT IN ANY HALLWAY 10 FEET OR MORE IN LENGTH.

LIGHT FIXTURE IN TUB OR SHOWER ENCLOSURES AND EXTERIOR LIGHT FIXTURES SHALL BE LABELED "SUITABLE FOR DAMP LOCATIONS"

APPLIANCES FASTENED IN PLACE, SUCH AS DISHWASHERS, GARBAGE DISPOSALS, TRASH COMPACTORS, MICROWAVE OVENS, ETC., SHALL BE SUPPLIED BY A SEPARATE BRANCH CIRCUIT RATED FOR THE APPLIANCE OR LOAD SERVED.

RECEPTACLES FOR FIXED APPLIANCES SHALL BE ACCESSIBLE, NOT BEHIND APPLIANCE.

A CIRCUIT SUITABLE FOR THE LOAD WITH A MINIMUM OF 30 AMPERES IS REQUIRED FOR AN ELECTRIC CLOTHES DRYER.

LIGHT FIXTURES IN TUB OR SHOWER ENCLOSURES SHALL BE LABELED "SUITABLE FOR DAMP LOCATION(S)".

**ENERGY NOTES:**

PERMANENTLY INSTALLED LUMINAIRES IN KITCHENS SHALL BE HIGH EFFICACY LUMINAIRES. UP TO 50% OF WATTAGE OF PERMANENTLY INSTALLED LUMINAIRES IN KITCHENS MAY BE IN LUMINAIRES THAT ARE NOT HIGH EFFICACY LUMINAIRES, PROVIDED THAT THESE LUMINAIRES ARE CONTROLLED BY SWITCHES SEPARATE FROM THOSE CONTROLLING THE HIGH EFFICACY LUMINAIRES.

EACH ROOM CONTAINING A WATER CLOSET SHALL HAVE AT LEAST ONE LUMINAIRE WITH LAMPS WITH AN EFFICACY OF NOT LESS THAN 40 LUMENS PER WATT FOR 15 WATT OR SMALLER, 50 LUMENS PER WATT FOR 16 WATT-40 WATT, & 60 LUMENS PER WATT FOR 40 WATT OR HIGHER. IF THERE IS MORE THAN ONE LUMINAIRE IN THE ROOM, THE HEIGHT EFFICACY LUMINAIRE SHALL BE SWITCHED AT AN ENTRANCE TO THE ROOM.

ALL LIGHTING FIXTURES RECESSED INTO INSULATED CEILINGS MUST BE APPROVED FOR ZERO-CLEARANCE INSULATION COVER (I.C.) OR AIR TIGHT (AT) RATED BY UNDERWRITERS LABORATORIES OR OTHER APPROVED LABORATORIES.

PROVIDE FLUORESCENT FIXTURES FOR BATHROOMS, LAUNDRY, UTILITY ROOMS AND GARAGES, OR A MANUAL ON / OCCUPANCY SENSOR CONTROL FOR ALL INCANDESCENT FIXTURES (DIMMERS DO NOT QUALIFY)

PROVIDE FLUORESCENT FIXTURES FOR ALL ROOMS, INCLUDING CLOSETS 70 SQ. FT. OR MORE (OTHER THAN KITCHEN, BATHROOM, LAUNDRY, UTILITY ROOM AND GARAGES), OR PROVIDE OCCUPANCY SENSORS OR DIMMERS.

FIREPLACES, DECORATIVE GAS APPLIANCES AND GAS LOGS: INSTALLATION OF FACTORY-BUILT AND MASONRY FIREPLACES SHALL INCLUDE:

- (A) CLOSABLE METAL OR GLASS DOORS.
- (B) COMBUSTION AIR INTAKE (6 SQ. IN. MINIMUM) TO DRAW AIR FROM OUTSIDE OF THE BUILDING DIRECTLY INTO FIRE BOX. THE COMBUSTION AIR INTAKE MUST BE EQUIPPED WITH A READILY ACCESSIBLE, OPERABLE AND LIGHT-FITTING DAMPER OR COMBUSTION AIR CONTROL DEVICE.  
 EXCEPTION: AN OUTSIDE COMBUSTION AIR INTAKE IS NOT REQUIRED IF THE FIREPLACE IS INSTALLED OVER CONCRETE SLAB FLOORING AND THE FIREPLACE IS NOT LOCATED ON AN EXTERIOR WALL.
- (C) A FLUE DAMPER WITH AN READILY ACCESSIBLE CONTROL.  
 EXCEPTION: WHEN A GAS LOG, LOG LIGHTER, OR DECORATIVE GAS APPLIANCE IS INSTALLED IN A FIREPLACE, THE FLUE DAMPER SHALL BE BLOCKED OPEN IF REQUIRED BY THE MANUFACTURER'S INSTALLATION INSTRUCTIONS OR THE STATE MECHANICAL CODE.

**PLUMBING AND MECHANICAL NOTES:**

HEATING SUPPLY AIR DUCTS SHALL BE MIN. NO. 26 GA. GALVANIZED SHEET METAL WITH SEALED ANNULAR OR A FIRE DAMPER PROVIDED WHEN THE DUCTS PENETRATE THE OCCUPANCY SEPARATION BETWEEN THE GARAGE AND THE HOUSE.

PLASTIC PLUMBING PIPE SHALL NOT BE USED FOR DOMESTIC WATER SUPPLY AND/OR SANITARY WASTE SYSTEM

SMOOTH METAL DUCT FOR DRYER EXHAUST EXTENDING TO OUTSIDE.

NON-REMOVABLE BACKFLOW PREVENTION DEVICES ON ALL EXTERIOR HOSE BIBS.

SIZE OF WATER CLOSETS. MAXIMUM ALLOWABLE 1.6 GALLONS PER FLUSH.

SHOWER & TUB/SHOWERS SHALL BE PROVIDED WITH PRESSURE BALANCE OR THERMOSTATIC MIXING VALVE CONTROLS. HANDLE POSITION STOPS SHALL BE PROVIDED ON SUCH VALVES AND SHALL BE ADJUSTED PER MANUFACTURER'S INSTRUCTIONS TO DELIVER A MAXIMUM MIXED WATER SETTING OF 120 DEGREES F. THE WATER HEATER THERMOSTAT SHALL NOT BE CONSIDERED A SUITABLE CONTROL FOR MEETING THIS PROVISION, U.P.C. 4107.

DOORS & PANELS OF SHOWERS AND BATHTUBS ENCLOSURES AND ADJACENT WALL OPENINGS WITHIN 60" ABOVE A STANDING SURFACE AND DRAIN INLET SHALL BE FULLY TEMPERED, LAMINATED SAFETY GLASS OR APPROVED PLASTIC.

TEMPERED GLASS SHALL BE AFFIXED WITH A PERMANENT LABEL.

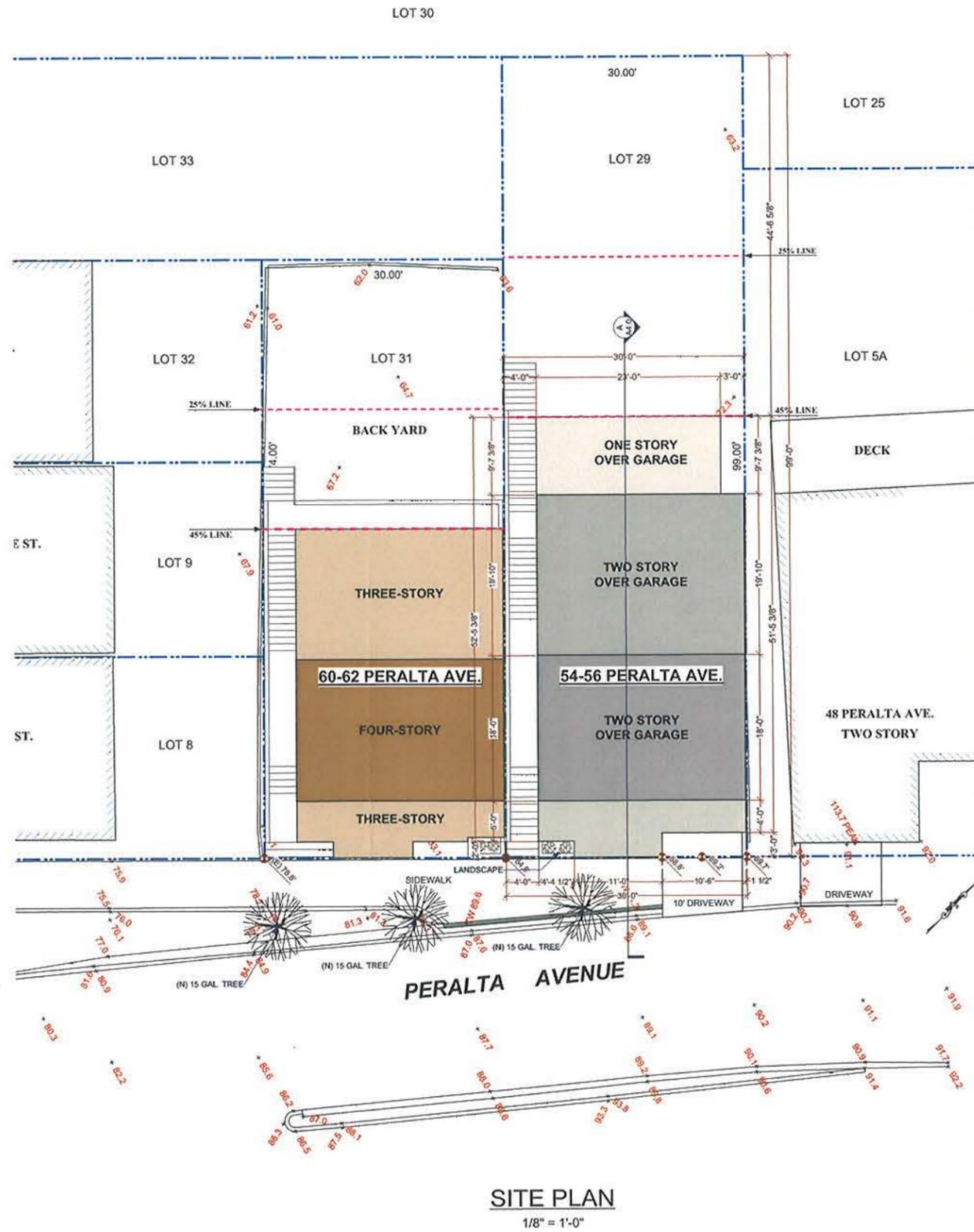
**SANITATION NOTES:**

SHOWER STALL FINISH SHALL BE CERAMIC TILE EXTENDING 70 INCHES ABOVE THE DRAIN INLET

MOISTURE RESISTANT UNDERLAYMENT (e.g. WATER RESISTANT GYP. BD.) TO A HEIGHT OF 70 INCHES ABOVE THE DRAIN INLET U.B.C. 8067.1.3.

**BEDROOM WINDOWS:**

MIN. OPENABLE AREA TO BE 5.7 S.F., MIN WIDTH: 20" MIN HEIGHT: 24" AND MAX SILL HT: 44"



SIA CONSULTING CORPORATION  
 1256 HOWARD STREET  
 SAN FRANCISCO CA 94103  
 TEL: (415) 922.0200  
 FAX: (415) 922.0203  
 WEBSITE: WWW.SIACONSULT.COM

SHEET TITLE

**SITE PLAN & NOTES**

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ISSUES / REVISIONS		
NO.	DATE	DESCRIPTION

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SHEET NO.	

