



PARKMERCED - BLOCK 06, LOTS 1, 3, & 5

455 SERRANO DRIVE (NORTH TOWER)

850 GONZALEZ DRIVE (SOUTH TOWER)

24 JULY 2015 | DESIGN REVIEW APPLICATION R4

PARKMERCED OWNER LLC.

**WOODS
BAGOT™**

TABLE OF CONTENTS

DRA A0.01 PROJECT PROGRAM AND DATA MATRIX

DRA A1.01 SITE PLAN

- DRA A2.11 LEVEL B2 FLOOR PLAN
- DRA A2.12 LEVEL B1 FLOOR PLAN
- DRA A2.13 LEVEL L1 FLOOR PLAN - SOUTH TOWER
- DRA A2.14 LEVEL L1 FLOOR PLAN - NORTH TOWER
- DRA A2.14A LEVEL L1 FLOOR PLAN ENLARGED - NORTH TOWER
- DRA A2.15 LEVELS L2-10 FLOOR PLANS - NORTH TOWER
- DRA A2.16 LEVEL L11 FLOOR PLAN - NORTH TOWER
- DRA A2.17 LEVEL ROOF FLOOR PLAN - NORTH TOWER

- DRA A5.01 SITE SECTION - NORTH/SOUTH
- DRA A5.02 SITE SECTION - EAST/WEST
- DRA A5.10 SITE ELEVATION - WEST
- DRA A5.11 NORTH TOWER ELEVATION - WEST
- DRA A5.12 NORTH TOWER ELEVATION - NORTH
- DRA A5.13 NORTH TOWER ELEVATION - EAST
- DRA A5.14 NORTH TOWER ELEVATION - SOUTH

- DRA A10.01 RENDERING - SITE - SOUTHWEST CORNER
- DRA A10.02 RENDERING - SOUTH TOWER - COURTYARD

- DRA A11.01 EXISTING SITE PHOTO
- DRA A11.02 EXISTING SITE PHOTO
- DRA A11.03 EXISTING SITE PHOTO
- DRA A11.04 EXISTING SITE PHOTO

- DRA A12.01 DS&G DIAGRAM - MAXIMUM HEIGHT
- DRA A12.02 DS&G DIAGRAM - MAXIMUM PLAN DIMENSION
- DRA A12.03 DS&G DIAGRAM - MAXIMUM DIAGONAL
- DRA A12.04 DS&G DIAGRAM - MAXIMUM APPARENT FACE 1
- DRA A12.05 DS&G DIAGRAM - MAXIMUM APPARENT FACE 2
- DRA A12.06 DS&G DIAGRAM - APPARENT CHANGE IN FACE
- DRA A12.07 DS&G DIAGRAM - COMPOUND SHAPE BUILDINGS
- DRA A12.08 DS&G DIAGRAM - TOWER SEPERATION
- DRA A12.09 DS&G DIAGRAM - SITE SETBACKS

ATTACHED APPENDIX

DESIGN STANDARD & GUIDELINE CHECKLIST
SUSTAINABILITY CHECKLIST

PROJECT DESCRIPTION

TWO 11-STORY COMPONENT TOWERS ARE PROPOSED FOR WEST SIDE OF BLOCK 6. THERE ARE TWO EXISTING TOWERS ON THE SAME BLOCK. THE NEW BUILDINGS WILL SIT ATOP A TWO-LEVEL UNDERGROUND PARKING STRUCTURE. ACCESS TO THE PARKING STRUCTURE WILL BE EITHER THRU THE BUILDING OR THRU STAND-ALONE ELEVATOR ENCLOSURES ENTERED FROM THE PEDESTRIAN PASEO AT SITE'S CENTER. THE TWO TOWERS WILL HAVE AMENITIES AT GROUND FLOOR WHICH WILL INCLUDE LOBBY, LOUNGE, GYM, AND COMMON WORK AREAS. THE GROUND FLOORS WILL ALSO FEATURE RESIDENTIAL UNITS TO PROVIDE SUBDIVIDED CHARACTER TO THE STREET FRONT. THE STORIES ABOVE ARE TO ALL BE MULTIFAMILY RESIDENTIAL DWELLING UNITS. THE BUILDINGS WILL BE CONFIGURED TO FEATURE GREEN OPEN SPACE DEDICATED TO THE BUILDING AT BOTH GROUND LEVEL AND ROOF DECK.



TOWER AND UNITS

		Unit Type Unit Area	Tower North													Common	Lobby	Fitness	Gross Floor Area
			0.1	J1.1	J1.1	J1.1	J1.1	J1.1	2.2	2.2	2.2	P2.2	3.2	3.2	P3.2				
			392	537	593	608	625	647	875	882	1 000	1, 167	1, 114	1, 321	1, 491				
			Total Units																
Residential	Rooftop															1 082			5, 940
	11		2	1	0	1	0	1	1	0	0	1		1	1	10			11, 570
	10		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	9		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	8		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	7		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	6		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	5		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	4		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	3		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
Lobby/Resid	2		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	1		0	0	1	0	2	0	1	1	1	0		0	0	6	2, 094	243	905
	Total Units		20	10	10	10	20	1	20	10	1	1	10	10	1	124			
	Percentage of Total		16%	8%	8%	8%	16%	1%	16%	8%	1%	1%	8%	8%	1%				
	TOTAL AREA		7, 840	5, 370	5, 930	6, 080	12, 500	647	17, 500	8, 820	1, 000	1, 167	11, 140	13, 210	1, 491		3, 176	243	905

		Unit Type Unit Area	Tower South													Common	Lobby	Fitness	Gross Floor Area
			0.1	J1.1	J1.1	J1.1	J1.1	J1.1	2.2	2.2	2.2	P2.2	3.2	3.2	P3.2				
			392	537	593	608	625	647	875	882	1 000	1, 167	1, 114	1, 321	1, 491				
			Total Units																
Residential	Rooftop															1 082			5, 940
	11		2	1	0	1	0	1	1	0	0	1		1	1	10			11, 570
	10		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	9		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	8		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	7		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	6		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	5		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	4		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	3		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
Lobby/Resid	2		2	1	1	1	2	0	2	1	0	0		1	1	12			11, 002
	1		0	0	1	0	2	0	1	1	1	0		0	0	6	2, 094	243	905
	Total Units		20	10	10	10	20	1	20	10	1	1	10	10	1	124			
	Percentage of Total		16%	8%	8%	8%	16%	1%	16%	8%	1%	1%	8%	8%	1%				
	TOTAL AREA		7, 840	5, 370	5, 930	6, 080	12, 500	647	17, 500	8, 820	1, 000	1, 167	11, 140	13, 210	1, 491		3, 176	243	905

DESIGN STANARDS AND GUIDELINES APPENDIX A COMPLIANCE

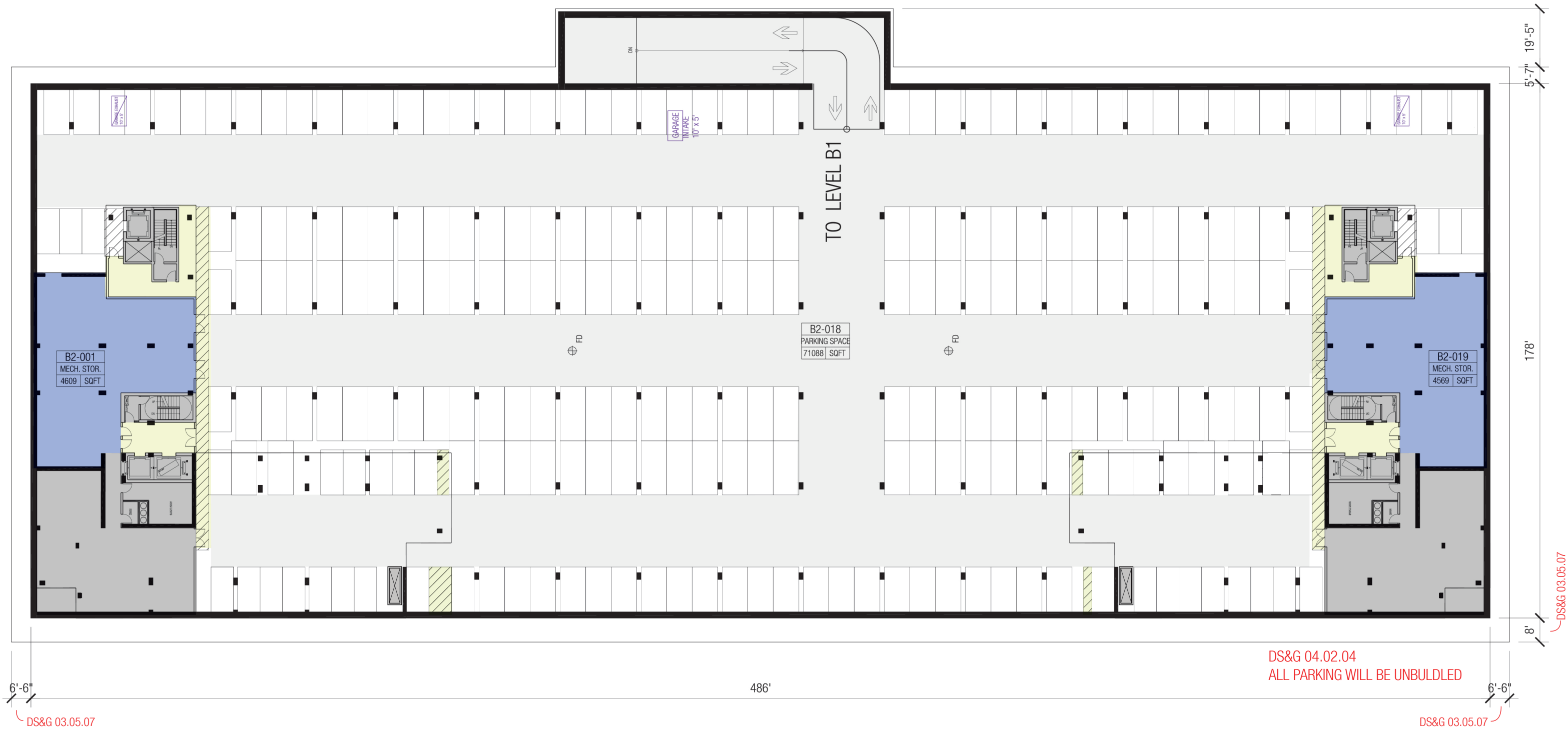
		Permitted	Provided
DSG §03.02.03	Building Footprint (required = max value)	30, 473	22, 150
	Existing Building Footprint:	29, 557	29, 557
	Common Open Space	8,256	8,392
	Private Open Space	2,808	5,451
	Total Parcel Area	200, 099	200, 099
DSG §03.02.01	Lot Coverage	5-30%	25.84%

PARKING AND TRANSPORTATION

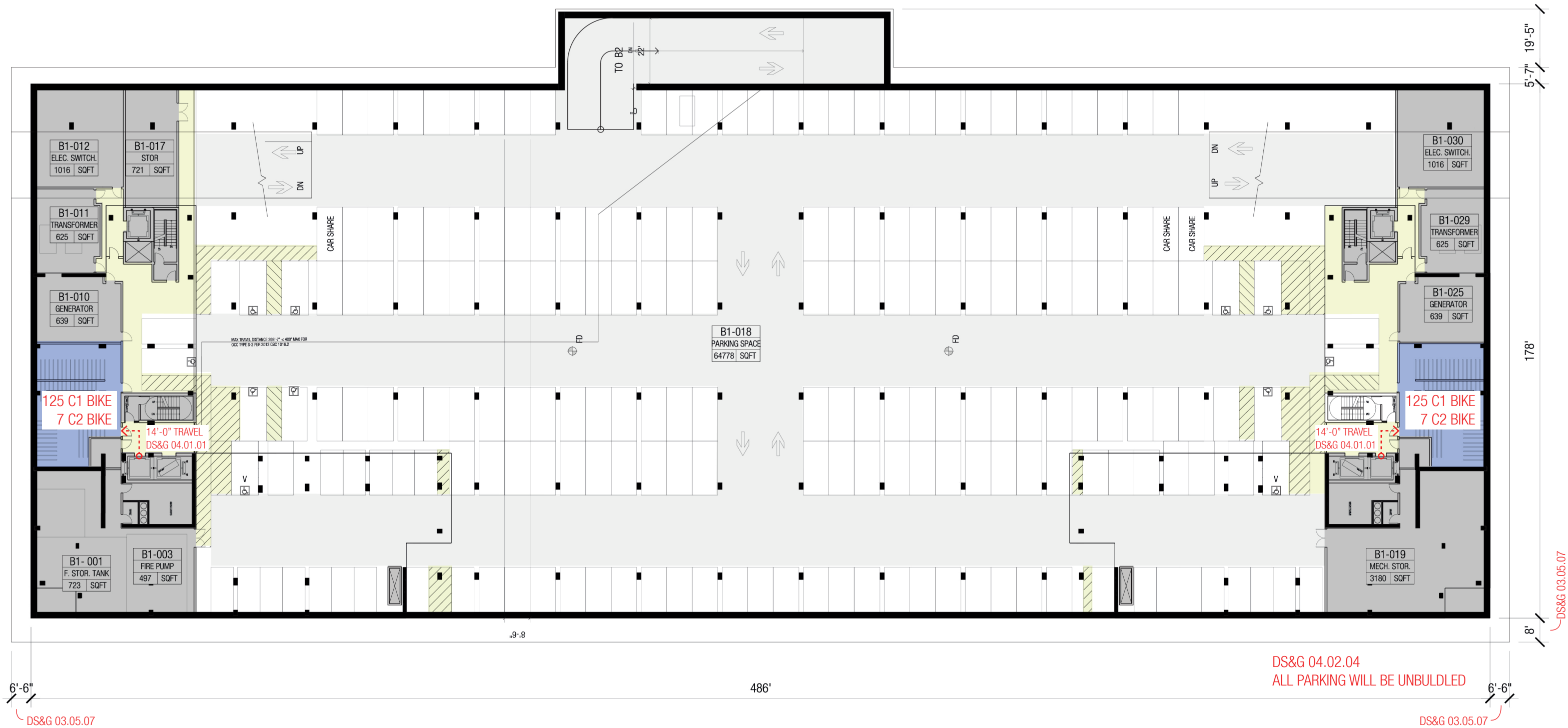
		Permitted	Provided
DSG §04.01.01	Bike Parking (Class 1)	212	250
	Bike Parking (Class 2)	14	14
	Parking Area	160, 000	142, 914
	Standard Parking Spaces	*	434
	Handicap Spaces	9	9
	Van Spaces	1	2
	Car Share Spaces	3	3
	Off-Street Loading Spaces	0	0
	Total Off-Street Parking Spaces	*	445
	On Street Loading	2	2

*Total number of units at completion of Phase 1A is estimated to be 3,610 units. Block 6 is providing 448 new parking spaces bringing the total parking count to 3,787. This yields a surplus of 174 spaces. Pursuant to the requirements of Planning Code section 3.3.2, the 174 spaces in excess of the 1:1 parking ratio during Sub-phase 1A will be cordoned off and brought on-line as new units are constructed during subsequent sub-phases.

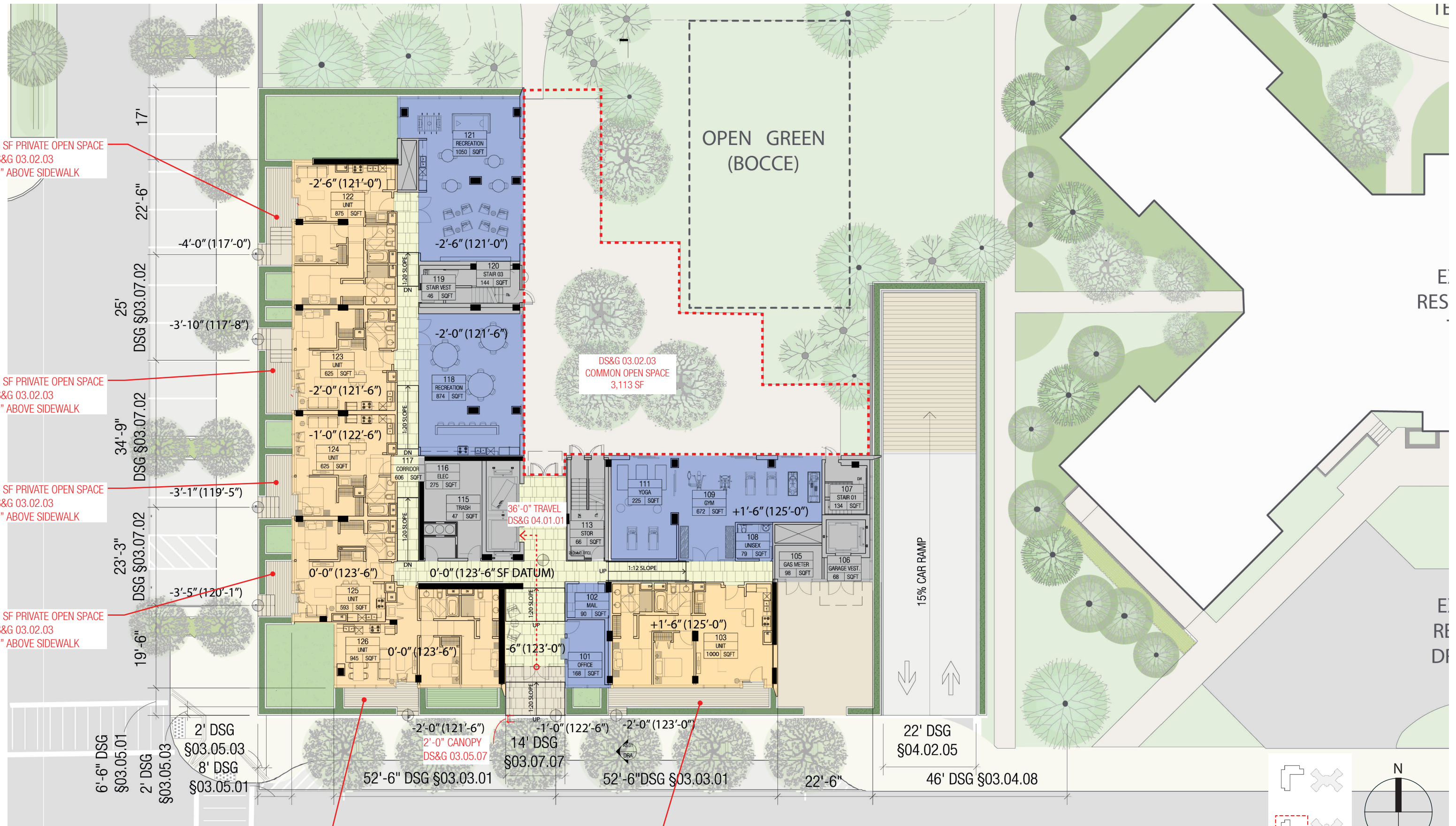




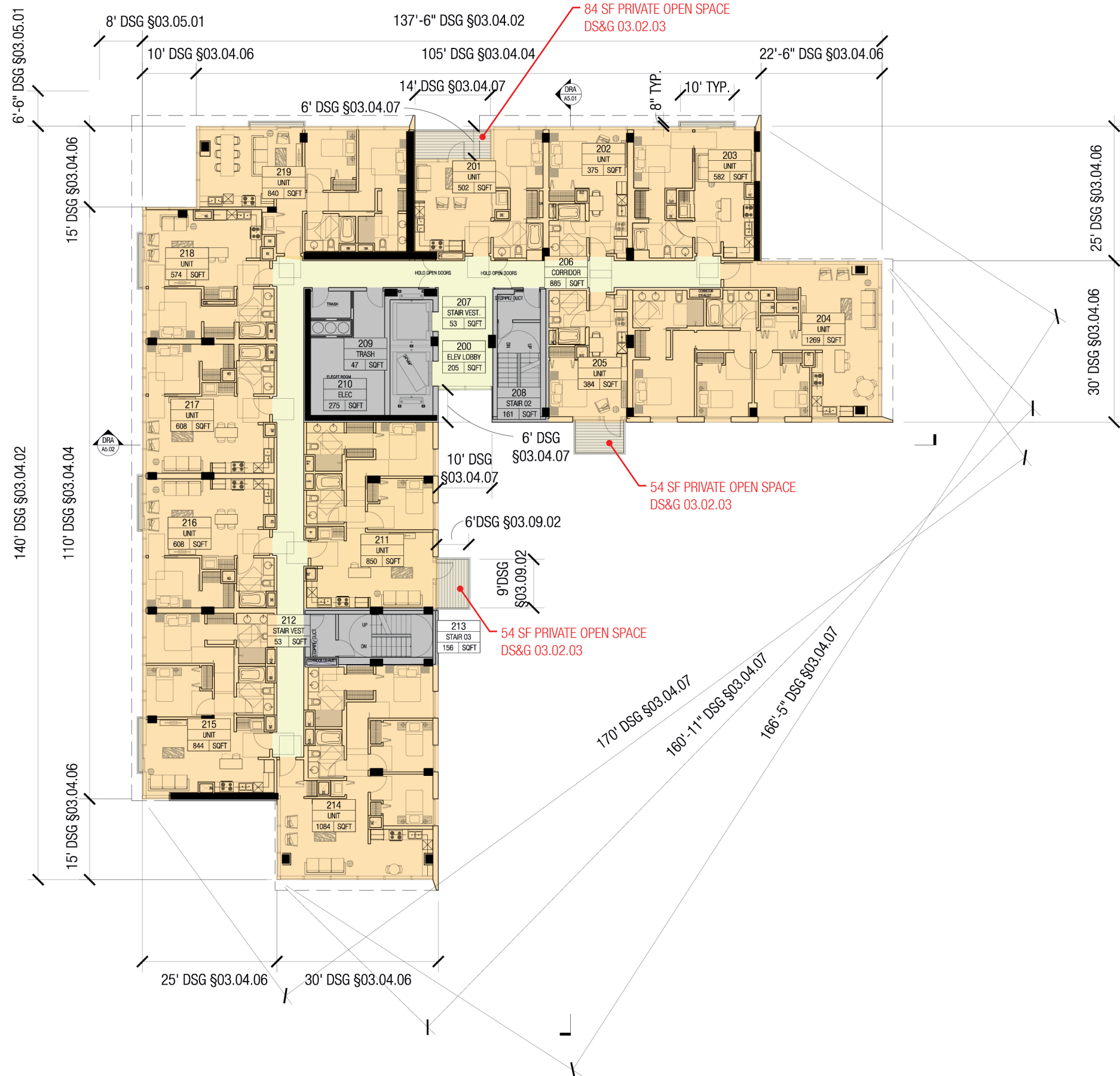
SCALE: 1'-0"=1/32"



SCALE: 1'-0"=1/32"



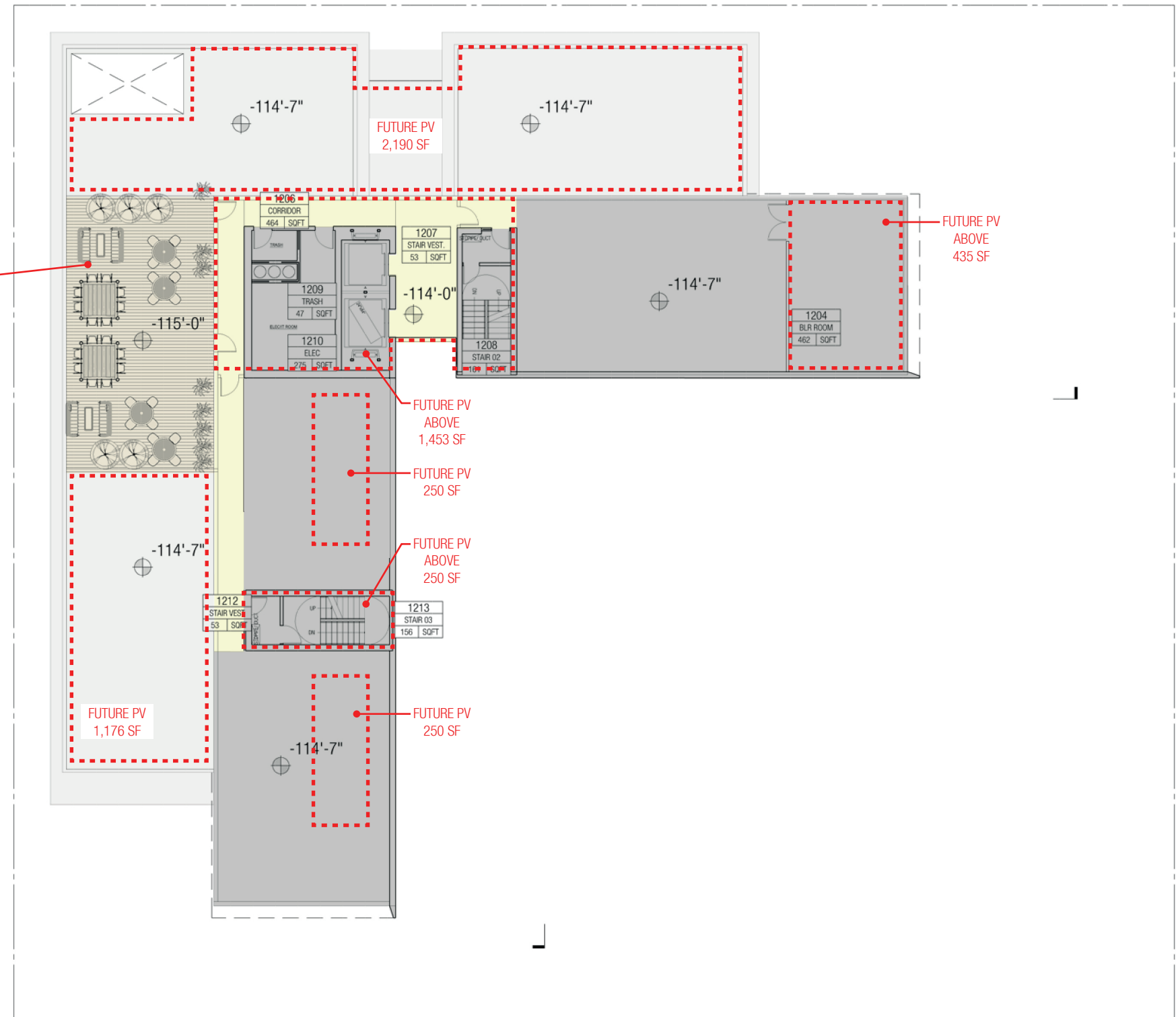
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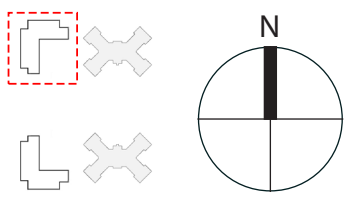
SCALE: 1'-0"= 3/64"

SCALE: 1'-0"= 3/64"

1083 SF COMMON OPEN SPACE
DS&G 03.02.03



DS&G 03.10.02	
ROOF AREA PER TOWER:	11,759 SF
DESIGNATED FUTURE PV REQUIRED:	5,880 SF
DESIGNATED FUTURE PV PROVIDED:	6,004 SF



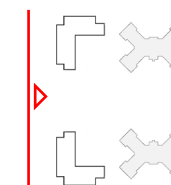


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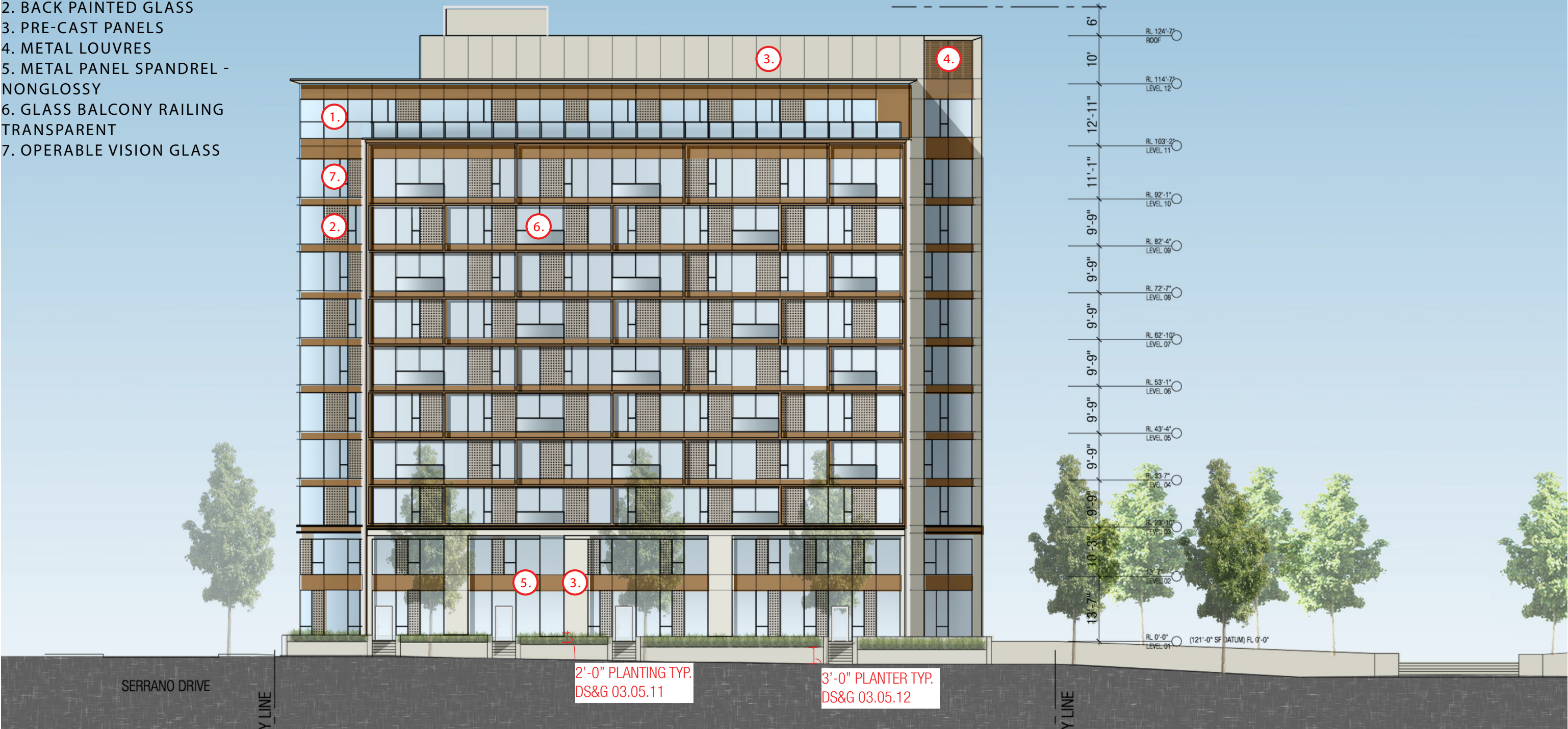
Note:
No discontinuous planting >42" & in excess of 15' length are
exhibited within private setbacks.