**A-1.0**



SIA CONSULTING CORPORATION  
 1256 HOWARD STREET  
 SAN FRANCISCO CA 94103  
 TEL: (415) 922.0200  
 FAX: (415) 922.0203  
 WEBSITE: WWW.SIACONSULT.COM

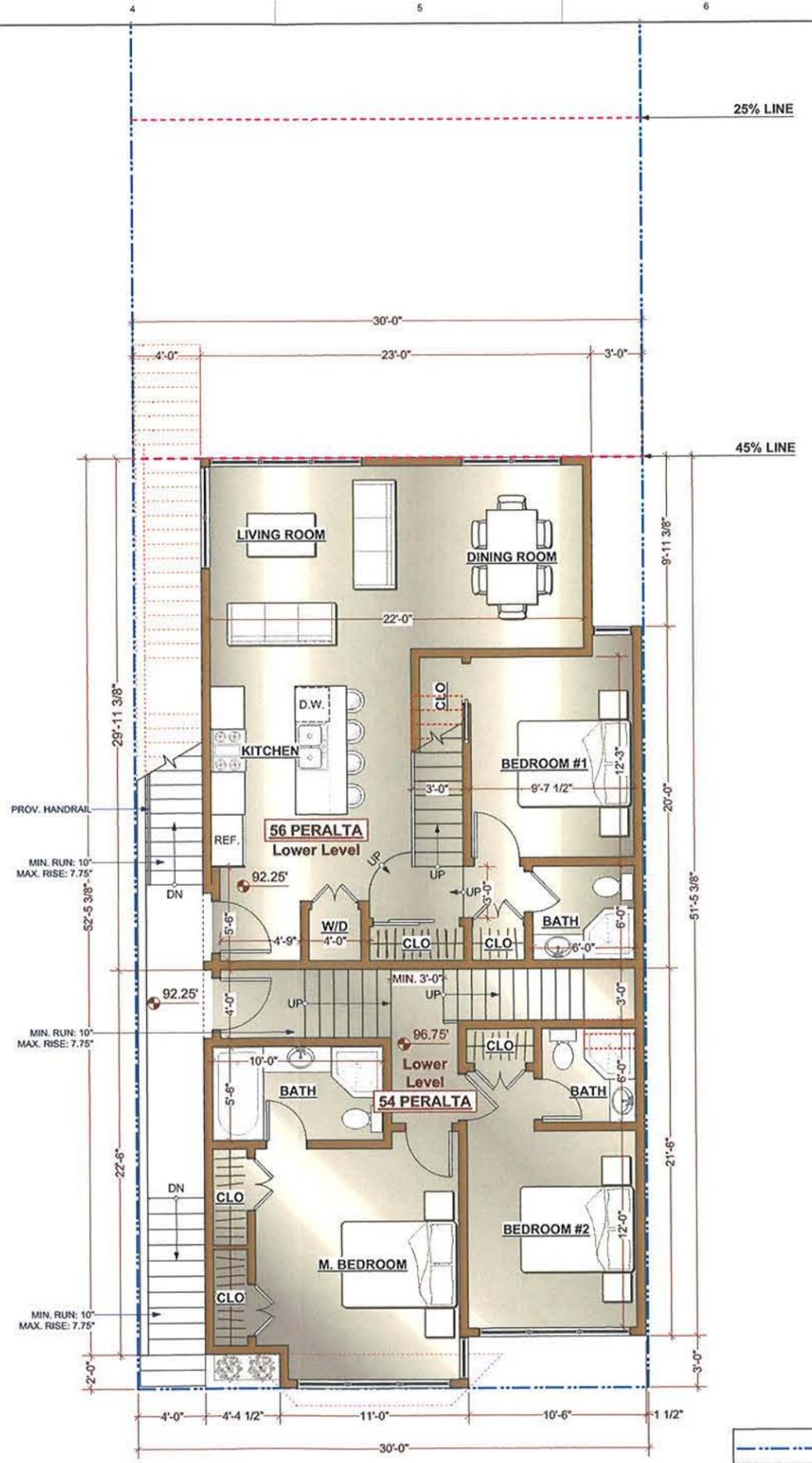
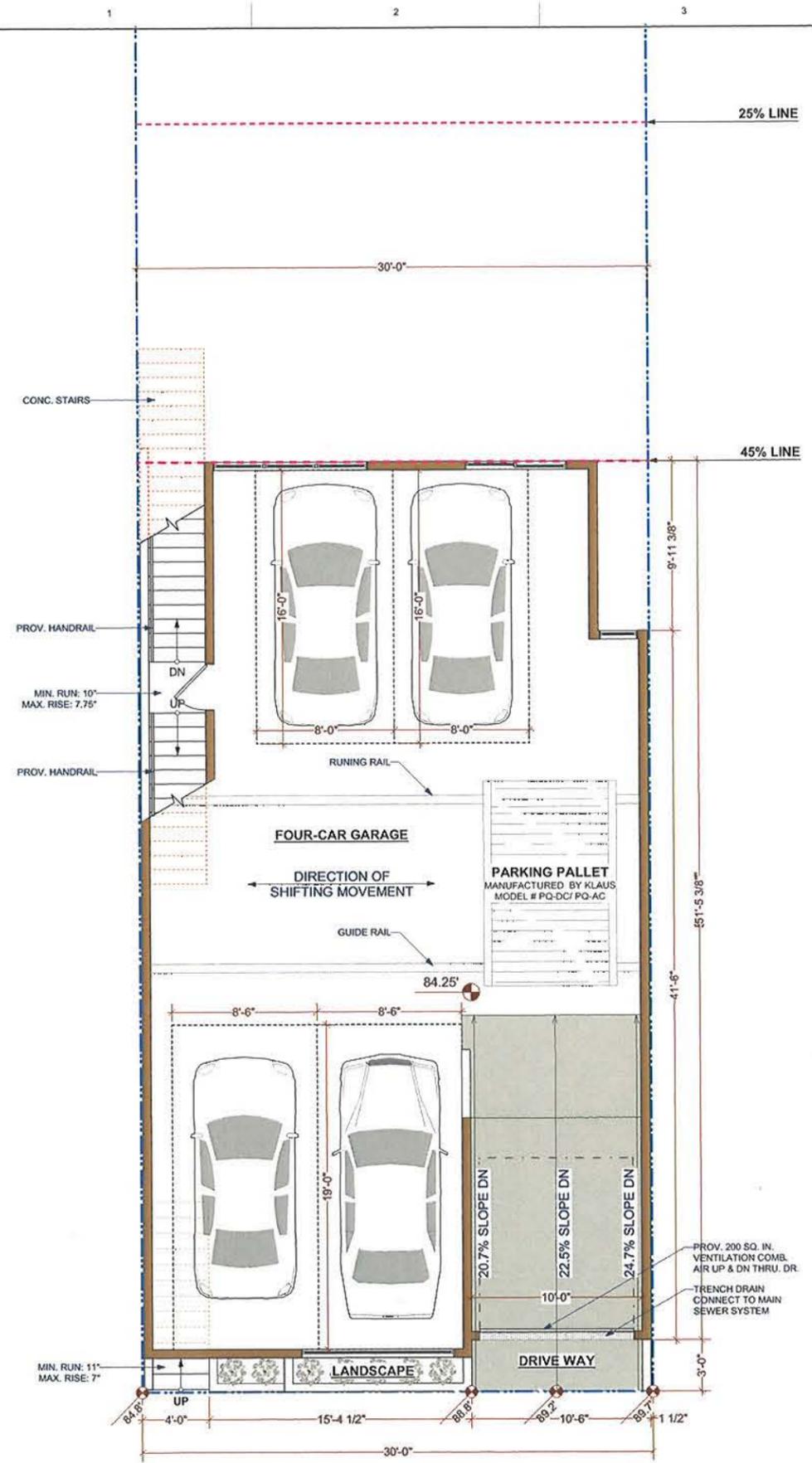
SHEET TITLE

**FIRST & SECOND FLOOR PLANS**

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ISSUES / REVISIONS		
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DATE	05/29/08
REVISED DATE	08/26/10
JOB NO.	08-1364A
SHEET NO.	A-2.0



	PROPERTY LINE
	(N) WALL TO BE CONSTRUCTED
	(N) WALL TO BE 1-HR FIRE RATED



SIA CONSULTING CORPORATION  
 1256 HOWARD STREET  
 SAN FRANCISCO CA 94103  
 TEL: (415) 922.0200  
 FAX: (415) 922.0203  
 WEBSITE: WWW.SIACONSULT.COM