SCALE: 1" = 40'



MATERIAL KEY

- 1. VISION GLASS
- 2. BACK PAINTED GLASS
- 3. PRE-CAST PANELS
- 4. METAL LOUVRES
- 5. METAL PANEL SPANDREL - NONGLOSSY
- 6. GLASS BALCONY RAILING TRANSPARENT
- 7. OPERABLE VISION GLASS



SCALE: 1"= 20'

MATERIAL KEY

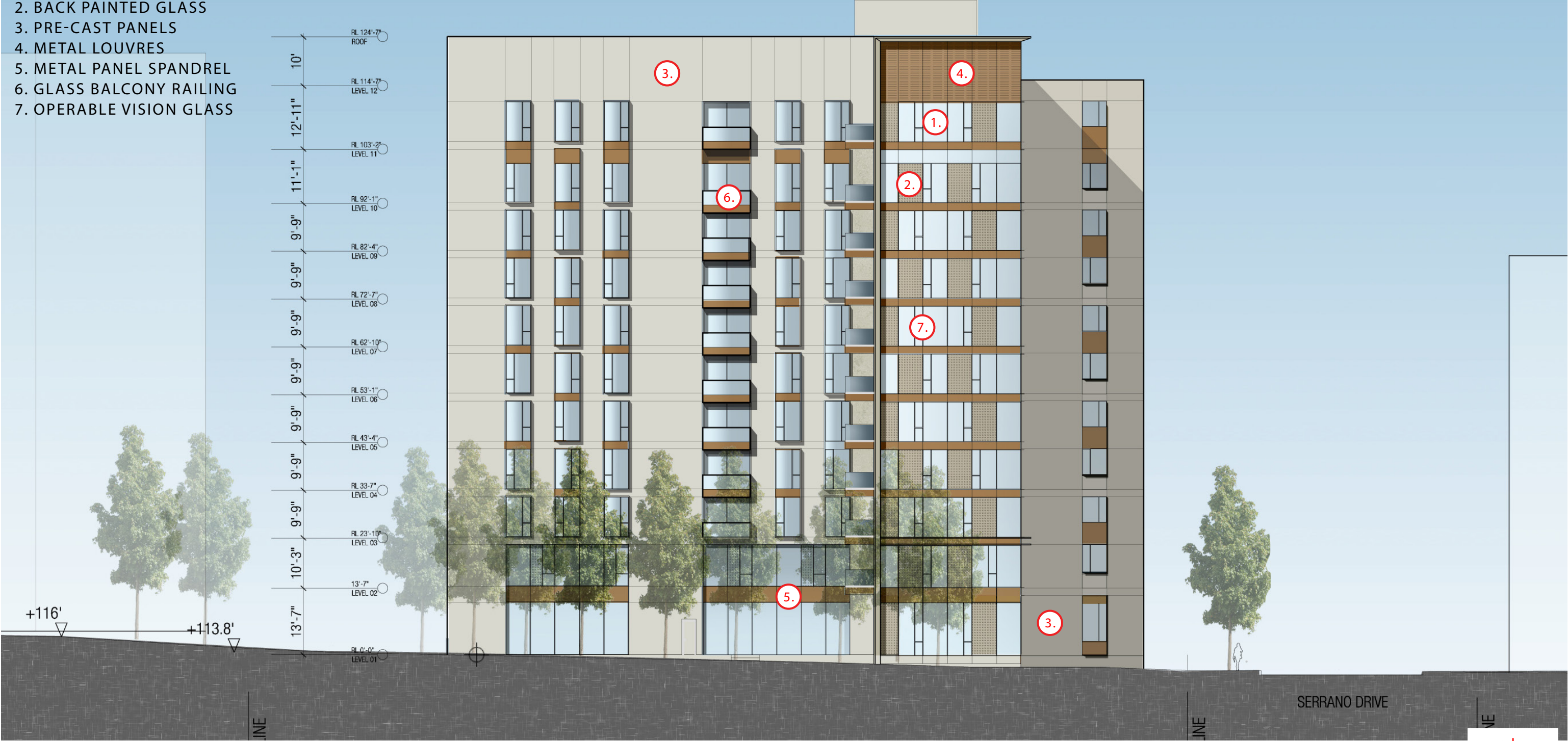
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SCALE: 1"= 20'

MATERIAL KEY

- 1. VISION GLASS
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- 7. OPERABLE VISION GLASS



SCALE: 1"= 20'

MATERIAL KEY

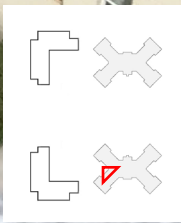
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- 7. OPERABLE VISION GLASS

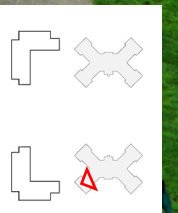


SCALE: 1"= 20'

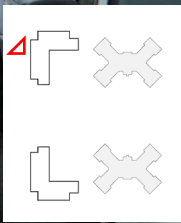












Parkmerced Block 06 - Sustainability Plan Checklist — Design Review Compliance Checklist for Buildings 04.23.15																							
Standard Number	Standard	Project Compliance																					
Page 19	Comply with the requirements of Chapter 01 (Land Use) of the Parkmerced Design Standards and Guidelines	See Design Standards and Guidelines Compliance Checklist																					
Page 23	Meet the requirements of Chapter 04 (Parking, Loading + Servicing) of the “Parkmerced Design Standards + Guidelines”	See Design Standards and Guidelines Compliance Checklist																					
Page 29	Design each building to divert, upon completion of the hydrology system, 100% of storm water for at least a 5-year storm event with a duration of 3 hours to the Parkmerced hydrology system without discharge to the City’s combined sewer-storm water system	100% of the roof run-off will be infiltrated within the block and will not be discharged to the City’s combined sewer system from the 5-year, 3 hour storm.																					
Page 29	Comply with the requirements of the San Francisco Building Code Chapter 13C (Green Building Requirements)	The Green Building Requirements that went into effect on January 1, 2014 have superseded the San Francisco Building Code Chapter 13C requirements. The project will comply with the current San Francisco Green Building requirements. Compliance will be demonstrated through LEED Silver Certification or Greenpoint Rating of 75 points of higher.																					
Page 29	Comply with the requirements of the Stormwater Management Ordinance (Ordinance 83-10; File No. 100102)	The project will comply with the requirements of the Stormwater Management Ordinance (Ordinance 83-10; File No. 100102).																					
Page 30	Meet the requirements of Chapters 02.16 through 02.26 (Open Space) of the “Parkmerced Design Standards + Guidelines”	See Design Standards and Guidelines Compliance Checklist																					
Page 41	If a recycled water source is made available to Parkmerced from a municipal source in quantities sufficient for irrigation, toilet flushing and laundry, design new buildings to have 60% less designed demand for potable water as compared to existing buildings	A recycled water source has not been made available to Parkmerced from a municipal source at this time.																					
Page 41	If a recycled water source is made available to Parkmerced from a municipal source in quantities sufficient for such purposes, use 100% recycled water for irrigation	Dedicated Recycled water services for irrigation purposes will be provided for each block. If made available, landscape irrigation will use 100% recycled water, assuming the water quality is sufficient for the health of the plants at Parkmerced.																					
Page 41	Install low-flow water fixtures in all new residential and non-residential buildings.	All new buildings will be specified with efficient low flow water fixtures as defined in the table below: <table><tr><td></td><td>Baseline</td><td>Design</td></tr><tr><td>Water Closets</td><td>1.6 gpf</td><td>1.6/0.9 gpf dual flush or 1.28 gpf single flush</td></tr><tr><td>Lavatories</td><td>1.5 gpm</td><td>1.5 gpm</td></tr><tr><td>Showers</td><td>2.0 gpm</td><td>1.5 gpm</td></tr><tr><td>Kitchen Faucets</td><td>1.8 gpm</td><td>1.5 gpm</td></tr><tr><td>Dishwashers</td><td>6.5 gal/cycle</td><td>2.9 gal/cycle</td></tr><tr><td>Washing machines</td><td>≤ 9.5 water factor</td><td>≤ 6.0 water factor</td></tr></table>		Baseline	Design	Water Closets	1.6 gpf	1.6/0.9 gpf dual flush or 1.28 gpf single flush	Lavatories	1.5 gpm	1.5 gpm	Showers	2.0 gpm	1.5 gpm	Kitchen Faucets	1.8 gpm	1.5 gpm	Dishwashers	6.5 gal/cycle	2.9 gal/cycle	Washing machines	≤ 9.5 water factor	≤ 6.0 water factor
	Baseline	Design																					
Water Closets	1.6 gpf	1.6/0.9 gpf dual flush or 1.28 gpf single flush																					
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Kitchen Faucets	1.8 gpm	1.5 gpm																					
Dishwashers	6.5 gal/cycle	2.9 gal/cycle																					
Washing machines	≤ 9.5 water factor	≤ 6.0 water factor																					
Page 49	Design new residential building envelopes to perform a minimum of 15% more efficiently than current Title 24 (2008) standards and all other buildings and building components to exceed current Title 24 (2008) standards by a minimum of 10%. In the future and as technology continues to advance, the Project Sponsor will endeavor to improve upon updated Title 24 standards	Residential envelopes are designed to perform a minimum of 15% more efficiently than Title 24 2008 envelope requirements. Compliance has been demonstrated using Energy Pro software (approved by the California Energy Commission for Title 24 compliance analysis).																					
Page 49	Install one vampire outlet per room controlled by one master switch near the front door to the dwelling unit	This requirement will be included in the design of the residential units. At least one controlled outlet will be installed per room controlled by one master switch near the front door to the dwelling unit.																					
Page 49	Install Tier 1 or better appliances in residential units	Tier 1 or better appliances (as defined by the Consortium for Energy Efficiency, and used by the Energy Star rating system) will be used in the residential units. See PAE’s Appliance Review Memo dated 04-03-2015.																					
Page 49	A measurement and verification plan should be implemented	A measurement and verification plan will be implemented for the project.																					

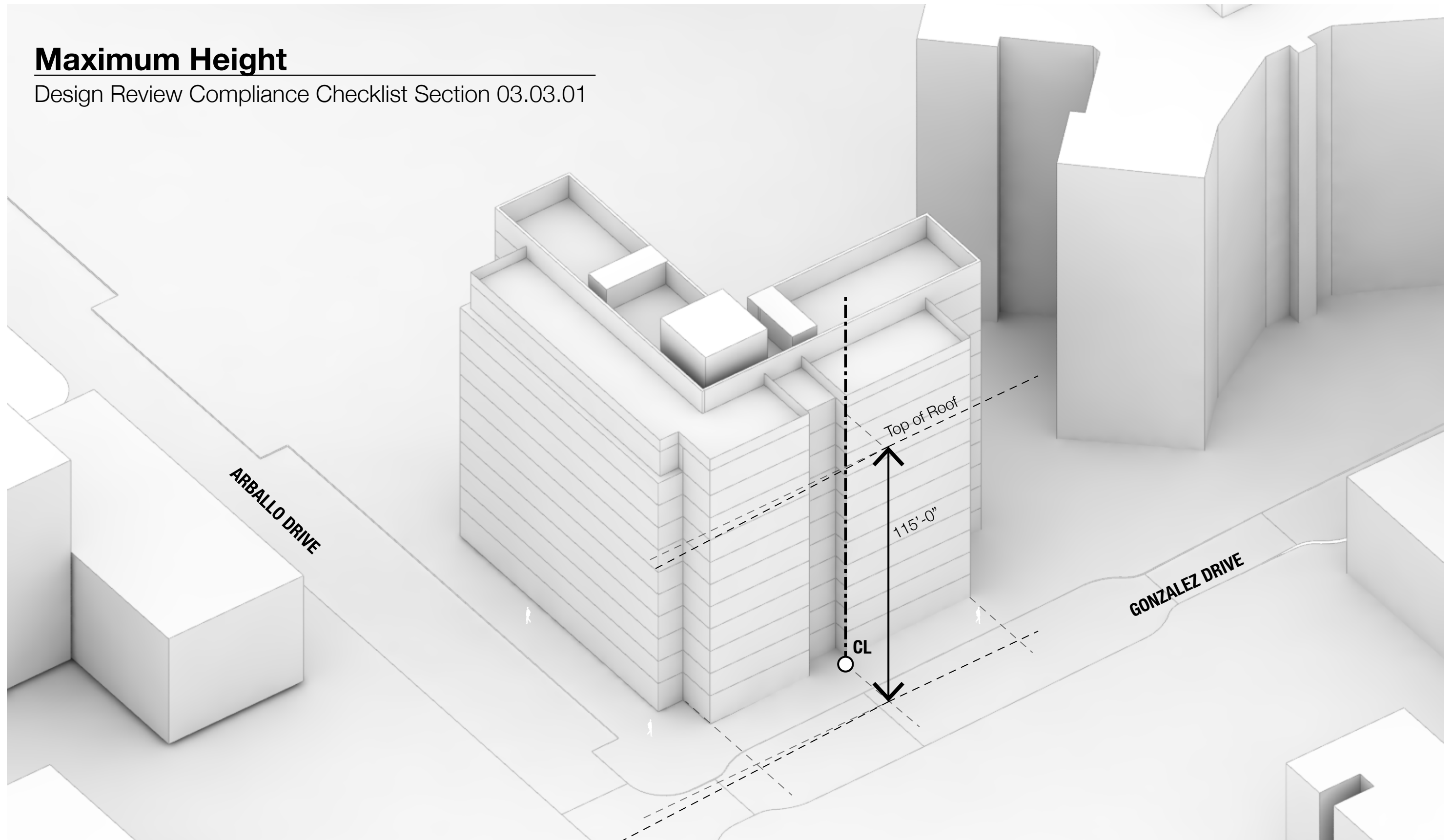
Parkmerced Block 06 - Sustainability Plan Checklist — Design Review Compliance Checklist for Buildings 04.23.15		
Standard Number	Standard	Project Compliance
Page 51	<p>The commitment to producing at least 10,396,625 kWhr/yr of renewable energy and 10,396,625 kWhr/yr electricity through a cogeneration facility, or some combination of both, but in no event less than 20,793,250 kWhr/yr, or otherwise satisfying this same 20,793,250 kWhr/yr commitment through energy efficiency and conservation measures is a significant benefit.</p> <ul style="list-style-type: none">- By full build-out, provide, either on- or off-site, renewable energy generation systems, such as solar, wind, hydrogen fuel-cells, small-scale or micro hydroelectric, and/or biomass, with the production of at least 10,396,625 kWhr/yr of the estimated total annual energy consumption;- By full build-out, generate 10,396,625 kWhr/yr of the estimated total annual energy consumption from an on-site cogeneration system; or <p>Providing a combination of power from the above two sources, or satisfying the combined 20,793,250 kWhr/yr requirement through energy efficiency or conservation savings</p>	<p>The project will demonstrate compliance with this requirement through a combination of energy efficiency savings versus the projected 18,382 kWh/yr per new residential unit energy use identified in the Development Agreement, Exhibit Q, Table 1 and compliance methods 1, 2 or 4 indicated below.</p> <p>Notes:</p> <p>The Development Agreement identifies four methods for demonstrating compliance with this requirement:</p> <ol style="list-style-type: none">1. Developer's construction and completion of on- or off-site facilities that meet 1,830.7 kWh/yr/new unit of renewable energy and 1,830.7 kWh/yr/new unit of cogeneration.2. Developer's payment to third party under contract to provide or construct renewable energy capacity that meet the 1,830.7 kWh/yr/new unit of renewable energy and 1,830.7 kWh/yr/new unit requirements by the estimated completion dates of the Development Phase.3. Developer's payment to SFPUC for the SFPUC to construct or provide renewable and/or cogeneration facilities that meet the 1,830.7 kWh/yr/new unit of renewable energy and 1,830.7 kWh/yr/new unit requirements.4. Developer to pay an in-lieu fee of \$6,589 per new residential unit for Renewable Energy and \$1,671 per new residential unit for cogeneration. The funds are deposited into the Parkmerced sustainability energy Account, which may be used for the purpose of constructing cogeneration or renewable energy facilities prior to the Certificate of Final completion for the building containing the 4,000th new residential unit. <p>Several configurations of cogeneration systems have been analyzed for implementation in this phase of the project. Life Cycle cost analysis of these options is in process.</p>
Page 57	Meet the requirements of the City's Mandatory Recycling and Compost Ordinance (Ordinance No. 100-09, File No. 081404)	All trash disposed by the residents will be segregated into 3 streams: waste, mixed recycling and compost. Trash collection systems will handle each stream separately. Specific methods and systems will be delineated in the Park Merced Master Trash Management Plan and further define in each specific building Trash Management Plan
Page 57	Provide a minimum of one centralized waste pick-up location on each block	Each block will have at minimum one central trash pickup location. Typically, each building within each block will have its own trash pickup location.
Page 57	Provide one hazardous waste drop-off location within each Neighborhood Commons	A hazardous waste drop off location will be located at Block 22 at the Neighborhood Commons. The collections at this facility will match the collections of the hazardous waste facility already in place at the existing site limiting items excepted to common household items such as batteries, light bulbs and basic electronics, etc.
Page 63	Buildings will generally use a minimum of 5% salvaged, refurbished or reused materials, based on cost, of the total value of materials on the project	The building improvements will meet the required minimum of 5% salvaged, refurbished or reused materials, based on cost, of the total value of materials on the project.
Page 63	Buildings will generally use materials with recycled content such that the sum of the post-consumer recycled content plus ½ of the pre-consumer content constitutes at least 10%, based on cost, of the total value of the materials in the project	The building improvements will generally use materials with recycled content such that the sum of the post-consumer recycled content plus ½ of the pre-consumer content constitutes at least 10%, based on cost, of the total value of the materials in the project.
Page 65	<p>Create and implement an erosion and sedimentation control plan for all new construction activities associated with the project. The plan should incorporate practices such as phasing, seeding, grading, mulching, filter socks, stabilized site entrances, preservation of existing vegetation, and other best management practices (BMPs) to control erosion and sedimentation in runoff from the entire project site during construction. The plan should list the BMPs employed and describe how they accomplish the following objectives:</p> <ul style="list-style-type: none">- Prevent loss of soil during construction by storm water runoff and/or wind erosion, including but not limited to stockpiling of topsoil for reuse- Prevent sedimentation of any affected storm water conveyance systems or receiving streams <p>Prevent polluting the air with dust and particulate matter</p>	An erosion and sedimentation control plan will be created and designed by the Civil Engineer for all new construction activities associated with the project; the General Contractor will implement the erosion and sedimentation control plan utilizing industry best management practices (BMPs).

Parkmerced Block 06 - Sustainability Plan Checklist — Design Review Compliance Checklist for Buildings 04.23.15		
Standard Number	Standard	Project Compliance
Page 65	<div>- Recycle or salvage a minimum of 50% of construction waste by identifying materials to be diverted from disposal and whether the materials will be sorted on-site or co-mingled. Calculations can be done by weight or volume, but must be consistent throughout</div>	During construction, the general contractor will recycle or salvage a minimum of 50% of construction waste by identifying materials to be diverted from disposal and whether the materials will be sorted on-site or co-mingled.

Assumptions:
An average of 2.3 people occupy each residence at Parkmerced.

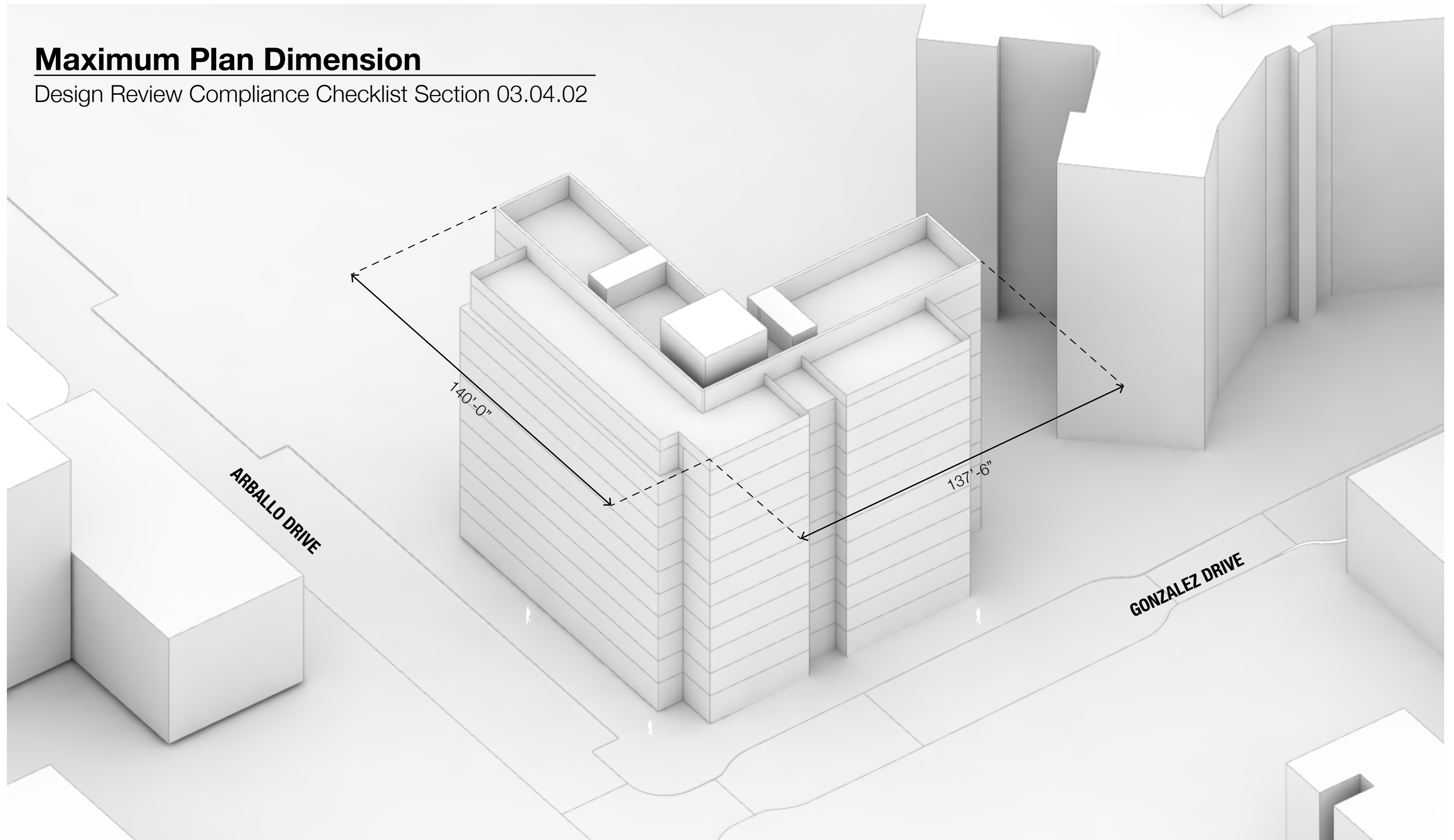
Maximum Height

Design Review Compliance Checklist Section 03.03.01



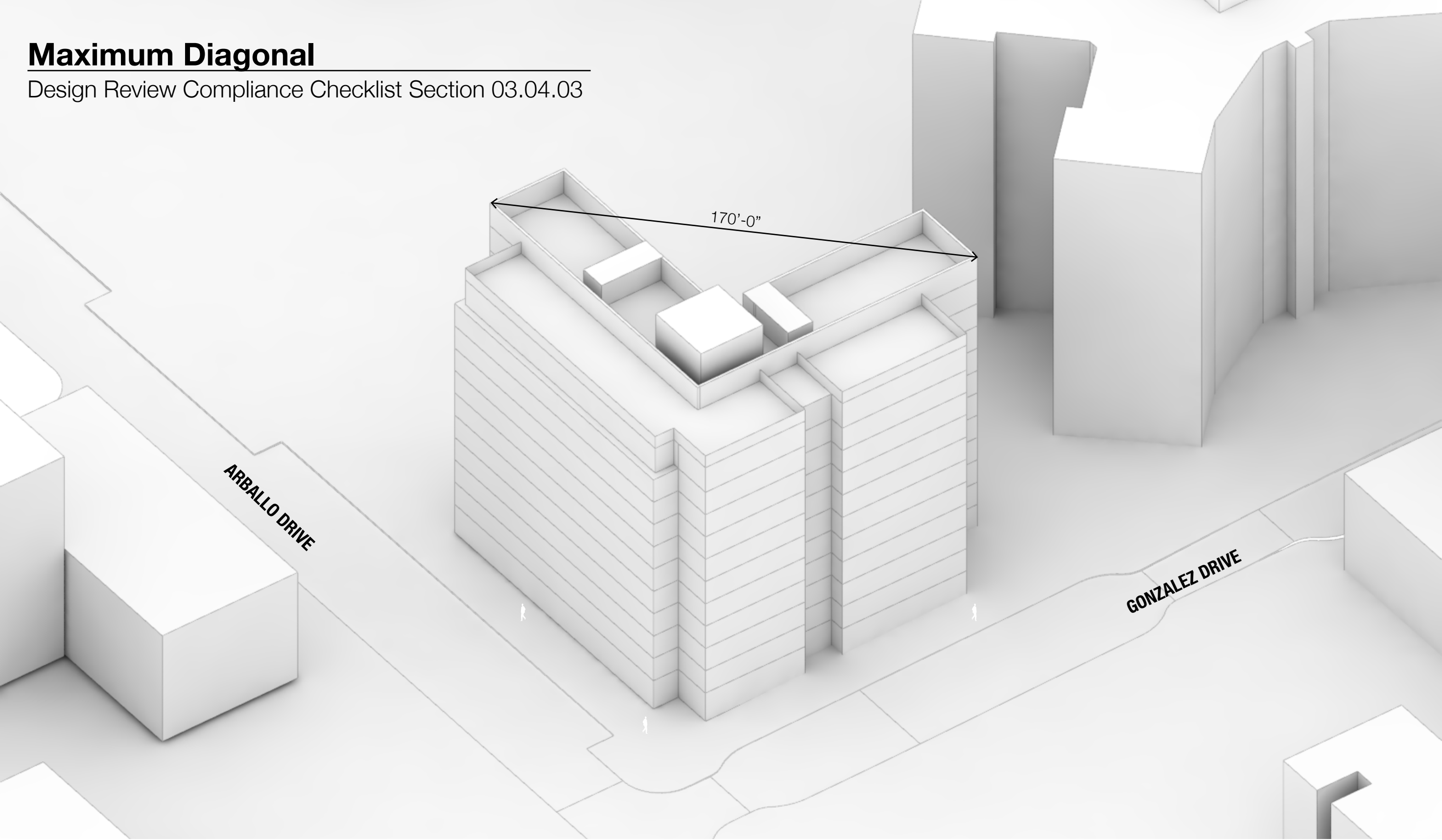
Maximum Plan Dimension

Design Review Compliance Checklist Section 03.04.02



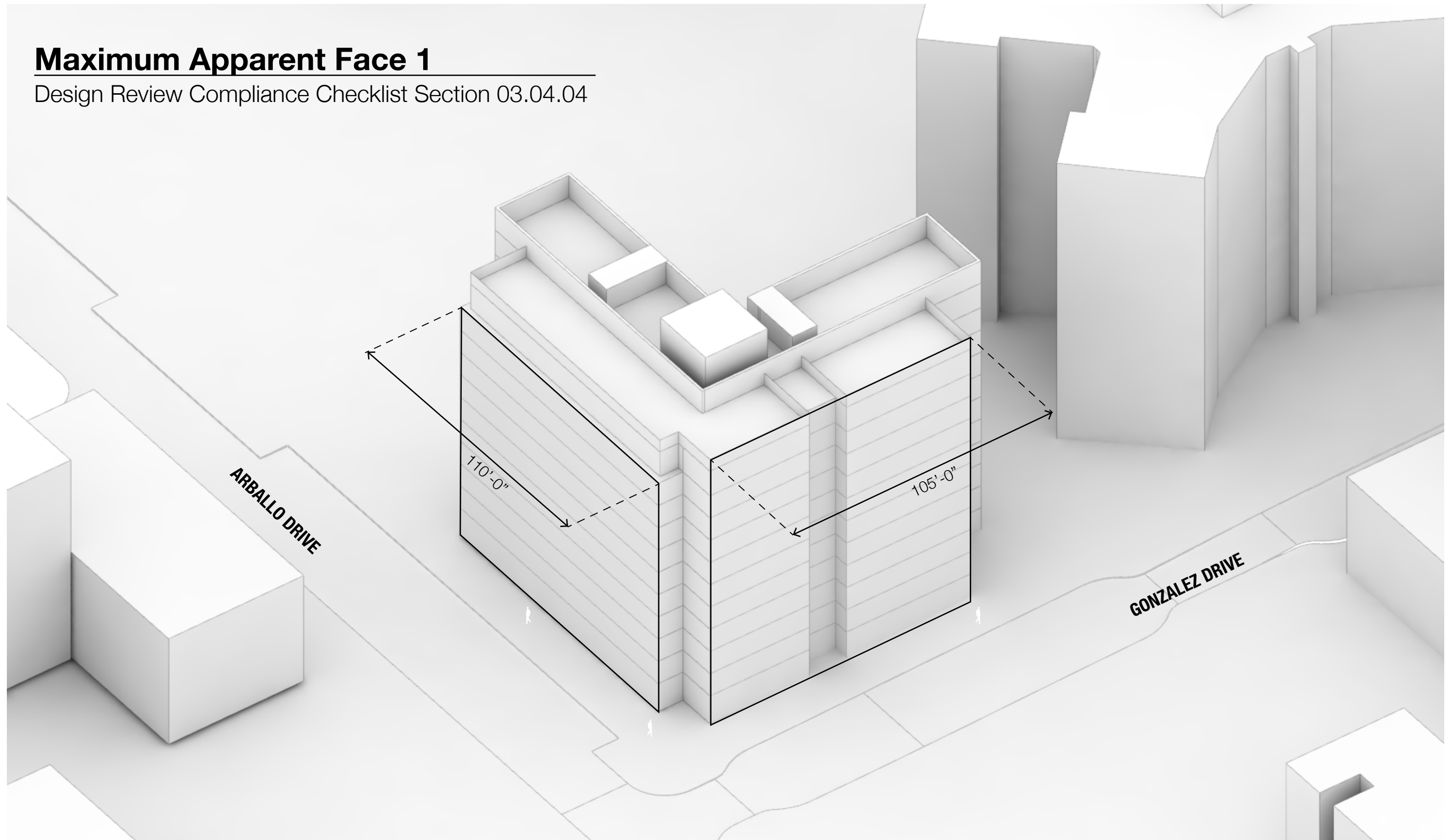
Maximum Diagonal

Design Review Compliance Checklist Section 03.04.03



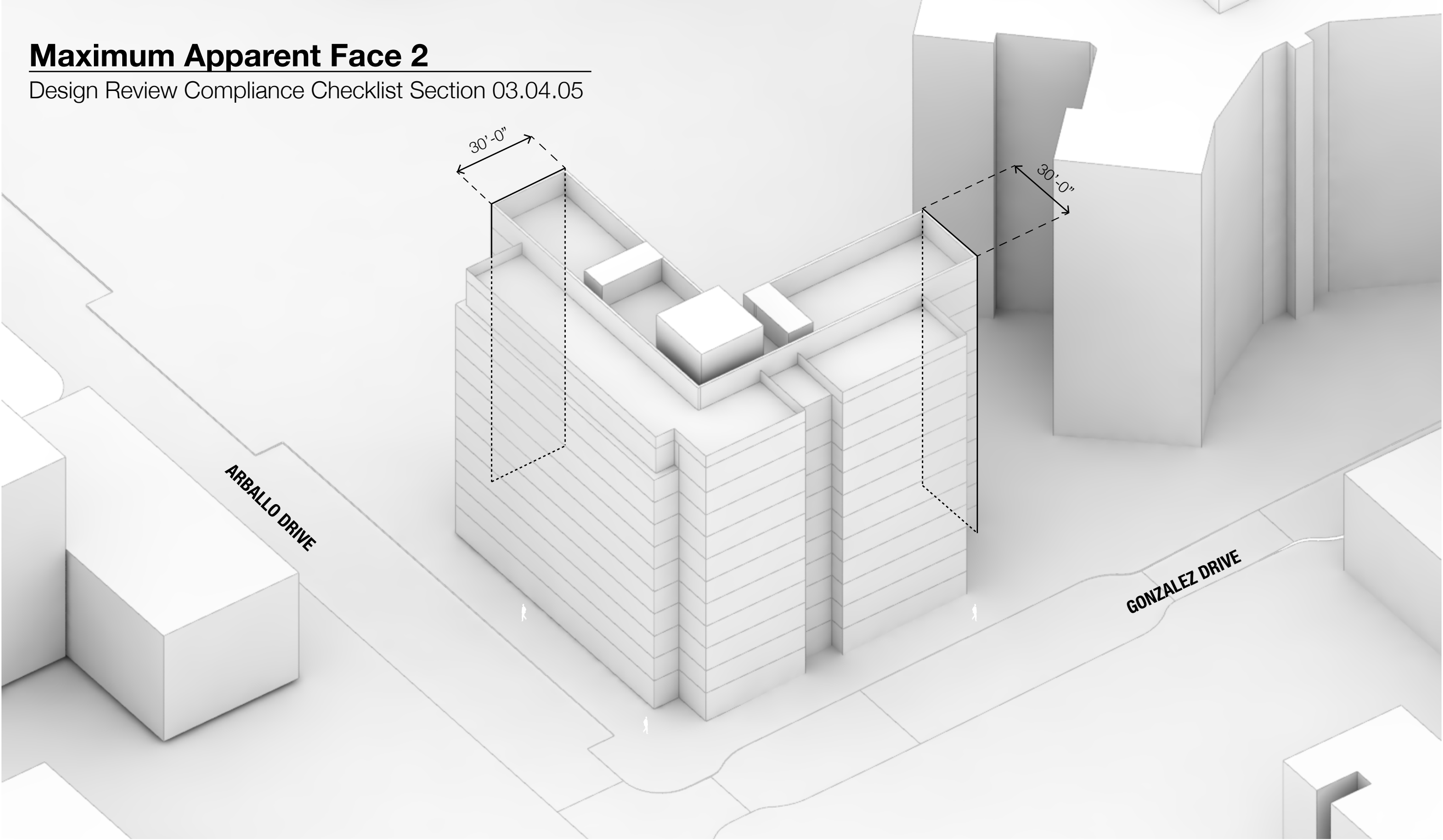
Maximum Apparent Face 1

Design Review Compliance Checklist Section 03.04.04



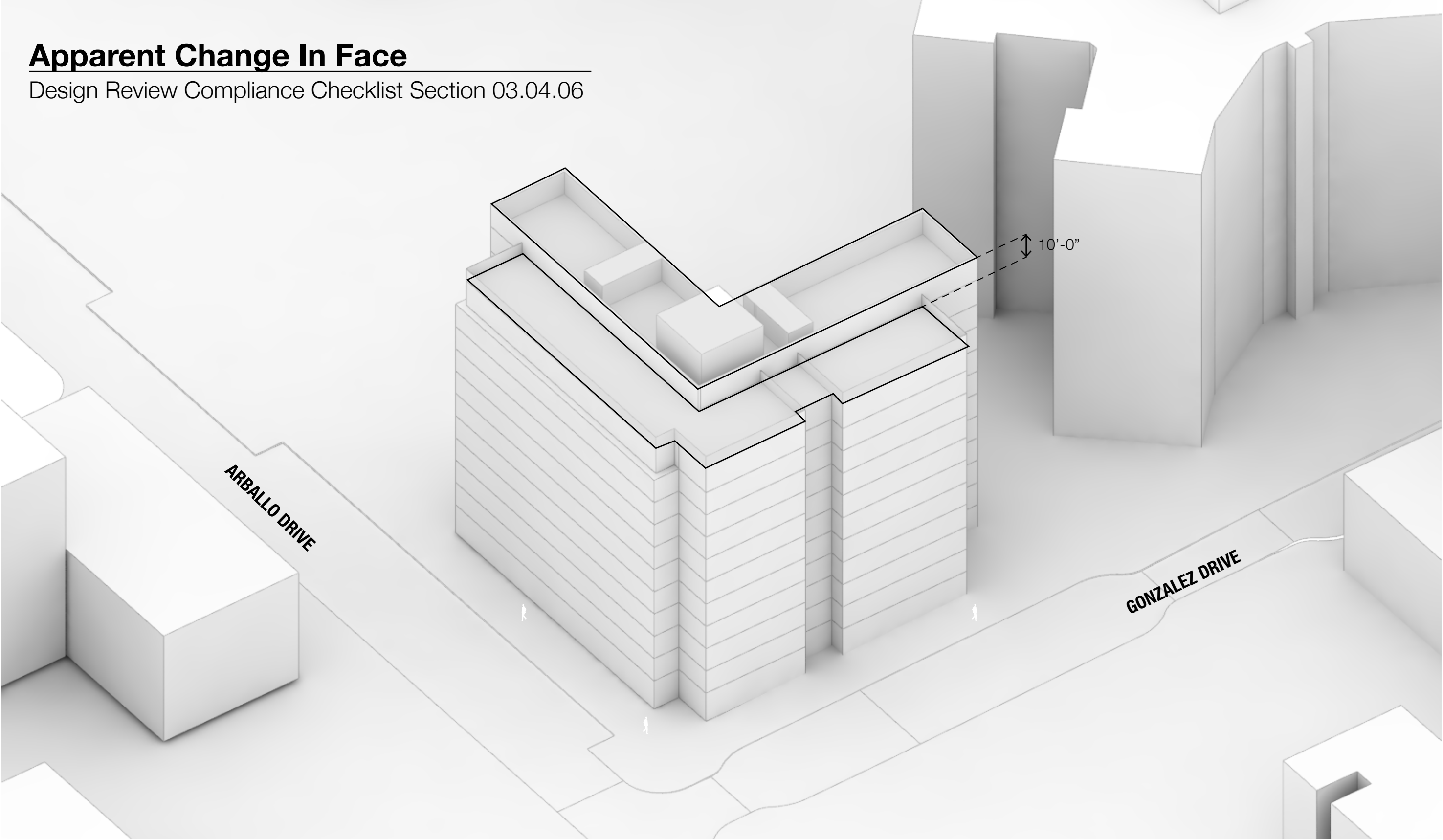
Maximum Apparent Face 2

Design Review Compliance Checklist Section 03.04.05



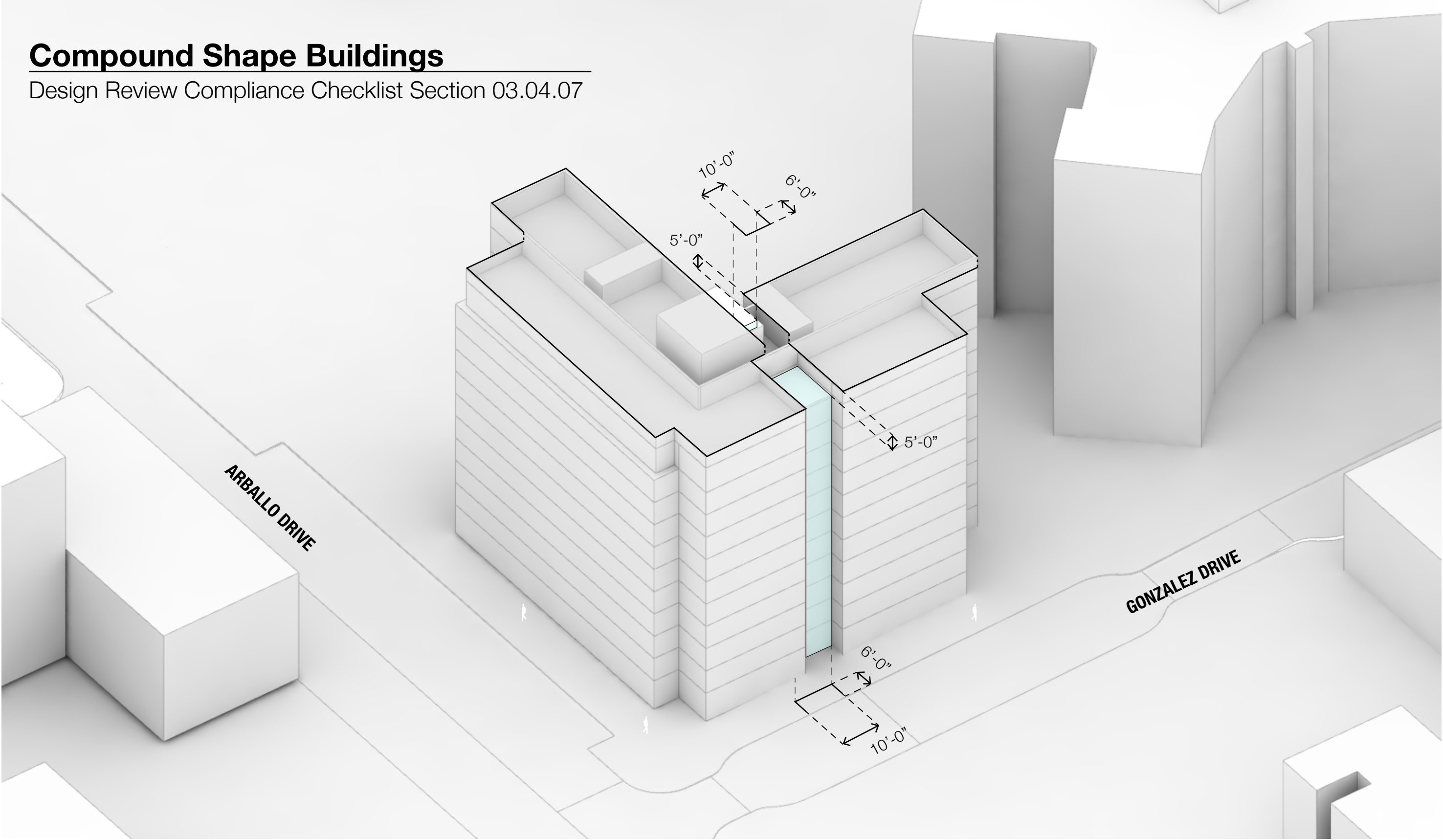
Apparent Change In Face

Design Review Compliance Checklist Section 03.04.06



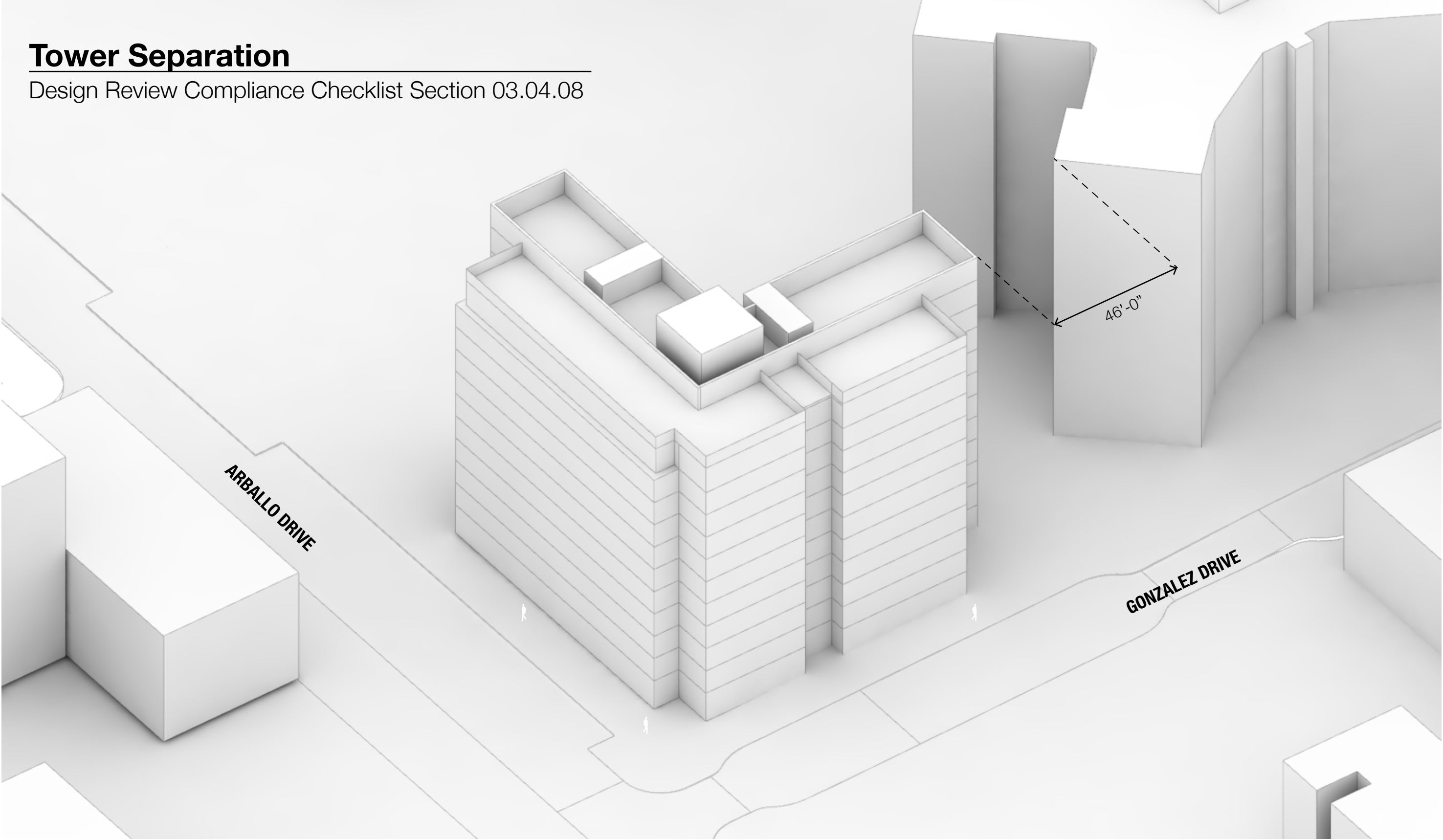
Compound Shape Buildings

Design Review Compliance Checklist Section 03.04.07



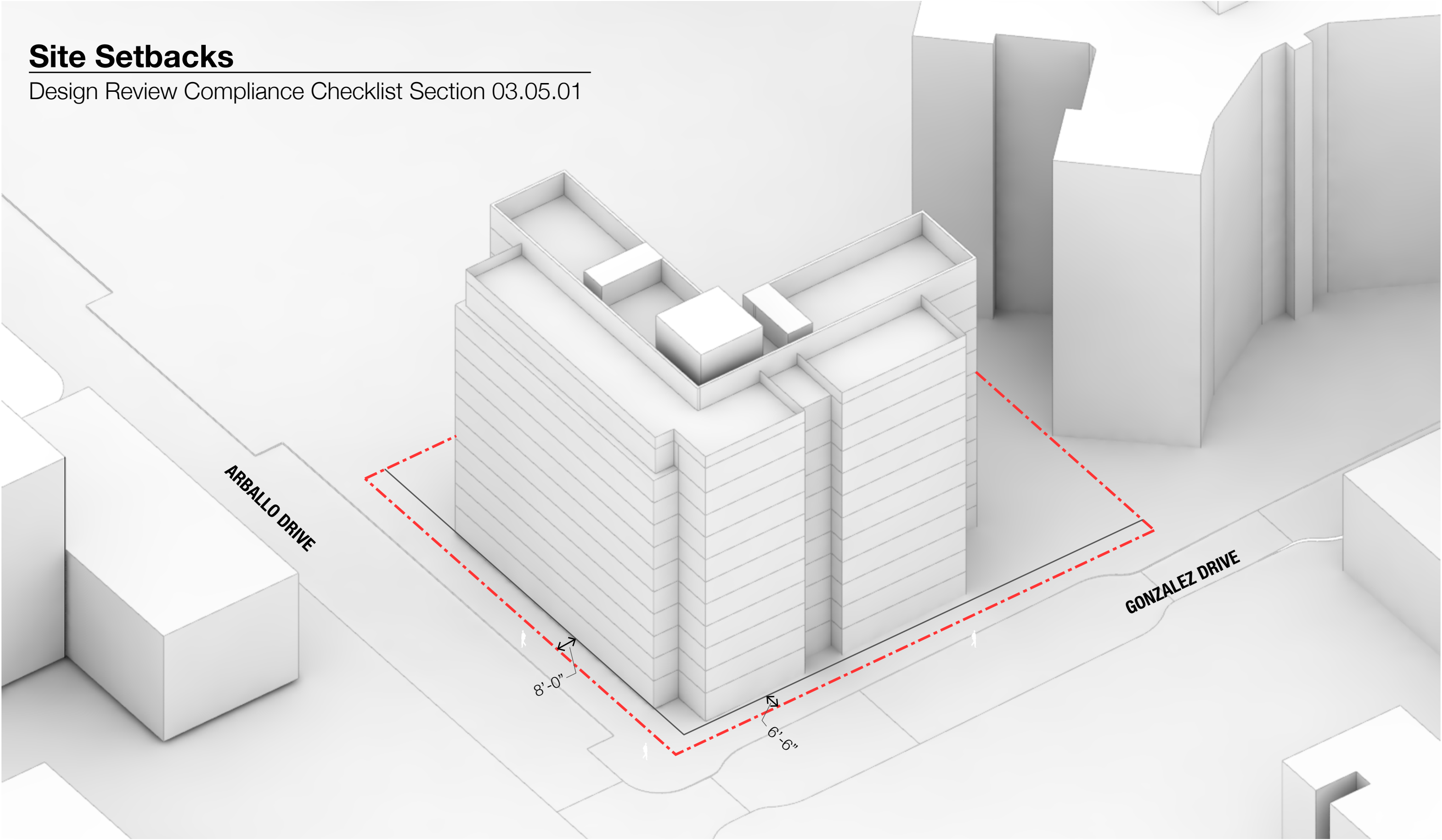
Tower Separation

Design Review Compliance Checklist Section 03.04.08

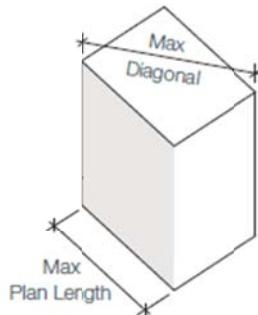
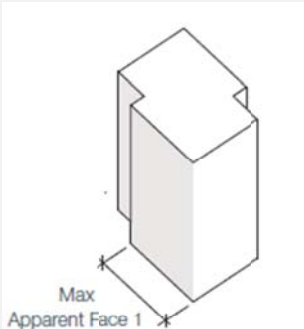


Site Setbacks

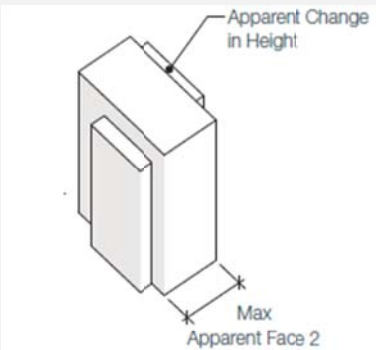
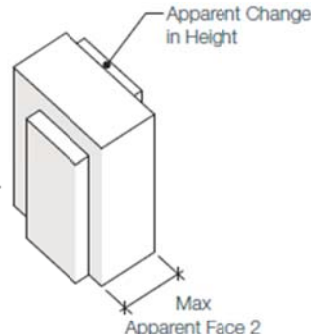
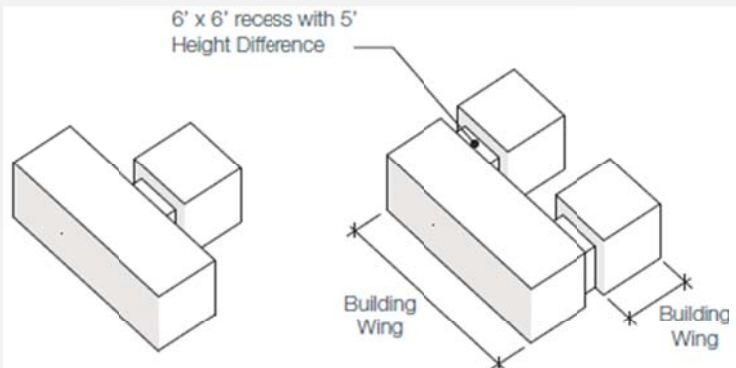
Design Review Compliance Checklist Section 03.05.01



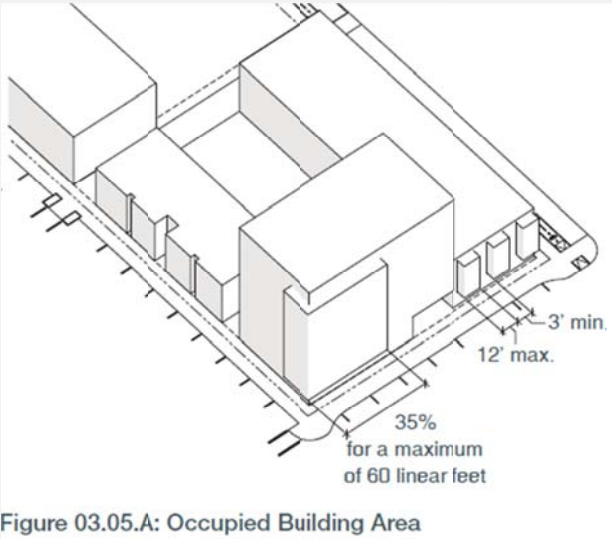
Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.01.01 Sustainability Performance	All buildings shall meet or exceed the requirements of the Parkmerced Sustainability Plan.	Block 06 will comply with all Parkmerced Sustainability Plan requirements. Refer to attached “Parkmerced Block 06 Sustainability Checklist”	
03.02.01 - 03.02.02 Lot Coverage	Lot coverage is calculated for each development block and is specifically listed in Appendix A of the Design Standards and Guidelines - Regulating Plan.	Total Block 06 Parcel area = 200,099 square feet Buildable Lots Property Area= 95,590 New Building Footprint= 11,075sf x 2 towers= 22,150 square feet Lot coverage required per DS&G 5%-30% Lot coverage provided 25.84% Refer to attached DRA A1.01	Percentage of lot coverage is defined as the total enclosed building footprint area divided by the total development block area. Designated public open spaces, such as Neighborhood Commons, are excluded from lot coverage calculations. Building encroachments, projections and obstructions as defined in Section 03.05 Building Controls - Setback are not included in the total enclosed building footprint area calculation. However, those portions of a pedestrian paseo that pass below occupied building area must be included in the total building footprint area. (03.02.02)