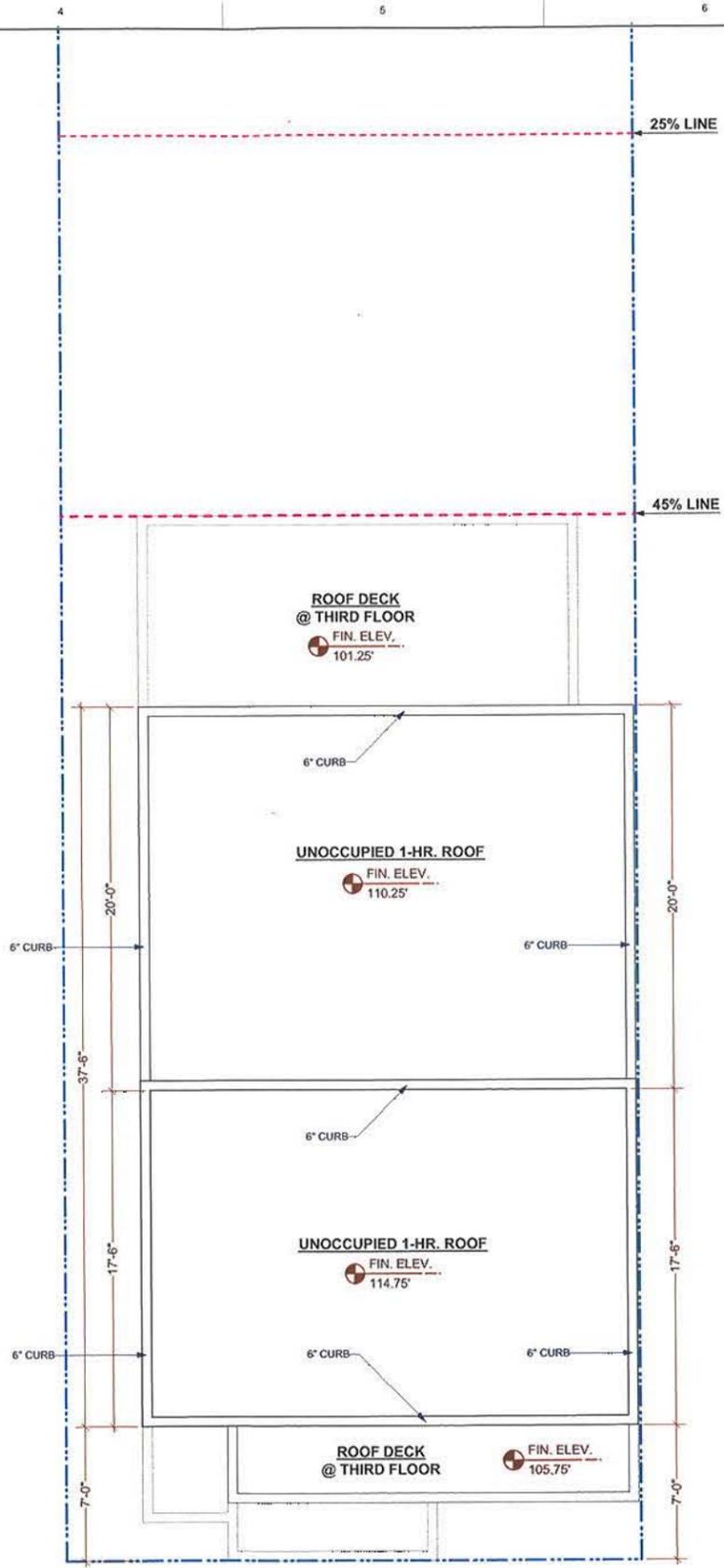
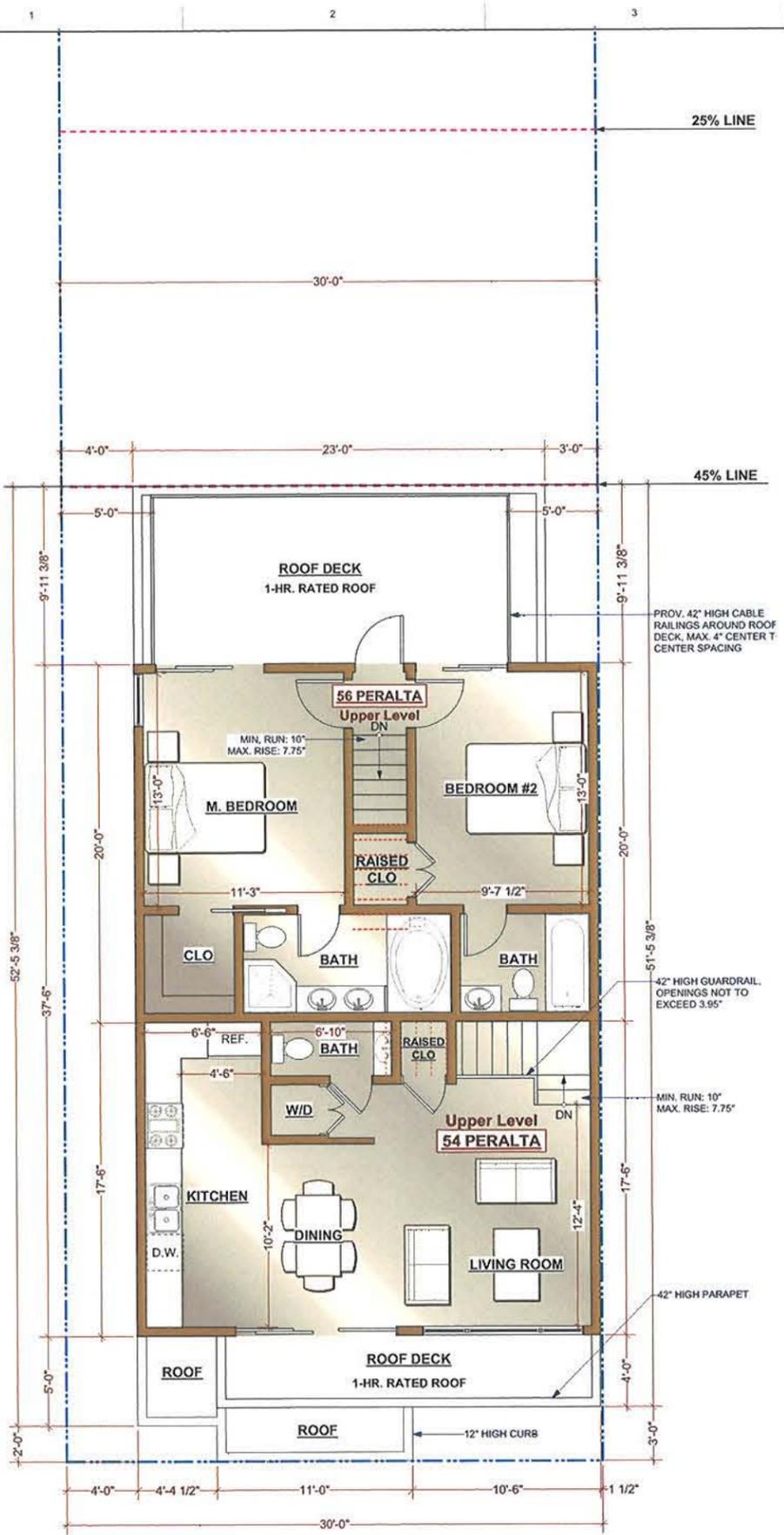
SHEET TITLE

**THIRD FLOOR PLAN  
 & ROOF PLAN**

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ISSUES / REVISIONS		
NO.	DATE	DESCRIPTION

DRAWN	R.L.
CHECKED	R.K.
DATE	05/29/08
REVISED DATE	08/26/10
JOB NO.	08-1364A
SHEET NO.	<b>A-2.1</b>



	PROPERTY LINE
	(N) WALL TO BE CONSTRUCTED
	(N) WALL TO BE 1-HR FIRE RATED









SIA CONSULTING CORPORATION  
 1256 HOWARD STREET  
 SAN FRANCISCO CA 94103  
 TEL: (415) 922.0200  
 FAX: (415) 922.0203  
 WEBSITE: WWW.SIACONSULT.COM

SHEET TITLE

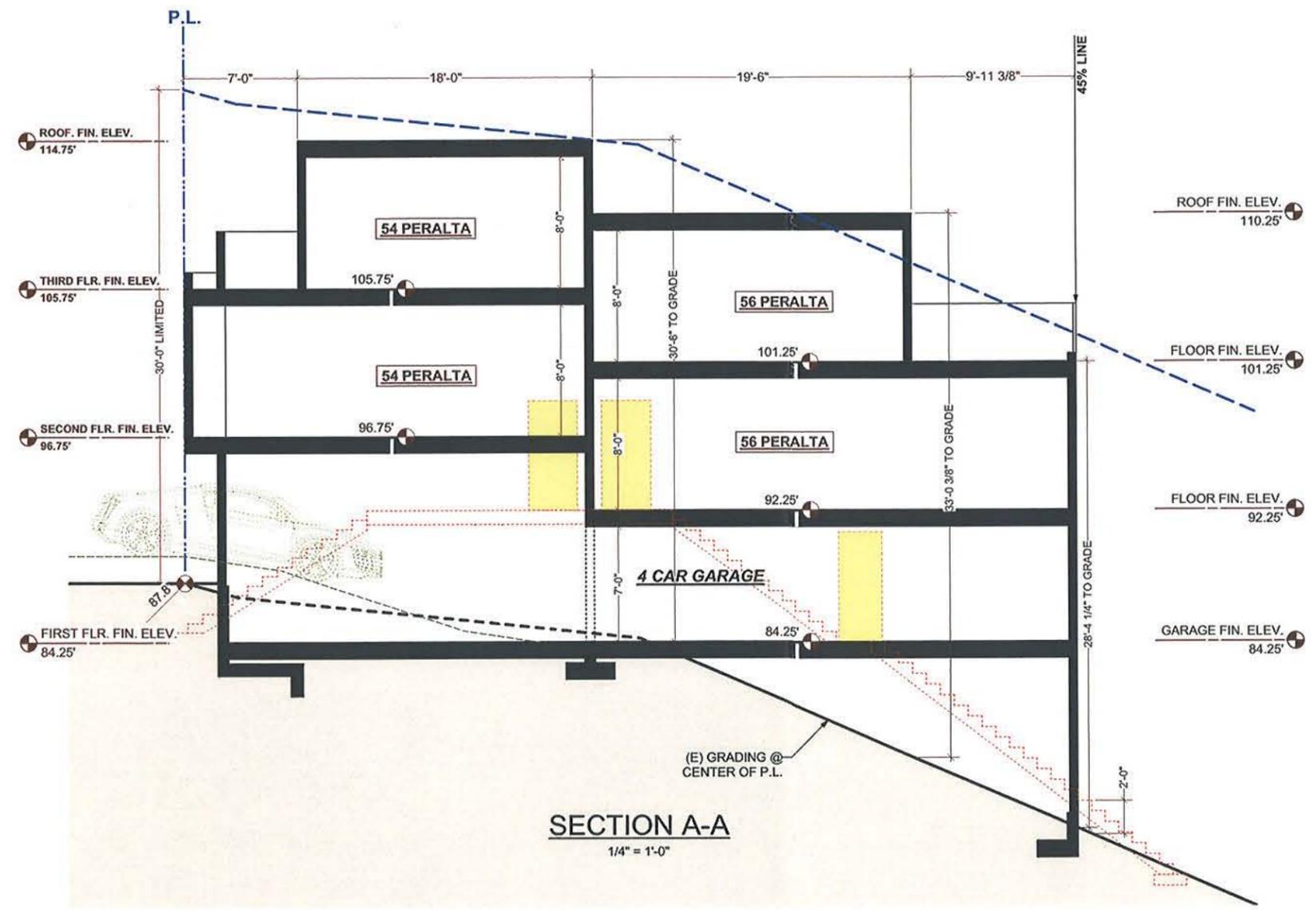
**SECTION A-A**

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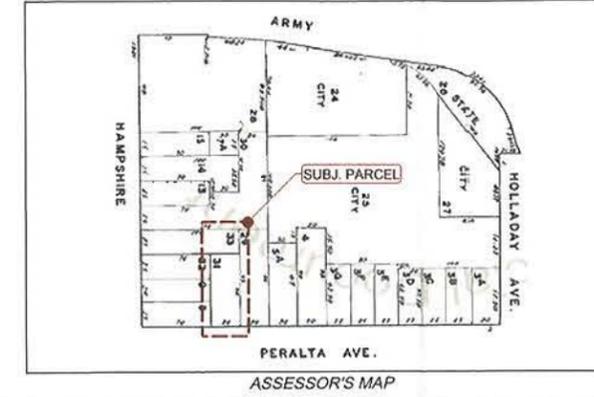
**A-4.0**



**SECTION A-A**  
 1/4" = 1'-0"

**SCOPE OF WORK:**

**PROPOSED NEW TWO-UNIT BUILDING  
@ 60-62 PERALTA AVE., SAN FRANCISCO, CA**



**PROJECT NAME**  
**60-62 PERALTA AVE**  
**SAN FRANCISCO, CA**



SIA CONSULTING CORPORATION  
1256 HOWARD STREET  
SAN FRANCISCO CA 94103  
TEL: (415) 922.0200  
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WEBSITE: WWW.SIACONSULT.COM