03.02.03 Usable Open Space	48 square feet of common open space or 36 square feet of private open space per unit. Both common and private open spaces must have a minimum dimension of 6 feet in any direction.	124 Units per Tower, 248 Total Units Number of Units with 36 SF min. Private Open Space = 76 Units 248 – 76 = 172 Units x 48 SF = 8,256 SF Required Common Open Space Roof Common Open Space per Tower Provided: 1,083 SF Ground Common Open Space per Tower Provided: 3,113 SF* Common Open Space Per Tower Provided: 4,196 SF Total Common Open Space Provided: 8,392 SF *Ground Common Open Space is only inclusive of the hardscape/terrace areas adjacent to the towers. The following areas have been excluded from these Open Space Calculations: Community Garden, bocce courts, Paseo easement zones, event terrace, and open green areas between the two towers. Refer to DRA A0.01, A2.13, A2.14, A2.15, A2.16, and A2.17	Courtyards and rooftop terraces shall count towards the provision of common open space. Setback areas, balconies and decks shall count towards the provision of private open space.
03.03.01 Maximum Height	Building height shall not exceed the maximum height as shown on the Maximum Height Plan (Fig. 03.03.C).	Building ground floor height determined by the back of curb height at the center of the new apparent building face on Serrano and Gonzalez. The dimension from these back of curb height dimensions to top of the roof of the last occupied floor is 114'-7" . The building heights are measured from the back of sidewalk grade location at the main building entries along Serrano Drive (for the north tower) and Gonzalez Drive (for the south tower). Due to sloping site conditions, the building height from the back of curb along Arballo will increase along the length of the buildings on Arballo Refer to attached DRA A12.01, DRA A5.01, DRA A5.02, and DRA A1.01 Areas indicating heights over 115'-0" height requirement are all unoccupied spaces defined in DS&G 03.03.06 Parapets 119'-0" Mechanical Screen Walls 124'-7" Plane extensions 124'-7" Elevator Overruns 130'-7" Stair Overruns 124'-7"	Photovoltaic and thermal solar collectors, rain water and fog collecting equipment, wind turbines and other sustainability components may project above the maximum height limit. (03.03.05) Those portions of a building that may project above the maximum height limit are: • Parapets up to 4 feet in height. • Mechanical enclosures and other rooftop support facilities that occupy less than 20% of the roof area up to 10 feet in height. • For buildings taller than 125 feet wall planes extensions such as those used for screening of mechanical equipment that are either 50% physically and visibly permeable or translucent, up to 10 feet in height. (03.03.06) Height limits are to be measured from the back of sidewalk grade, at the center line of the predominant building face, to the roof of the top occupied floor of each building. Height limits on sloped sites are to extend into the site horizontally from the uphill property line to the mid-point of the development block and extend from the downhill property line at an angle equal to the slope of the grade (Fig. 03.03.A). (03.03.02) Sloped roofs, in excess of 30 degrees from the horizontal, are to be measured to the midpoint of the vertical dimension of the roof. (03.03.03)

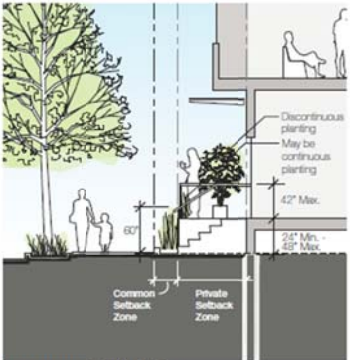
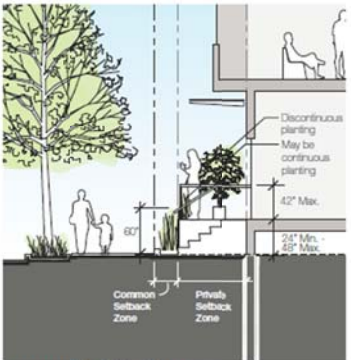
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Standard Number	Standard	Block 06 Compliance	Implementing Standards										
03.03.04 Appropriate Scale	Residential buildings that are no greater than 35 feet in height must be located along a public right-of-way or easement that is no more than 45 feet in width.	03.03.04 Does not apply. All Block 06 Residential building heights are above 35'-0"											
03.03.06 Projections	Those portions of a building that may project above the maximum height limit are: •Parapets up to 4 feet in height. •Mechanical enclosures and other rooftop support facilities that occupy less than 20% of the roof area up to 10 feet in height. •For buildings taller than 125 feet wall planes extensions such as those used for screening of mechanical equipment that are either 50% physically and visibly permeable or translucent, up to 10 feet in height.	Responses to this comment are provided in the Parkmerced Owner’s Responses to the Master Planning Comments											
03.04.02 Maximum Plan Dimension	<table><tr><th>Building Height</th><th>Max Plan Length</th></tr><tr><td>Up to 35’</td><td>NA</td></tr><tr><td>36’ – 45’</td><td>NA</td></tr><tr><td>46’ – 85’</td><td>200’</td></tr><tr><td>86’ – 145’</td><td>140’</td></tr></table>	Building Height	Max Plan Length	Up to 35’	NA	36’ – 45’	NA	46’ – 85’	200’	86’ – 145’	140’	Maximum Plan Dimension Required – 140'-0" Maximum Plan Dimension Provided – 140'-0" Refer to attached DRA A12.02, DRA A2.15	
Building Height	Max Plan Length												
Up to 35’	NA												
36’ – 45’	NA												
46’ – 85’	200’												
86’ – 145’	140’												
03.04.03 Maximum Diagonal	<table><tr><th>Building Height</th><th>Max Diagonal</th></tr><tr><td>Up to 35’</td><td>NA</td></tr><tr><td>36’ – 45’</td><td>NA</td></tr><tr><td>46’ – 85’</td><td>NA</td></tr><tr><td>86’ – 145’</td><td>170’</td></tr></table>	Building Height	Max Diagonal	Up to 35’	NA	36’ – 45’	NA	46’ – 85’	NA	86’ – 145’	170’	Maximum Diagonal Required – 170'-0" Maximum Diagonal Provided – 170'-0" Refer to attached DRA A12.03, DRA A2.15	 <p>Figure 03.04.A: Maximum Plan Length and Diagonal</p>
Building Height	Max Diagonal												
Up to 35’	NA												
36’ – 45’	NA												
46’ – 85’	NA												
86’ – 145’	170’												
03.04.04 Maximum Apparent Face 1	Face 1: The maximum apparent face width for a building face parallel to the short axis of the building or a building wing is limited as described in Table 2 – Bulk + Massing Control Matrix and Figure 03.04.B – Maximum Apparent Face 1 . <table><tr><th>Building Height</th><th>Max Apparent Face 1</th></tr><tr><td>Up to 35’</td><td>30’</td></tr><tr><td>36’ – 45’</td><td>120’</td></tr><tr><td>46’ – 85’</td><td>80’</td></tr><tr><td>86’ – 145’</td><td>110’</td></tr></table>	Building Height	Max Apparent Face 1	Up to 35’	30’	36’ – 45’	120’	46’ – 85’	80’	86’ – 145’	110’	Maximum Apparent Face 1 Width Required – 110'-0" Maximum Apparent Face 1 Width Provided – 110'-0" Refer to attached DRA A12.04, DRA A2.15	 <p>Figure 03.04.B: Maximum Apparent Face 1</p>
Building Height	Max Apparent Face 1												
Up to 35’	30’												
36’ – 45’	120’												
46’ – 85’	80’												
86’ – 145’	110’												