**GENERAL NOTES:**

1. ALL WORK SHALL BE PERFORMED IN COMPLETE COMPLIANCE WITH ALL APPLICABLE CODES, LAWS, ORDINANCES AND REGULATIONS OF ALL AUTHORITIES HAVING JURISDICTION OVER THE WORK. ALL CONTRACTORS SHALL HOLD HARMLESS THE ARCHITECT/ENGINEER AND THE OWNER FROM ALL DAMAGES AND/OR PENALTY ARISING OUT OF VIOLATION THEREOF.
2. ALL ATTACHMENTS, CONNECTIONS OR FASTENING OF ANY NATURE ARE TO BE PROPERLY AND PERMANENTLY SECURED IN CONFORMANCE WITH THE BEST PRACTICE OF THE BUILDING INDUSTRY. DRAWINGS SHOWS ONLY SPECIAL REQUIREMENTS TO ASSIST THE CONTRACTOR AND DO NOT ILLUSTRATE EVERY DETAIL.
3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, MEASUREMENTS AND CONDITIONS IN THE FIELD BEFORE BEGINNING WORK. ANY DISCREPANCIES, ERROR OR OMISSIONS SHALL BE BROUGHT TO THE ARCHITECT/ENGINEER'S ATTENTION IMMEDIATELY.
4. UNLESS OTHERWISE NOTED, ALL ANGLES SHALL BE RIGHT ANGLES. ALL LINES WHICH APPEAR PARALLEL SHALL BE PARALLEL, AND ALL ITEMS WHICH APPEAR CENTERED SHALL BE CENTERED. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL LINES TRUE LEVEL, PLUMB AND SQUARE.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SHORING AND PROTECTION DURING CONSTRUCTION. ALL EXISTING IMPROVEMENTS TO REMAIN SHALL BE PROTECTED. ALL MATERIALS DELIVERED TO THE SITE SHALL BE PROPERLY STORED AND PROTECTED UNTIL INSTALLATION. ALL LUMBER SHALL BE PROTECTED FROM MOISTURE AND STORED ABOVE GROUND.
6. DETAILED AND/OR LARGER SCALE DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL AND SMALLER SCALE DRAWINGS. FIGURED DIMENSIONS SHALL TAKE PRECEDENCE OVER SCALED DIMENSIONS. ALL SCALED DIMENSIONS SHALL BE VERIFIED.
7. ALL WORK SHALL BE DONE UNDER PERMIT. PLANS AND CALCULATIONS, IF REQUIRED, SHALL BE SUBMITTED TO AND APPROVED BY THE BUILDING DEPARTMENT. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS.

**ABBREVIATION**

&	AND	F.D.	FLOOR DRAIN
@	A T	FDN	FOUNDATION
A.B.	ANCHOR BOLT	FL	FLOOR
ABV	ABOVE	FLUOR	FLUORESCENT
A.F.F.	ABOVE FINISHED FLOOR	F.O.F.	FACE OF FINISH
ALT	ALTERNATE	FURR	FURRING
ALUM	ALUMINUM	G.B.	GRAB BAR
APPROX	APPROXIMATE	GND	GROUND
ASPH	ASPHALT	GRP	GROUP
BD	BOARD	G YP	GYPSPUM
BLDG	BUILDING	H.C.	HANDICAPPED
BLKG	BLOCKING	HR	HOUR
BOT	BOTTOM	MAX	MAXIMUM
CLG	CEILING	MIN.	MINIMUM
CLO	CLOSET	(N)	NEW
CLR	COUNTER	N.T.S.	NOT TO SCALE
CNTR	CLEAR	O.C.	ON CENTER
CONC	CONCRETE	OFF	OFFICE
CORR	CORRIDOR	P.L.	PROPERTY LINE
CTR	CENTER	PLYWD	PLYWOOD
DBL	DOUBLE	RDWD	REDWOOD
DET	DETAIL	REQD	REQUIRED
D.F.	DRINKING FOUNTAIN	SF	SQUARE FOOT
DIA	DIAMETER	STD	STANDARD
DN	DOWN	STRUCT	STRUCTURAL
DR	DOOR	SQ	SQUARE
DWR	DRAWING	TC	TOP OF CURB
(E)	EXISTING	TYP	TYPICAL
EA	EACH	U.N.O	UNLESS NOTED OTHERWISE
EL	ELEVATION	V.I.F.	VERIFY IN FIELD
ELEC	ELECTRICAL	W.H.	WATER HEATER
EQ	EQUAL	W/	WITH
EXT.	EXTERIOR	WD	WOOD

**PROJECT DATA**

<b>LOT AREA :</b>	<b>2,220 S.F.</b>
<b>FIRST FLOOR AREA:</b>	<b>772 S.F.</b>
<b>SECOND FLOOR AREA:</b>	<b>1,006 S.F.</b>
<b>THIRD FLOOR AREA:</b>	<b>1,055 S.F.</b>
<b>FOURTH FLOOR AREA:</b>	<b>440 S.F.</b>
<b>TOTAL FLOOR AREA:</b>	<b>3,266 S.F.</b>
<b>60 PERALTA:</b>	
<b>FLOOR AREA:</b>	<b>1,728 S.F.</b>
<b>OPEN SPACE:</b>	<b>148 S.F.</b>
<b>62 PERALTA:</b>	
<b>FLOOR AREA:</b>	<b>1,545 S.F.</b>
<b>OPEN SPACE:</b>	<b>441 S.F.</b>
<b>COMMON OPEN SPACE(BACK YARD):</b>	<b>849 S.F.</b>
<b>NUMBER OF STORIES:</b>	<b>4</b>
<b>NUMBER OF UNITS:</b>	<b>2</b>
<b>BUILDING HEIGHT:</b>	<b>28'-9"</b>
<b>ZONING DISTRICT:</b>	<b>RH-2</b>
<b>OCCUPANCY GROUP:</b>	<b>R-3</b>
<b>TYPE OF CONSTRUCTION:</b>	<b>V A</b>
<b>A.P.N. :</b>	<b>5512-031</b>
<b>APPLICABLE CODES:</b>	<b>2007 CALIFORNIA CODE EDITIONS W/ SAN FRANCISCO AMENDMENTS</b>

**SHEET TITLE**

**COVER SHEET**

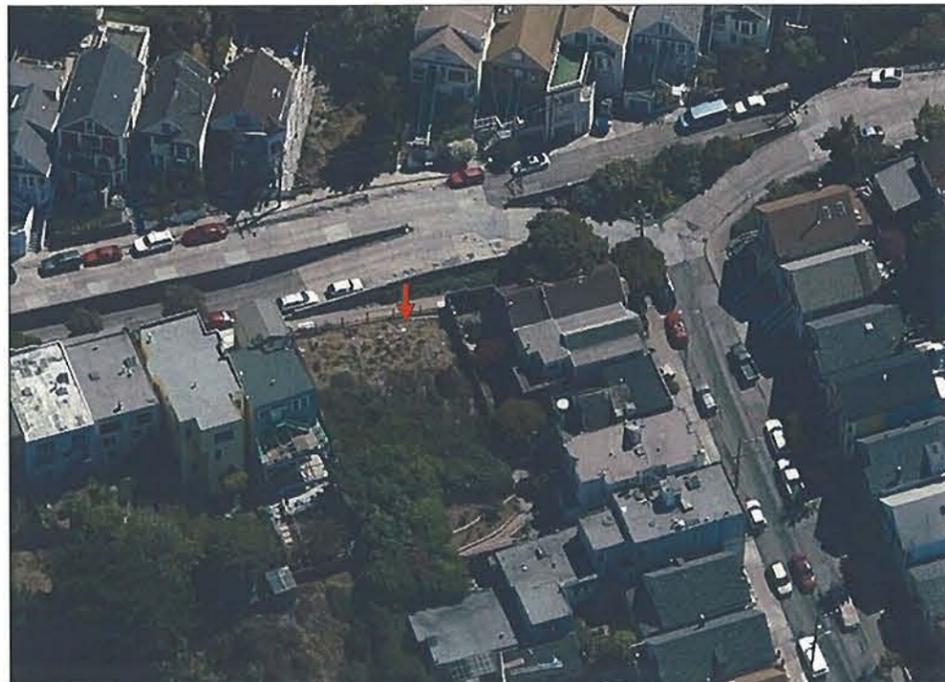
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**ISSUES / REVISIONS**

NO.	DATE	DESCRIPTION

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<b>CHECKED</b>	R.K.
<b>DATE</b>	05/29/08
<b>REVISED DATE</b>	08/17/10
<b>JOB NO.</b>	08-1364B
<b>SHEET NO.</b>	<b>A-0.0</b>

**AERIAL PHOTO**



**APPENDIX:**

A-0.0	COVER SHEET
A-1.0	SITE PLAN & NOTES
A-2.0	FIRST & SECOND FLOOR PLANS
A-2.1	THIRD & FOURTH FLOOR PLANS
A-2.2	ROOF PLAN, & SECTION A-A
A-3.0	FRONT ELEVATION
A-3.1	REAR ELEVATION, & WINDOW DETAILS
A-3.2	LEFT & RIGHT ELEVATIONS
C-1	SURVEY

**ELECTRICAL NOTES:**

ELECTRICAL SUBPANEL(S) ON FLOOR PLAN(S) PANELS SHALL NOT BE LOCATED IN THE VICINITY OF EASILY IGNITABLE MATERIAL(S) SUCH AS CLOTHES CLOSETS. PANELS IN FIREWALL SHALL BE RELOCATED OR PROPERLY PROTECTED TO MAINTAIN FIREWALL SEPARATION.

GFCI PROTECTED OUTLETS AT THE FOLLOWING LOCATIONS.  
 (A) GARAGE  
 (B) UNFINISHED BASEMENT, CRAWL AND STORAGE SPACES.  
 (C) WITHIN 6' OF SINK OR BASIN  
 (D) EXTERIOR (WATERPROOF)

RECEPTABLE OUTLETS AT THE FOLLOWING LOCATIONS.  
 (A) 12' O.C. MAX. AND WITHIN 6' OF THE END OF WALLS.  
 (B) ANY WALL SPACE 2 OR MORE FEET WIDE.  
 (C) AT EACH KITCHEN AND DINING AREA COUNTER SPACE WIDER THAN 12". SO THAT NO POINT IN ANY HALLWAY 10 FEET OR MORE IN LENGTH.

LIGHT FIXTURE IN TUB OR SHOWER ENCLOSURES AND EXTERIOR LIGHT FIXTURES SHALL BE LABELED "SUITABLE FOR DAMP LOCATIONS"

APPLIANCES FASTENED IN PLACE, SUCH AS DISHWASHERS, GARBAGE DISPOSALS, TRASH COMPACTORS, MICROWAVE OVENS, ETC., SHALL BE SUPPLIED BY A SEPARATE BRANCH CIRCUIT RATED FOR THE APPLIANCE OR LOAD SERVED.

RECEPTACLES FOR FIXED APPLIANCES SHALL BE ACCESSIBLE, NOT BEHIND APPLIANCE.

A CIRCUIT SUITABLE FOR THE LOAD WITH A MINIMUM OF 30 AMPERES IS REQUIRED FOR AN ELECTRIC CLOTHES DRYER.

LIGHT FIXTURES IN TUB OR SHOWER ENCLOSURES SHALL BE LABELED "SUITABLE FOR DAMP LOCATION(S)".