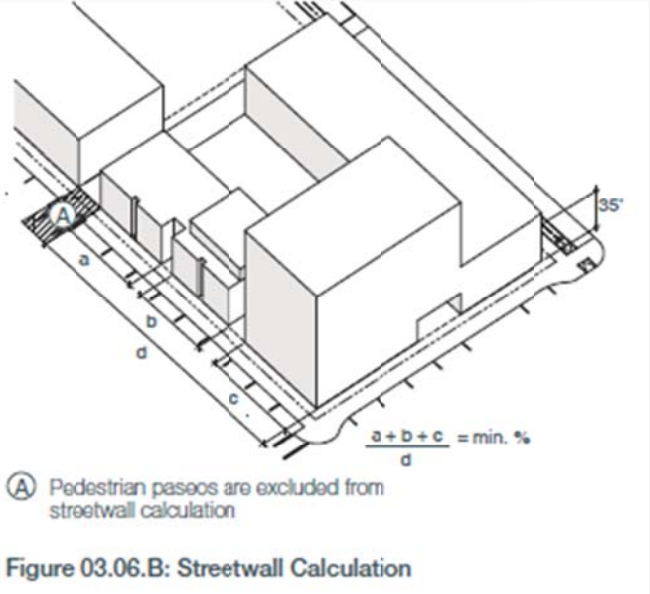
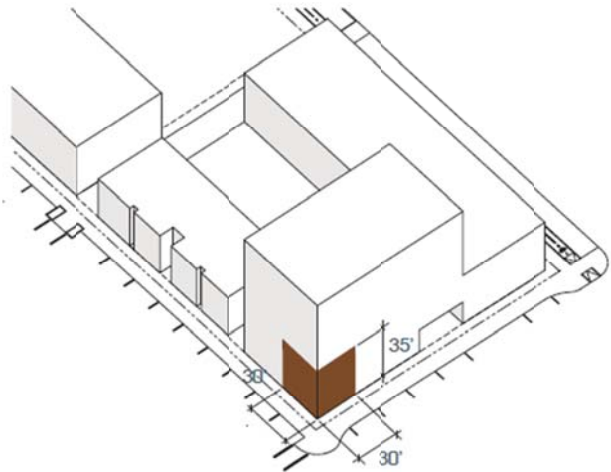
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Standard Number	Standard	Block 06 Compliance	Implementing Standards										
03.04.05 Maximum Apparent Face 2	<p>Face 2: The maximum apparent face width for a building face parallel to the long axis of the building or a building wing is limited as described in Table 2 – Bulk + Massing Control Matrix and Figure 03.04.C – Maximum Apparent Face 2 and Apparent Change in Height.</p> <table><tr><th>Building Height</th><th>Max Apparent Face 2</th></tr><tr><td>Up to 35'</td><td>NA</td></tr><tr><td>36' – 45'</td><td>80'</td></tr><tr><td>46' – 85'</td><td>40'</td></tr><tr><td>86' – 145'</td><td>40'</td></tr></table>	Building Height	Max Apparent Face 2	Up to 35'	NA	36' – 45'	80'	46' – 85'	40'	86' – 145'	40'	<p>Maximum Apparent Face 2 Width Required – 40'-0"</p> <p>Maximum Apparent Face 2 Width Provided – 30'-0"</p> <p>Refer to attached DRA A12.05, DRA A2.15</p>	 <p>Figure 03.04.C: Maximum Apparent Face 2 and Apparent Change in Height</p>
Building Height	Max Apparent Face 2												
Up to 35'	NA												
36' – 45'	80'												
46' – 85'	40'												
86' – 145'	40'												
03.04.06 Apparent Change in Height	<p>All buildings taller than 85 feet shall include a minimum change in height of 10 feet between the distinct building masses or faces generated by Standard 03.04.05.</p> <table><tr><th>Building Height</th><th>Change in Apparent Face</th></tr><tr><td>Up to 35'</td><td>Minimum 1' deep x 1' wide notch (or) Minimum 2' offset of building massing</td></tr><tr><td>36' – 45'</td><td>Minimum 2' deep x 3' wide notch (or) Minimum 2' offset of building massing</td></tr><tr><td>46' – 85'</td><td>Minimum 5' deep x 5' wide notch (or) Minimum 5' offset of building massing</td></tr><tr><td>86' – 145'</td><td>Minimum 10' deep x 10' wide notch (or) Minimum 10' offset of building massing</td></tr></table>	Building Height	Change in Apparent Face	Up to 35'	Minimum 1' deep x 1' wide notch (or) Minimum 2' offset of building massing	36' – 45'	Minimum 2' deep x 3' wide notch (or) Minimum 2' offset of building massing	46' – 85'	Minimum 5' deep x 5' wide notch (or) Minimum 5' offset of building massing	86' – 145'	Minimum 10' deep x 10' wide notch (or) Minimum 10' offset of building massing	<p>Apparent Change in Height Required – 10'-0"deep x10'-0" wide x 10"-0" height differential</p> <p>Apparent Change in Height Provided – 10'-0"deep x15'-0" wide x 10"-0" height differential</p> <p>Refer to attached DRA A12.06, DRA A2.15, DRA A5.01, DRA A5.02</p>	 <p>Figure 03.04.C: Maximum Apparent Face 2 and Apparent Change in Height</p>
Building Height	Change in Apparent Face												
Up to 35'	Minimum 1' deep x 1' wide notch (or) Minimum 2' offset of building massing												
36' – 45'	Minimum 2' deep x 3' wide notch (or) Minimum 2' offset of building massing												
46' – 85'	Minimum 5' deep x 5' wide notch (or) Minimum 5' offset of building massing												
86' – 145'	Minimum 10' deep x 10' wide notch (or) Minimum 10' offset of building massing												
03.04.07 Compound Shape Buildings.	<p>Compound shaped buildings comprised of building wings including, but not limited to, 'L', 'T', 'U' or 'E' shaped plans shall be articulated into a series of smaller, simple discrete volumes in order to reduce their apparent mass. Articulation must include a minimum 6 foot by 6 foot recess at the intersection of two discrete volumes, accompanied by a minimum 5 foot difference in height between the roof of each building wing and the recessed portion of the building.</p>	<p>Compound Shape Building Recess Required – 6'-0" deep x 6'-0" wide x 5'-0" height differential</p> <p>Compound Shape Building Recess Provided – 6'-0" deep x 10'-0" wide x 5'-0" height differential</p> <p>Refer to attached DRA A12.07, DRA A2.15, DRA A5.12</p>	 <p>Figure 03.04.D: Compound Shapes</p>										
03.04.08 Tower Separation	<p>Buildings taller than 105 feet shall maintain a minimum distances of 45 feet clear from any portion of another building taller than the 105 feet.</p>	<p>Tower Separation Provided – 45'-0"</p> <p>Tower Separation Provided – 46'-0"</p> <p>Refer to attached DRA A12.08, DRA A1.01</p>											

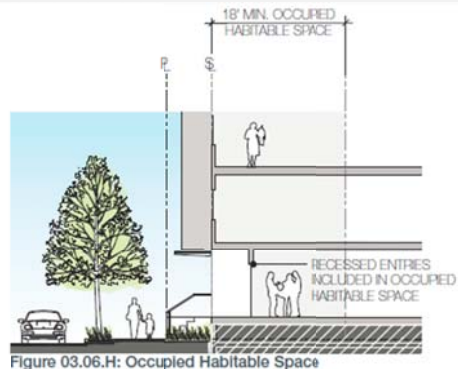
Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -

Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.05.01 - 03.05.02 Setback Plan	<p>Parcels will be developed in accordance with the setbacks illustrated on the Setback Plan (Fig. 03.05.B).</p> <p>The extent of the setback of each building or structure shall be taken as the horizontal distance, measured perpendicularly, from the property line to the predominant building wall closest to such property line, excluding permitted projections.</p>	<p>Arballo Drive Setback Required – 8'-0"</p> <p>Arballo Drive Setback Provided – 8'-0"</p> <p>Gonzales Drive and Serrano Drive Setback Required – 6'-6"</p> <p>Gonzales and Serrano Setback Provided – 6'-6"</p> <p>Refer to attached DRA A12.09, DRA A1.01</p>	
03.05.03 Common v. Private Setback	<p>Building setbacks are divided into common and private setback areas (Fig. 03.05.C).</p> <p>Setback dimensions are as follows:</p> <ul style="list-style-type: none">• 0' Setback / no common setback area• 6'-6" Setback / 1'-6" common setback area• 8' Setback / 2' common setback area• 10' Setback / 3' common setback area• 20' Setback / 10' common setback area	<p>Common setback required at Arballo Drive - 2'-0"</p> <p>Common setback provided at Arballo Drive – 2'-0"</p> <p>Common setback required at Serrano Drive and Gonzales Drive – 1'-6"</p> <p>Common setback provided at Serrano Drive and Gonzales Drive – 2'-0"</p> <p>Refer to attached DRA A2.13, DRA A2.14</p>	<p>Private setback areas are intended for use by adjacent individual residential dwelling units. Common setback areas must be treated as a unified, planted landscape buffer area that is required to be implemented and maintained by the building owner or homeowner's association. Stairs and stoops are excluded from the common area requirement and may extend into the common area as indicated in Figure 03.05.C - Setback Control Sections.</p>
03.05.04 Occupied Building Area	<p>Occupied building area may encroach into the public right-of-way and project into the setback, only above 12 feet from grade, as indicated in Figure 03.05.C - Setback Control Sections.</p> <p>Occupied building encroachments and projections may extend into the public right-of-way and setback, respectively, for a maximum of 55% of the length of the street frontage.</p> <p>Up to 35% of the building face area may encroach into the public right-of-way and/or project into the setback for a maximum of 60 linear feet parallel to the street frontage. The remaining 20% is limited to segments no greater than 12 feet in width.</p> <p>Individual encroachments/projections must have a minimum horizontal separation of 3 feet parallel to the street frontage (Fig. 03.05.A - Occupied Building Area).</p>	<p>Encroaching Occupied Building Area Allowed – 4'-0"</p> <p>Encroaching Occupied Building Area Provided – 0'-0"</p>	 <p>Figure 03.05.A: Occupied Building Area</p>
03.05.05 Active Use Projection	<p>Where active uses occur, building massing is permitted to project into the entire setback at the ground floor as an extension of the adjacent active use.</p>	<p>No Active use building mass provided within building setbacks.</p>	<p>Active uses include, but are not limit to: locally serving retail and services; community rooms and kitchens; and recreational and arts facilities. Lobbies greater than 20 feet in face width are not included as active use. Usable open space must be created on the roof of that projection at the second habitable floor. Commercia Base Requirements - Section 03.08 will apply.</p>
03.05.06 Encroachments + Projections	<p>Awnings, canopies, marquees, signs, shading devices, cornices and lighting may encroach into the public right-of-way and project into the setback above a minimum height of 10 feet from sidewalk grade, as indicated in Figure 03.05.C – Setback Control Sections.</p>	<p>All encroachments and Projections (Canopies and Awnings) located 10'-0" above sidewalk grade height are limited to 2'-0" from the curb line. (4'-6" maximum depth along Serrano Drive and Gonzales Drive, and 6'-0" maximum depth along Arballo Drive)</p>	
03.05.07 Permitted Obstructions	<p>Walls, fences, lighting, elevated private outdoor space, stairs leading to residential entries, guardrails, handrails and other similar building and landscape elements are permitted obstructions within the setback as indicated in Figure 03.05.C – Setback Control Sections.</p>	<p>Building obstructions provided within building setbacks have been limited to Walls, fences, lighting, elevated private outdoor space, stairs leading to residential entries, guardrails, handrails and other similar building and landscape elements listed in Figure 03.05.C</p> <p>Refer to DRA A2.14A , A2.11, A2.12, A2.13, and A2.14</p>	
03.05.08 Basement Levels	<p>Basement Levels Basement levels of buildings are permitted to project into the setback as indicated in Figure 03.05.C – Setback Control Sections; however, projections must be a minimum of 3 feet below grade to allow for a minimum planting depth.</p>	<p>The outer surface of the B1 and B2 Block 06 basement structure is aligned with the 8'-0" setback along Arballo Drive and the 6'-6" setback along Serrano and Gonzales Drive. The basement provided does not encroach in or underneath any building setback.</p> <p>Refer to attached DRA A2.11, DRA A2.12</p>	