**ENERGY NOTES:**

PERMANENTLY INSTALLED LUMINAIRES IN KITCHENS SHALL BE HIGH EFFICACY LUMINAIRES. UP TO 50% OF WATTAGE OF PERMANENTLY INSTALLED LUMINAIRES IN KITCHENS MAY BE IN LUMINAIRES THAT ARE NOT HIGH EFFICACY LUMINAIRES, PROVIDED THAT THESE LUMINAIRES ARE CONTROLLED BY SWITCHES SEPARATE FROM THOSE CONTROLLING THE HIGH EFFICACY LUMINAIRES.

EACH ROOM CONTAINING A WATER CLOSET SHALL HAVE AT LEAST ONE LUMINAIRE WITH LAMPS WITH AN EFFICACY OF NOT LESS THAN 40 LUMENS PER WATT FOR 15 WATT OR SMALLER, 50 LUMENS PER WATT FOR 16 WATT-40WATT, & 60 LUMENS PER WATT FOR 40 WATT OR HIGHER. IF THERE IS MORE THAN ONE LUMINAIRE IN THE ROOM, THE HEIGHT EFFICACY LUMINAIRE SHALL BE SWITCHED AT AN ENTRANCE TO THE ROOM.

ALL LIGHTING FIXTURES RECESSED INTO INSULATED CEILINGS MUST BE APPROVED FOR ZERO CLEARANCE INSULATION COVER (I.C.) OR AIR TIGHT (AT) RATED BY UNDERWRITERS LABORATORIES OR OTHER APPROVED LABORATORIES.

PROVIDE FLUORESCENT FIXTURES FOR BATHROOMS, LAUNDRY, UTILITY ROOMS AND GARAGES, OR A MANUAL ON / OCCUPANCY SENSOR CONTROL FOR ALL INCANDESCENT FIXTURES (DIMMERS DO NOT QUALIFY)

PROVIDE FLUORESCENT FIXTURES FOR ALL ROOMS, INCLUDING CLOSETS 70 SQ. FT. OR MORE (OTHER THAN KITCHEN, BATHROOM, LAUNDRY, UTILITY ROOM AND GARAGES), OR PROVIDE OCCUPANCY SENSORS OR DIMMERS.

FIREPLACES, DECORATIVE GAS APPLIANCES AND GAS LOGS: INSTALLATION OF FACTORY-BUILT AND MASONRY FIREPLACES SHALL INCLUDE:  
 (A) CLOSABLE METAL OR GLASS DOORS.  
 (B) COMBUSTION AIR INTAKE (6 SQ. IN. MINIMUM) TO DRAW AIR FROM OUTSIDE OF THE BUILDING DIRECTLY INTO FIRE BOX. THE COMBUSTION AIR INTAKE MUST BE EQUIPPED WITH A READILY ACCESSIBLE, OPERABLE AND LIGHT-FITTING DAMPER OR COMBUSTION AIR CONTROL DEVICE.  
 EXCEPTION: AN OUTSIDE COMBUSTION AIR INTAKE IS NOT REQUIRED IF THE FIREPLACE IS INSTALLED OVER CONCRETE SLAB FLOORING AND THE FIREPLACE IS NOT LOCATED ON AN EXTERIOR WALL.  
 (C) A FLUE DAMPER WITH AN READILY ACCESSIBLE CONTROL.  
 EXCEPTION: WHEN A GAS LOG, LOG LIGHTER, OR DECORATIVE GAS APPLIANCE IS INSTALLED IN A FIREPLACE, THE FLUE DAMPER SHALL BE BLOCKED OPEN IF REQUIRED BY THE MANUFACTURER'S INSTALLATION INSTRUCTIONS OR THE STATE MECHANICAL CODE.

**PLUMBING AND MECHANICAL NOTES:**

HEATING SUPPLY AIR DUCTS SHALL BE MIN. NO.26 GA. GALVANIZED SHEET METAL WITH SEALED ANNULAR OR A FIRE DAMPER PROVIDED WHEN THE DUCTS PENETRATE THE OCCUPANCY SEPARATION BETWEEN THE GARAGE AND THE HOUSE.

PLASTIC PLUMBING PIPE SHALL NOT BE USED FOR DOMESTIC WATER SUPPLY AND/OR SANITARY WASTE SYSTEM

SMOOTH METAL DUCT FOR DRYER EXHAUST EXTENDING TO OUTSIDE.

NON-REMOVABLE BACKFLOW PREVENTION DEVICES ON ALL EXTERIOR HOSE BIBS.

SIZE OF WATER CLOSETS, MAXIMUM ALLOWABLE 1.6 GALLONS PER FLUSH.

SHOWER & TUB/SHOWERS SHALL BE PROVIDED WITH PRESSURE BALANCE OR THERMOSTATIC MIXING VALVE CONTROLS. HANDLE POSITION STOPS SHALL BE PROVIDED ON SUCH VALVES AND SHALL BE ADJUSTED PER MANUFACTURER'S INSTRUCTIONS TO DELIVER A MAXIMUM MIXED WATER SETTING OF 120 DEGREES F. THE WATER HEATER THERMOSTAT SHALL NOT BE CONSIDERED A SUITABLE CONTROL FOR MEETING THIS PROVISION, U.P.C. 4107.

DOORS & PANELS OF SHOWERS AND BATHTUBS ENCLOSURES AND ADJACENT WALL OPENINGS WITHIN 60" ABOVE A STANDING SURFACE AND DRAIN INLET SHALL BE FULLY TEMPERED. LAMINATED SAFETY GLASS OR APPROVED PLASTIC.

TEMPERED GLASS SHALL BE AFFIXED WITH A PERMANENT LABEL.

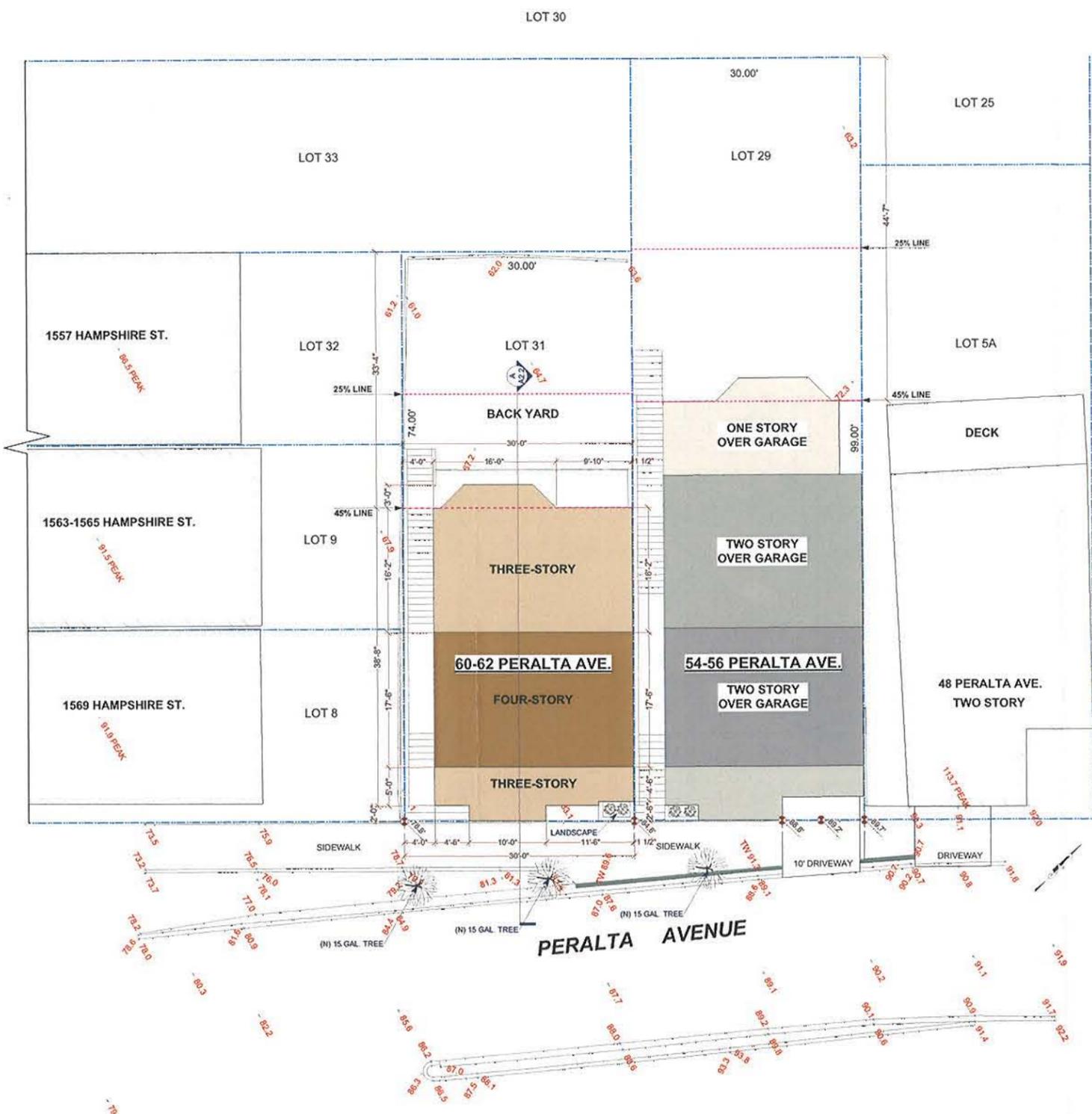
**SANITATION NOTES:**

SHOWER STALL FINISH SHALL BE CERAMIC TILE EXTENDING 70 INCHES ABOVE THE DRAIN INLET

MOISTURE RESISTANT UNDERLAYMENT (e.g. WATER RESISTANT GYP. BD.) TO A HEIGHT OF 70 INCHES ABOVE THE DRAIN INLET U.B.C. 8067.1.3.

**BEDROOM WINDOWS:**

MIN. OPENABLE AREA TO BE 5.7 S.F., MIN WIDTH: 20" MIN HEIGHT: 24" AND MAX SILL HT: 44"



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SHEET TITLE  
**SITE PLAN,  
 & NOTES**

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ISSUES / REVISIONS		
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**SITE PLAN**  
 1/8" = 1'-0"



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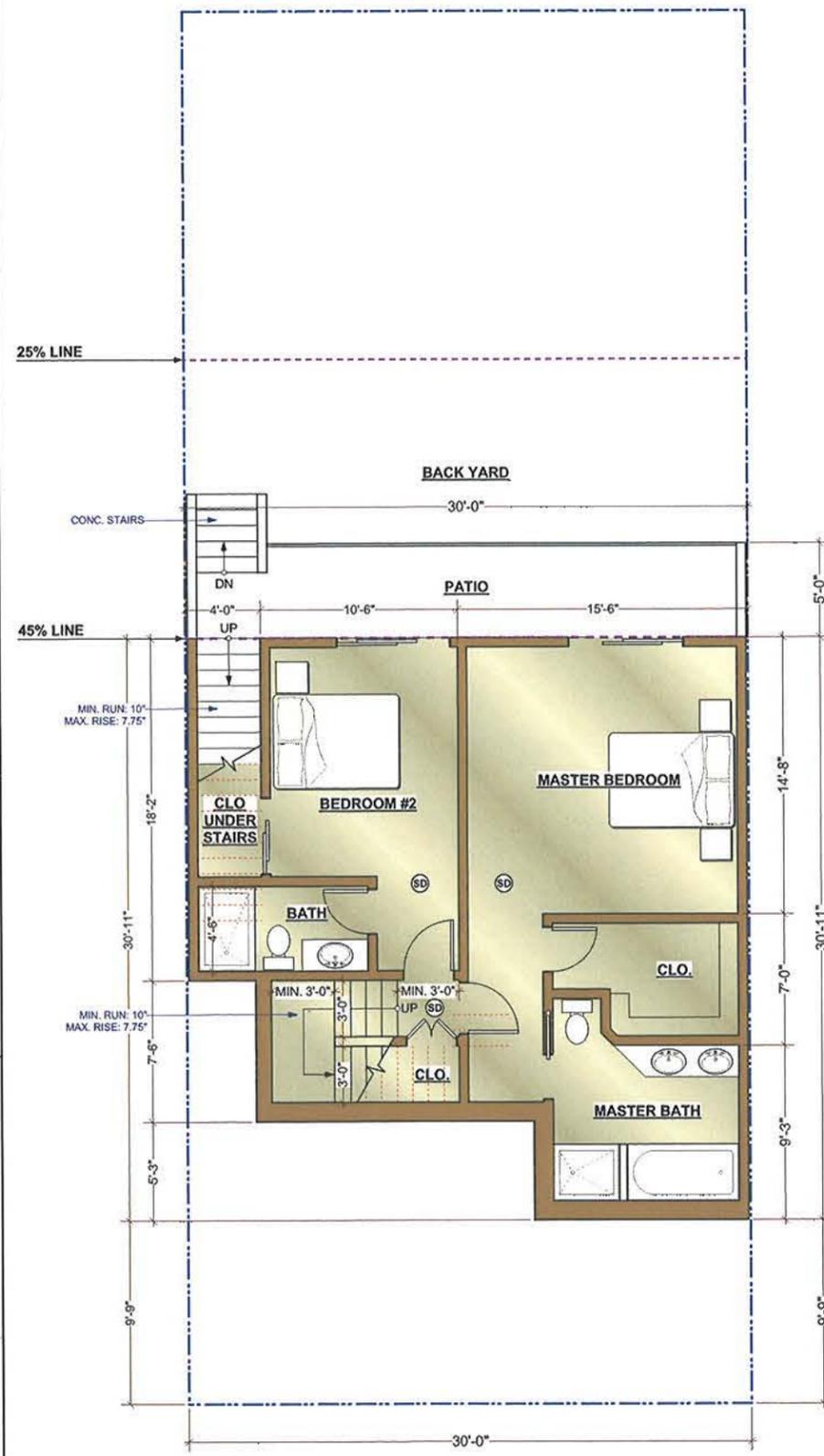
**FIRST & SECOND FLOOR PLANS**

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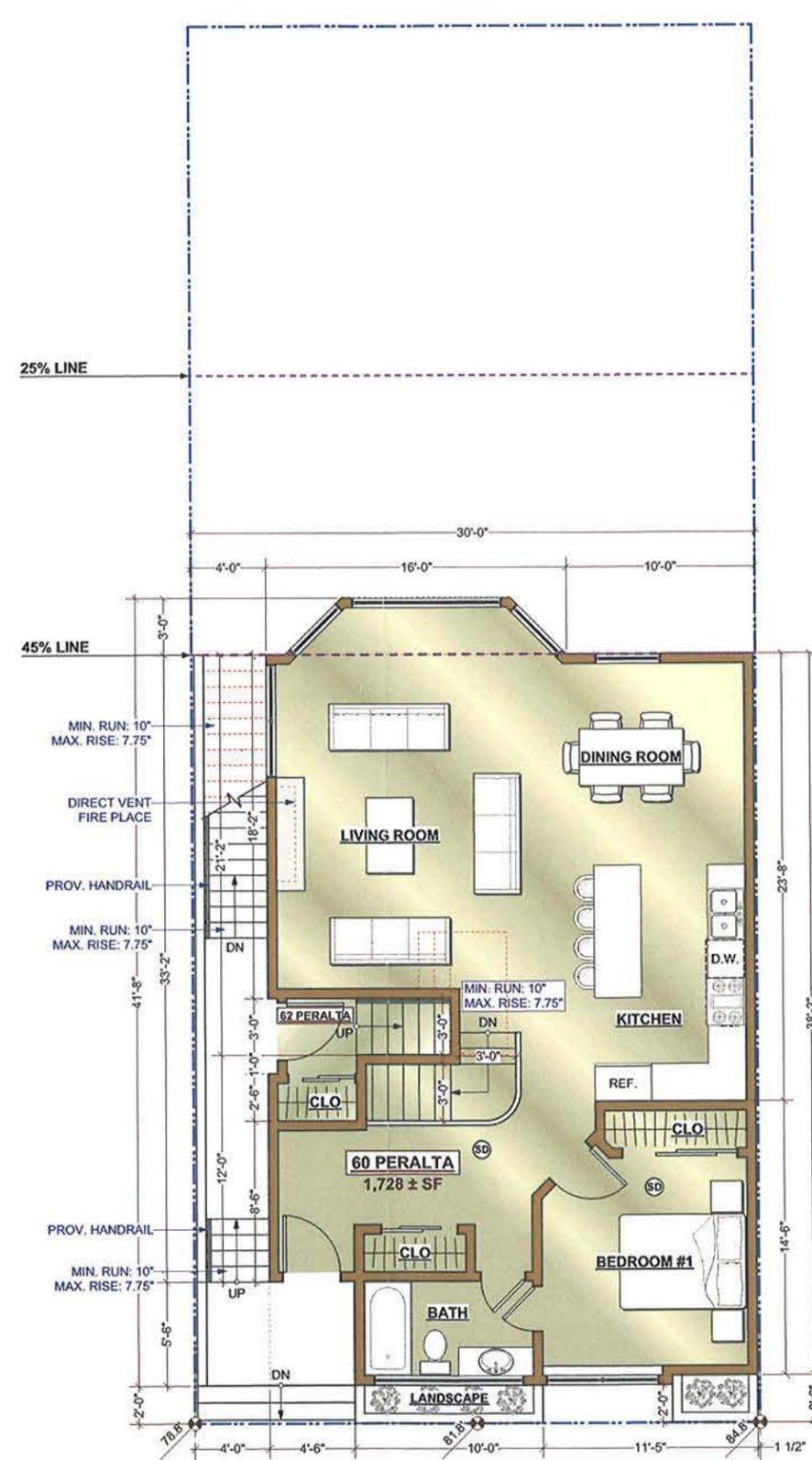
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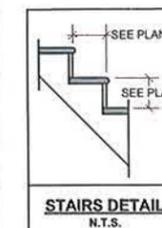
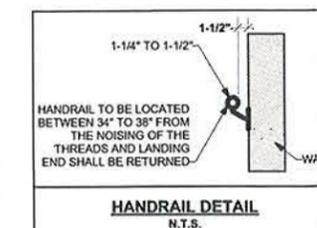
SHEET NO.  
**A-2.0**



**FIRST FLOOR PLAN**  
 1/4" = 1'-0"



**SECOND FLOOR PLAN**  
 1/4" = 1'-0"



SD	SMOKE DETECTOR, 110-V INTERCONNECTED WITH BATT
---	PROPERTY LINE
---	(N) WALL TO BE CONSTRUCTED
---	(N) WALL TO BE 1-HR FIRE RATED



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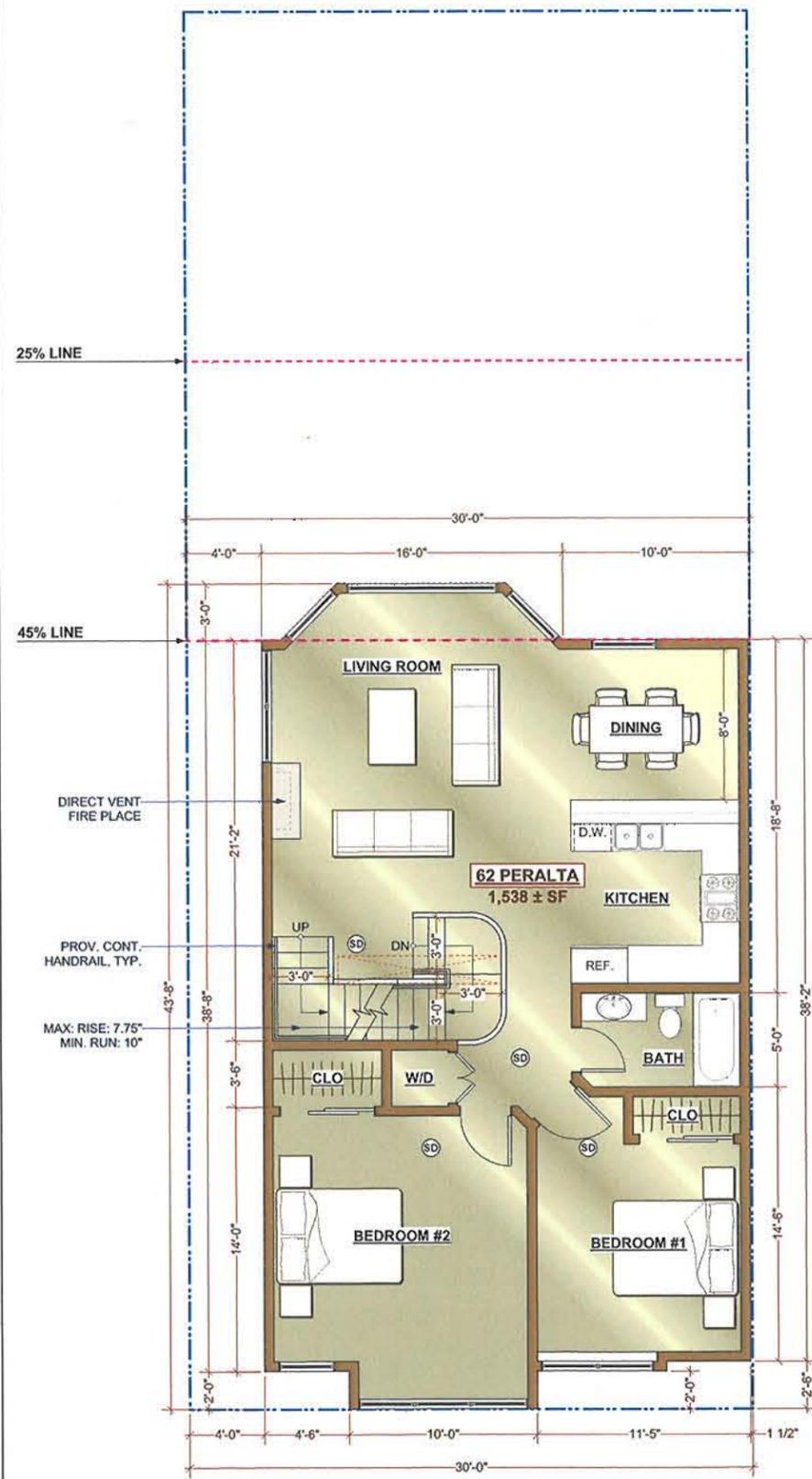
**THIRD & FOURTH FLOOR PLANS**

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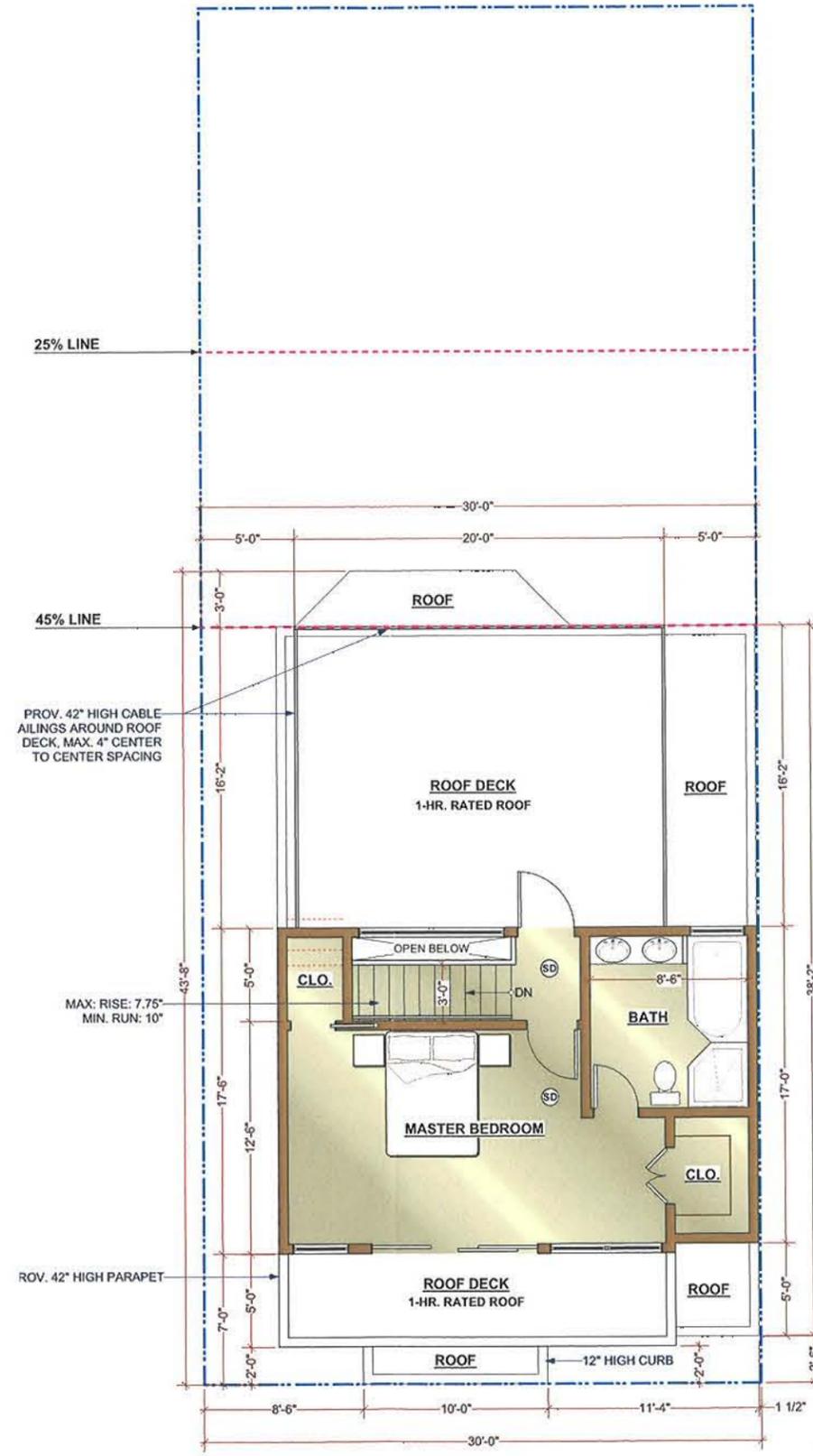
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SHEET NO.	<b>A-2.1</b>



**THIRD FLOOR PLAN**  
 1/4" = 1'-0"



**FOURTH FLOOR PLAN**  
 1/4" = 1'-0"

	SMOKE DETECTOR, 110-V INTERCONNECTED WITH BATT
	PROPERTY LINE
	(N) WALL TO BE CONSTRUCTED
	(N) WALL TO BE 1-HR FIRE RATED



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SHEET TITLE

**ROOF PLAN,  
 & SECTION A-A**

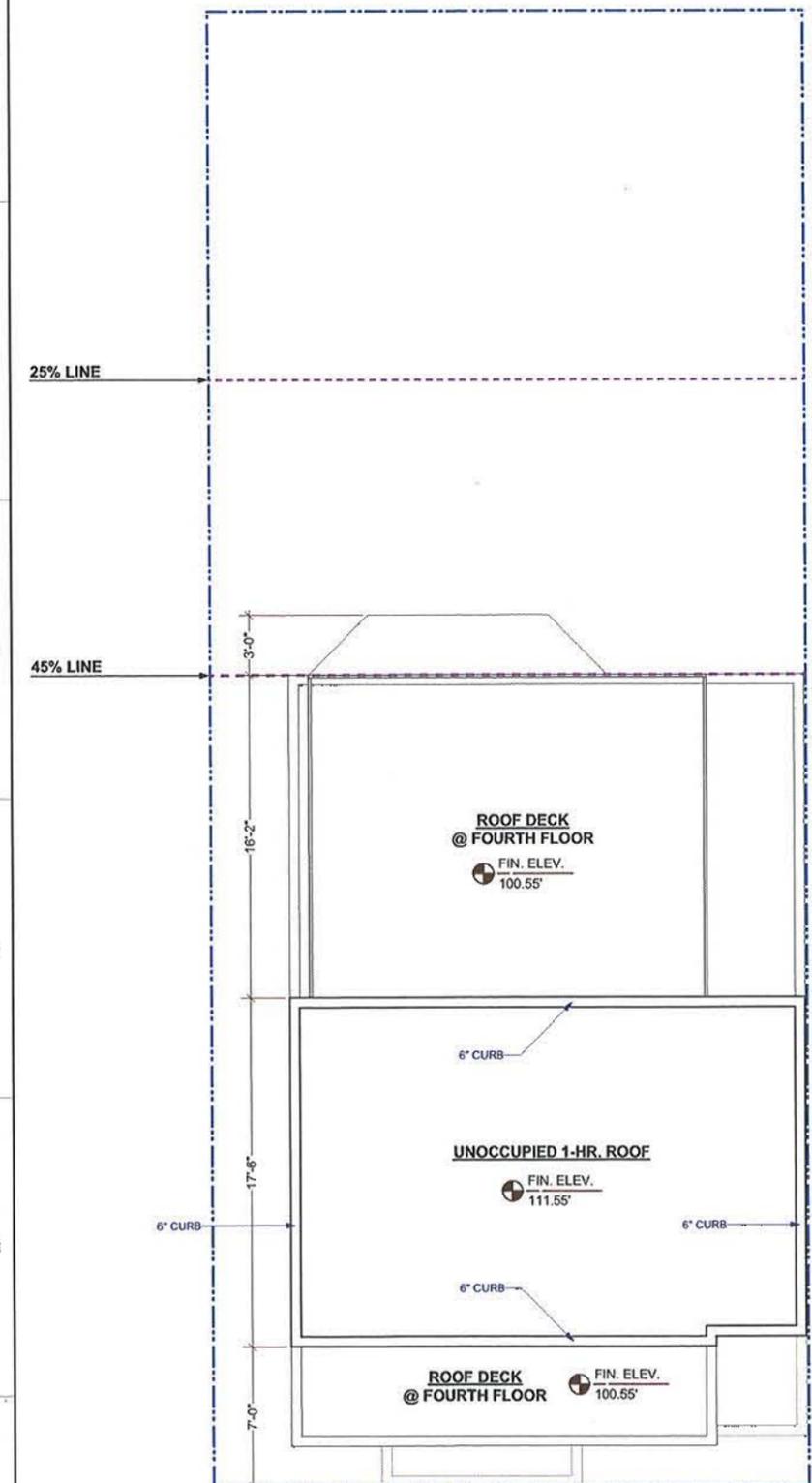
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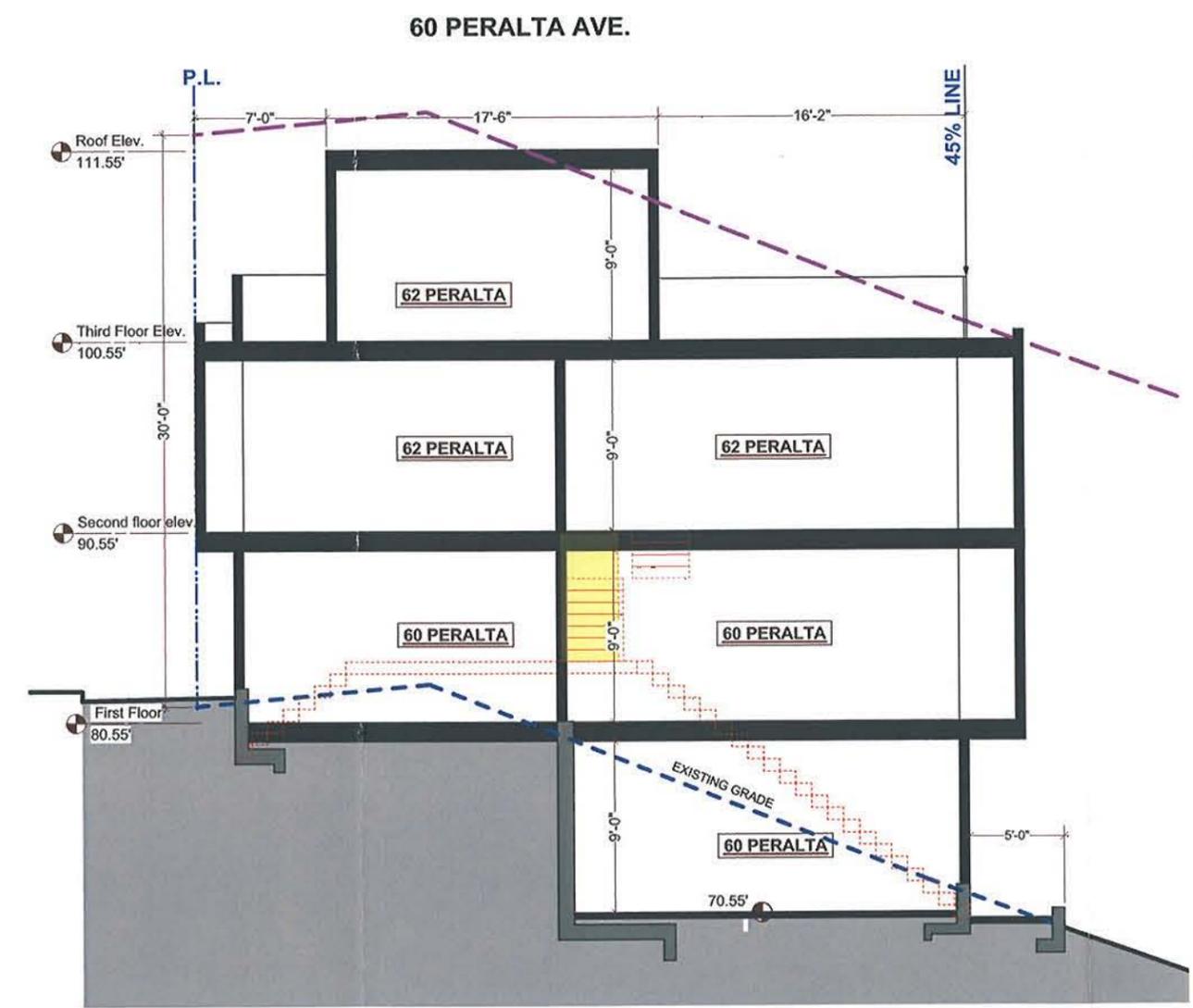
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**A-2.2**



**ROOF PLAN**  
 1/4" = 1'-0"



**SECTION A-A**  
 1/4" = 1'-0"



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**FRONT ELEVATION**

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SHEET NO.	

**A-3.0**









60 PERALTA AVE.

54 PERALTA AVE.

44

39



# Bernal Heights

## East Slope Design Review Board

Terry Milne, external secretary • 321 Rutledge • San Francisco 94110 • [285-8978]

February 18, 2011

Ben Fu, Planner  
Dept. of City Planning  
1650 Mission Street  
San Francisco, Calif. 94103

Re: 2010.03567DV  
54/56 Peralta Ave  
62/64 Peralta Ave  
Block/lot: 5512-29+31

Dear City Planner:

The East Slope Design Review Board held a neighborhood meeting in February to review the revised plans for this project to construct a two-unit residential building on each of two adjoining lots that are currently vacant. This meeting was a follow up to comments from the Board that were discussed at previous meetings in July and August 2009 and April 2010.

To reinforce our previous letter, the Board cannot recommend that the Department of City Planning approve this project as proposed. The Board also cannot support the parking variances that are part of the project. The Board believes that the design does not comply with the East Slope Building Guidelines.

The Review Board's primary concern is the request for a variance for three parking spaces. The size and bulk of the plans require seven parking spaces according to Section 242 of the Planning Code. The plans provide only four spaces, squeezed into one of the buildings. The intent of Section 242 is to limit the bulk and mass of new construction, particularly on steeply sloping lots. We also believe that the neighborhood has a limited ability to provide on-street parking for the current residents. The radical parking scheme will have a burdensome impact on the adjoining houses. A modification to the plans that reduces the size and bulk will reduce the need for a parking variance.

There is neighborhood concern about the impact that cutting the street wall and curb needed for parking egress will have on the drainage of rain water away from neighbors below the proposed buildings. The drawings do not show enough detail to be assured that this will not be a significant problem. The Board would like to know if a Public Works hearing will be required for the sidewalk and street wall changes.

February 18, 2011 ~~~ 54/56 + 62/64 Peralta Ave (Page 2)

In the Board's previous letter, we had issues with the scale of 62/64 Peralta Avenue at the street wall, and its effect on the adjacent property on Hampshire Street. The current plans do not address these concerns, and we do not believe that they comply with the East Slope Building Guidelines. Our suggestion is to set back the top floor a minimum of fifteen feet from the front wall and five feet from the west side.

At our meetings, neighbors expressed some similar concerns which we expect they will explain in detail at their requested Discretionary Review.

- Scale of buildings: the neighbors object to the large mass of the buildings relative to the character of the existing neighborhood, as well as the number of units proposed.
- Drainage: the neighbors are concerned about the impact cutting through the street wall to get to the garage will have on the water drainage down Peralta Avenue.
- Parking egress: the neighbors expressed concern that the slope of the sidewalk will impact safety by making it difficult for cars exiting the garage to see pedestrians.
- Parking: the neighbors are skeptical that the parking scheme will be practical for regular use by the tenants of the two buildings. The concern is that the garage will not be in full use, putting extra cars on the street, burdening the already limited street parking.

Thank you for your consideration.

Cordially



Jeff Sardan, Chair, Bernal Heights ESDRB

cc: Reza Khoshnevisan - Architect  
Bob Besso, D. R. Requestor  
Scott Sanchez, Zoning Administrator

March 9, 2011

1

Christina Olague, President  
San Francisco Planning Commission  
City and County of San Francisco  
1650 Mission Street, 4th Floor  
San Francisco, CA 94103

RE: 54-56 Peralta and 60-62 Peralta  
Hearing Date: March 17, 2011  
Neighborhood Objections and Opposition to Project as Currently Proposed

President Olague and Members of the Planning commission:

My name is Bob Besso, I am the listed appellant for the Discretionary Review, Case number 2010.367D, the proposed development of two lots at 54-62 Peralta Avenue, in the Bernal Heights Special Use District. I own the property at 1569 Hampshire St., which directly adjoins the proposed development. I am also the representing member of the Far North East Bernal Heights Neighborhood Block Club. I moved to this address in 1983 and bought the property in 1989. The Block Club has been active in Neighborhood affairs since 1970, and some neighbors and Club members have lived on this block of Peralta Avenue since the 1960's.

We filed for the Discretionary Review on the proposed development of these two lots, because of the:

- (1) Developer and owner violations of and abnormalities related to the required pre-planning process;
- (2) Non-compliance of the Special Use District Guidelines;
- (3) Owner and developer contempt for and abandonment of the Neighborhood negotiation and mitigation process; and
- (4) Hazards associated with the disruption of the public access sidewalk and the removal of a section of street curb and retaining wall for abnormal driveway access and egress

The Plans are Inadequate and Unclear as the Adjacent Buildings are Not In Scale or Correctly Depicted

These proposed buildings are much larger than the surrounding buildings and are not shown in a manner which allows a comparison to the adjacent structures. The proposed building at 54-56 Peralta is actually taller than the up-hill neighbor at 48 Peralta. The plans do not have information showing the relationship of the project to adjacent properties, including the position and height of all adjacent buildings and location of the windows facing the subject property. My property at 1569 Hampshire is not shown in scale and neither are my downhill neighbors' homes on Hampshire, which will also be significantly impacted by this project as proposed. The regulations require that all adjacent buildings be shown in scale as well as depictions of all windows and openings that will be blocked.

The Project fails to comply with the Bernal Heights East Slope Design Guidelines.

The Guidelines were accepted by the Planning Commission November 13, 1986. Page 2, paragraph 5 of the guidelines states: "... *These Guidelines are an effort to retain the spirit of the neighborhood and to establish criteria for new housing design that will ensure, as much as possible, the continued existence of the East Slope's unique character.*". The Developer has completely ignored the guidelines with their proposed overbuilding of these problem lots.

These lots have never been developed because they slope downhill steeply in two directions and have limited street access due to the erosion control curb and retaining wall. The Bernal Heights Special Use District Guidelines specifically identify, among other things architectural massing and bulk and accommodations thereof, as indicated on page 13, paragraph one under PROBLEM “*Around the East Slope of Bernal Heights the remaining open lots are steeply sloping. Consequently, the main problem here is one of heights and massing. The downhill slopes are particularly problematic...*” Ironically, the developer’s request for a parking variance is directly related to the mass and bulk of this non-compliant development. We believe that the size of the project is at the root of problem with this development, and virtually the entire source of our objections.

The proposed buildings are simply too big, inconsistent with and dominating of surrounding buildings. The inability to comply with the provisions of the Special Use District clearly demonstrate this fact. The provisions of the Special Use District are not based on ‘archaic’ parking requirements, but rather, the rules are in place to limit the size of new construction and new additions to a size that will fit the neighborhood, its steep hills, small lots and very narrow streets. Please do not ignore the Special Use District requirements and make this speculative for-profit development comply with all provisions of the Special Use District.

Contempt And Abandonment Of The Neighborhood Mitigation Process

Bernal Heights is a special and unique place. The Planning Code specifically put into place and empowered neighborhood and community groups and a design review board to review projects and bring them into compliance with the code and the community expectations and standards. The developer in this instance is asking the Planning Commission to substitute its opinions and values for that of the Community, the neighbors, and the Design Review Board that is specifically set up to pass judgment on such projects. The Design Review Board was clear in its letter of June 5, 2010:

**“The Board believes that the project does not comply with either the letter or the spirit of the Bernal Heights East Slope Building Guidelines.”**

This is all new construction. No Variances should be granted at all. The need for variances is a “reflection of the size of the buildings and not the site access.” The project is deemed too massive and too tall at the street and side walls and looms over the surrounding buildings. It does not meet the building design guidelines which protect and acknowledging the existing neighborhood, and mimic the steeply sloping hill.

The Design Review Board urged numerous changes on the project and invited the developer to return to present requested changes. Prior to returning to the neighborhood mitigation process or notifying the Board, the developer filed for a parking variance utilizing existing plans with a faulty parking element. Only after our neighborhood discovered the variance filing and our subsequent filing for DR, did the developer return to the design review board. However the developer ignored the modification requests of the Board and returned only to show new 3D models of the same project. We find the developers behavior to be in complete contempt of the pre-planning, and neighborhood mitigation process which is designed to avoid Discretionary Reviews.

The Planning Code and Special Use District specially enables this Board to be an arbiter of design matters. As community volunteers, members of the Board devotes countless hours of community service to the neighborhood they know and love. Trust the Board, and the Building Guidelines. Do not send the message to the development community that Guidelines can be ignored and circumvented. Uphold the Board's findings and request reductions in this project, in full compliance with the Special Use District rules. This does not mean more parking, but rather smaller buildings.

Water Run-Off And Driveway Access/Egress Hazards.

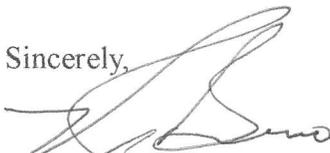
This is a difficult and steep section of the hill. The surface water drains very steeply to Hampshire Street, and very steeply down to Cesar Chavez. The surface drainage system here relies on the retaining walls and street curbs put into place some 90 years ago, and when it rains hard in our neighborhood the run-off becomes potentially hazardous (\*see note below). The developer keeps saying, "trust us" the retaining wall and curb removal for driveway access will not be a not a problem".

The developer also says that cars backing up the proposed steep driveway ramp from the sub-grade parking garage, across the modified sidewalk and into a one-way section of the street between two retaining walls, will not be a problem either. This hazardous vehicle crossing of of the public access sidewalk and into a tight street section of one-way traffic, is directly related to overbuilding these problem lots. An at grade parking scheme consistent with the neighborhood and compliant with guidelines would be far less hazardous (\*see note below).

We believe that the owner and developer have the right to build on these lots. However, the development must comply with established guidelines reflecting the character of the neighborhood, and it must not transfer the development problems of the lots to our neighborhood in the form of hazards in perpetuity.

Please require the developer to submit plans in full compliance with the East Slope Building Guidelines, so that the health, safety, and continuity of our neighborhood is maintained.

Sincerely,



Bob Besso

\*Please watch the video posted on YouTube "Hazardous issues associated with the proposed development of 54-62 Peralta Ave." <http://www.youtube.com/watch?v=ZBOMN-OA11Q>