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.05.09 Transition	All buildings shall activate the transition zone between private living spaces and public rights-of-ways, easements and semi- private courtyards with private yards, porches, and primary living spaces.	New Construction at Block 06 will comply with all 03.05.09 Transition requirements.	
03.05.10 Planting	Regionally appropriate vegetation must be used for landscaping in transition zones. Regional appropriate planting is drought tolerant, resistant to local pests and is well suited to the specific temperature and humidity of the marine micro-climate at Parkmerced.	New Construction at Block 06 will comply with all 03.05.10 Planting requirements.	
03.05.11 Buffer Planting	The height of plants and trees within common setback areas or shall not exceed 60 inches in height from back of sidewalk grade. Within private setback areas, or other private outdoor spaces, planters containing foliage and trees more than 42 inches in height as measured from the first habitable floor, are limited to 50% of the street frontage in segments no greater than 15 feet in length (Fig. 03.05.D) .	Refer to DRA A2.14A, A5.10 and A5.11 Dimensions added to DRA A2.14A, A5.10 A5.11 and A5.14 confirm planting heights < 60". Note: No discontinuous planting >42" & in excess of 15' length are exhibited within private setbacks.	 Figure 03.05.D: Setback Zone
03.05.12 Common Boundary Structures	Walls, fences and other boundary structures taller than 36 inches are not permitted within the common setback area.	New Construction at Block 06 will comply with all 03.05.12 Common Boundary Structures requirements. Refer to DRA A2.14A, A5.10, A5.11 and A5.14 Clarified on DRA A 5.10	
03.05.13 Private Boundary Structure	Walls, fences and other boundary structures within the private setback area facing a public right-of- way shall not exceed 48 inches from sidewalk or courtyard grade. Along a sloped street frontage, walls, fences and other boundary structures are permitted up to 5 feet in height from back of sidewalk grade for 50% of the associated streetwall, in segments no greater than 15 feet. Guardrails and handrails within the private setback area may exceed 5 feet in height from sidewalk grade, if they are more than 70% physically and visually permeable. Glass panels are not permitted at the ground floor (Fig. 03.05.D) .	4'-0" high planting walls are provided along Serrano, Gonzales and Arballo Drives. 5'-0" high planting wall is provided at Arballo Drive for 15'-0" wide length. All guardrails and handrails provided are more than 70% physically and visually permeable. Glass panels are not provided at the ground floor.	 Figure 03.05.D: Setback Zone

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.06.01 Predominant Building Face	<p>Figure 03.06.D - Streetwall Plan indicates the minimum percentages of building massing that must be constructed to meet the setback line.</p> <p>The minimum percentage of building massing must also be constructed to a minimum height of 35 feet above sidewalk grade as indicated in Fig. 03.06.B.</p> <p>Minor variations along the streetwall (including within Corner Zones) are allowed and count towards the overall streetwall requirements.</p> <p>Minor variations include: covered pass-throughs up to 2 habitable floors in height; recessed building entries less than 2 habitable floors in height; recessed balconies; vertical recesses up to 3 feet deep and 4 feet wide; and minor setbacks from the streetwall no greater than 2 feet from the setback line for any given length to allow architectural articulation of the facade (Fig. 03.06.E) (03.06.04).</p>	Block 06 does not provide for “Predominant Building Face” per 03.06D.	<p>The streetwall is defined as that portion of the building massing, directly fronting onto either a public right-of-way or easement that is constructed to meet the setback line. The streetwall percentage of a project for a given street frontage is calculated by dividing the sum of the length of all building faces built up to the setback line on that block frontage by the total length of the project lot on that block frontage.</p> <p>Pedestrian paseos, as indicated on the Easements + Walks Plan (Fig. 02.01.B), are excluded from streetwall calculations (03.06.02).</p> <div><p>Figure 03.06.B: Streetwall Calculation</p></div>
03.06.03 Corner Zones	<p>A 100% streetwall for a minimum of 30 feet from the corner of the building and a minimum of 35 feet high (Fig. 03.06.C) is required within the Corner Zones illustrated on Figure 03.06.D.</p> <p>Minor variations along the streetwall (including within Corner Zones) are allowed and count towards the overall streetwall requirements.</p> <p>Minor variations include: covered pass-throughs up to 2 habitable floors in height; recessed building entries less than 2 habitable floors in height; recessed balconies; vertical recesses up to 3 feet deep and 4 feet wide; and minor setbacks from the streetwall no greater than 2 feet from the setback line for any given length to allow architectural articulation of the facade (Fig. 03.06.E) (03.06.04).</p>	Block 06 does not provide for “Corner Zones” per 03.06D.	<div><p>Figure 03.06.C: Corner Zone</p></div>

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -

Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.06.05 Building Base Articulation	At a minimum, all buildings must articulate the first habitable floor with a finer grain of architectural detailing to enhance the pedestrian experience. Buildings taller than 50 feet must articulate the first two habitable floors with a finer grain of architectural detailing. This may include, but is not limited to, architectural elements such as canopies, awnings, overhangs, projections, recesses, greater dimensional depth of facade elements, and material and surface change and texture (Fig. 03.06.F) .	Minimum Building Base Articulation required – First 2 habitable floors Minimum Building Base Articulation provided – First 2 habitable floors	
03.06.06 Active Ground Floors	Buildings taller than 65 feet and adjacent to Neighborhood Commons must include active ground floor uses that are visible from and oriented towards the neighborhood commons (Fig. 03.06.G) . Active uses include, but are not limit to: locally serving retail and services; community rooms and kitchens; and recreational and arts facilities. Lobbies greater than 20 feet in face width are not included as active use.	Ground floor courtyards and amenity spaces provided are oriented towards the central neighborhood commons.	
03.06.07 Occupied Habitable Space	All buildings must include 18 feet of occupied habitable space, measured perpendicularly, from the streetwall and paseos and includes the ground floor. Recessed entries may be included in occupied habitable space (Fig 03.06.H) . Garage entries, loading and service entries, transformer rooms, exit stairs and elevators are exempt for 20% of the building perimeter or 60 LF, whichever is less. Buildings that occupy an entire block, except on blocks 04, 08W, 08E, 16SW, 16NW and 18, are exempt for 100 LF. These elements must be incorporated into the overall architectural expression of the building.	All occupied habitable space facing street frontage and paseos provide a minimum of 25'-0" deep habitable space perpendicular to the street and paseo.	 Figure 03.06.H: Occupied Habitable Space
03.07.01 Residential Unit Entries	Each ground floor residential unit must have an individual entry door directly from an adjacent courtyard, dedicated open space, public right-of-way or easement.	All 14 Ground floor units provided have entries from the street and interior corridors.	
03.07.02 Residential Rhythm	Where ground floor residential units face a public right-of-way or easement residential entries must occur at a minimum average of 1 door per 35 linear feet of building frontage.	Maximum Distance between Ground Floor Entries Required - 35'-0" Maximum Distance between Ground Floor Entries Provided - 30'-0" Refer to attached DRA A2.13, DRA A2.14	
03.07.03 Recessed Entries	Residential entries must be sheltered from the rain and wind and provide an entry light. Ground floor residential unit entries must be recessed a minimum of 18 inches from the streetwall.	All 7 Ground floor unit entries are recessed 18" from adjacent window wall Refer to DRA A2.13 and A2.14	
03.07.04 Residential Openness	At least 50% of the ground floor facade of residential buildings shall be devoted to transparent windows and doors to allow maximum visual interaction between sidewalk areas and the interior of residential units. The use of dark or mirrored glass is not permitted.	50% minimum visual transparency provided at Ground floor residential units.	
03.07.05 Floor-to-Floor Heights	Ground floor residential units must have a minimum floor to floor height of 10 feet.	Ground Floor to floor height required - 10'-0" Ground Floor to floor height provided - 13'-7" Refer to DRA A5.01 and A5.02	

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.07.06 Elevated Residential Units	A 24 to 48 inch elevation change must be provided between the first habitable floor of ground floor residential dwelling units and the sidewalk grade in order to provide adequate separation between the interior of residential units and the public realm, while maintaining visual connection. Along a sloped street frontage, elevation change between the first habitable floor of the ground floor residential dwelling unit and the back of sidewalk grade are permitted to be up to 5 feet in height for 50% of the streetwall, in segments no greater than 15 feet.	Internal slopes have been added to the ground floor interior corridors in order to comply with the 24”-48” ground floor unit height requirement. There are now 6 units per ground floor tower, 12 total units. 1 Unit of these 12 units does not comply with the 03.07.06 requirement. Unit 103 within the North Tower is located 6” above the adjacent sidewalk curb height. This Unit represents a 1/12(8.33%) deviation from the requirement.	
03.07.07 Street Lobby Width	Residential lobbies should be limited to no greater than approximately 30 feet wide along the street frontage.	14’-0” wide Residential Lobby provided at Serrano and Gonzales Drive. Refer to attached DRA A2.13, A2.14	
03.09.01 Projected Windows	Enclosed building area which encroaches into the right-of-way or projects into the setback must comprise of at least 55% glazing on a minimum of two separate faces.	No enclosed building area provided encroaches into setbacks or right-of-way.	
03.09.02 Balconies	10% of all units above the first habitable floor must have an open balcony or terrace of a minimum of 36 square feet. Balconies and terraces shall not have a dimension of less than 6 feet in any direction. Buildings must include a minimum of 2 balconies or terraces per floor, located on opposing faces of the building to reduce the apparent building mass from any viewing angle.	Typical Tower Floor – 12 residential units (2) 6’-0”x6’-0” balconies required per floor (2) 6’-0”x9’-0” balconies per floor provided. (1) 6’-0”x14’-0” balconies per floor provided. Refer to attached DRA A2.15	
03.09.03 Glazing	Glazing must be of low reflectance (12% of visible exterior light).	New Construction at Block 06 will comply with all 03.09.03 Glazing requirements.	
03.09.04 Mechanical Equipment	Space for the location of ducts, exhaust pipes and other appurtenances associated with commercial and residential uses must be integrated into the building design. Ducts or exhaust pipes must not be located adjacent to areas designated for courtyards or Neighborhood Commons.	New Construction at Block 06 will comply with all 03.19.04 Mechanical Equipment requirements.	
03.09.05 Solid Waste	All garbage, recycling and composting facilities must be placed fully within the building and shall not be visible from the public right-of-way.	New Construction at Block 06 will comply with all 03.09.05 Solid Waste requirements.	
03.10.01 Screening	Mechanical equipment located on top of buildings must be screened from public view and from neighboring buildings with enclosures, parapets, setbacks, landscaping, or other means. Any enclosure or screening used must be designed as a logical extension of the building, using similar materials and detailing as the rest of the building’s surfaces.	All mechanical roof equipment provided is located behind 10’-0” high mechanical screen wall plane extensions.	
03.10.02 Solar Panels	50% of roof area must be designed to permit installation of south oriented solar panels.	Roof Area = 11,759 SF per tower Potential Solar Panel Area Required = 5,880 SF per tower Potential Solar Panel Area Provided = 6,004 SF per tower Refer to: DRA A2.17	

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.12.04 Restrictions	<p>No sign, except as provided in Planning Code Section 603 or 604, shall be permitted in the Parkmerced Special Use District without a permit being duly issued therefor.</p> <p>No general advertising signs are permitted. Roof signs, wind signs, and signs on canopies are not permitted. No sign shall have or consist of any moving, rotating, or otherwise physically animated part, or lights that give the appearance of animation by flashing, blinking, or fluctuating, except those moving or rotating or otherwise physically animated parts used for rotation of barber poles and the indication of time of day and temperature. Back-lit box signs, defined as signs with an internal light source and one or more translucent faces illuminated for visibility onto which opaque letters are affixed are not permitted. Where possible, exposed junction boxes, lamps, tubing, conduits, or raceways are discouraged.</p>	New Construction at Block 06 will comply with all 03.12.04 Sign Restriction requirements.	
03.12.05 Height	Except as provided by section 03.12 of the Parkmerced Design Standards and Guidelines, no sign shall exceed a height of 24 feet.	New Construction at Block 06 will comply with all 03.12.05 Sign Height requirements.	
03.12.06 Business Signs	<p>Business signs are permitted for business establishments within the Mixed Use-Social Heart (PM-MU1) or the Neighborhood Commons (PM-MU2) districts, as follows:</p> <p>(a) Wall Signs. One wall sign shall be permitted for each Business Frontage. The area of each wall sign shall not exceed 3 square feet per foot of each Business Frontage, or 45 square feet, whichever is less. However, for general grocery store uses, the area of each wall sign shall not exceed 3 square feet per foot of each Business Frontage, or 150 square feet, whichever is less.</p> <p>(b) Projected Signs. One projecting sign shall be permitted for each 30 feet, or fraction thereof, of Business Frontage. The area of the first such projecting sign shall not exceed 24 square feet and the area of any subsequent sign shall not exceed 10 square feet. In lieu of the 24 square foot projecting sign, a business may be allowed a single three-dimensional projecting sign of not more than 48 cubic feet in volume.</p> <p>(c) Awnings. Sign copy on an awning shall be permitted in lieu of each permitted projecting sign. The area of such sign copy shall not exceed 30 square feet.</p> <p>(d) Window Signs. The total area of all window signs shall not exceed 1/3 the area of the window on or in which the signs are located. Such signs may be non-illuminated, indirectly illuminated, or directly illuminated.</p>	New Construction at Block 06 does not provide for any Business establishments.	

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
03.12.07 Neighborhood Signs	<p>Neighborhood signs are defined as Identifying Signs and/or non-temporary Sale or Lease Signs. Neighborhood Signs are permitted as follows:</p> <p>(a) Wall Signs. One wall sign shall be permitted for each building containing at least one residential unit, and for each building containing a use for which the primary purpose is to administer the marketing, maintenance, and/or management of the rental units within the Parkmerced Special Use District. The area of each wall sign shall not exceed 50 square feet. No wall sign shall exceed a height of 24 feet, and any sign exceeding 18 square feet in area shall be set back at least 25 feet from all street property lines. Such signs may be nonilluminated, indirectly, or directly illuminated. No wall sign shall be permitted along any interior lot line.</p> <p>Notwithstanding the foregoing, two additional wall signs shall be permitted up to 100 feet in height and up to 450 square feet in area provided that no portion of the sign is publicly visible for more than one-hundred eighty (180) days per calendar year. For the purposes of this paragraph, any period of any day shall be counted as a full day. Any application for a wall sign permitted pursuant to this paragraph must be accompanied by a schedule of days on which the sign will be publicly visible. The owner of the property on which such sign is located shall sign and have notarized any such schedule and shall notify the Planning Department promptly upon any change to this schedule.</p> <p>(b) Freestanding Signs. (1) Up to ten (10) signs shall have a maximum area of 150 square feet each and be limited to 12 feet in height; (2) Up to fifteen (15) signs shall have a maximum area of 75 square feet each and be limited to 24 feet in height.</p>	New Construction at Block 06 will comply with all 03.12.07 Neighborhood Sign requirements.	
03.13.01 Energy Efficiency	Designs shall use energy efficient bulbs and fixtures.	New Construction at Block 06 will provide energy efficient bulbs and fixtures.	
03.13.02 Luminaires	Traditional “glowtop” luminaires shall not be used, as they are a significant source of light pollution. Instead, luminaires which direct light downward and towards the intended use are to be employed.	New Construction at Block 06 will comply with all 03.13.02 Luminaires requirements.	
03.13.03 Light Pollution	All lighting must be shielded to prevent glare to private and public uses, especially residential units. The angle of maximum candela from each interior luminaire as located in the building shall intersect opaque building interior surfaces and not exit out through the windows.	New Construction at Block 06 will comply with all 03.13.03 Light Pollution requirements.	

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -																							
Standard Number	Standard			Block 06 Compliance	Implementing Standards																		
04.01.01 Bicycle Parking	<table><tr><th>Land Use</th><th>Minimum Parking Rates</th><th>Estimated Supply</th></tr><tr><td>Residential</td><td>1 / 2 Units</td><td>4,450</td></tr><tr><td>Grocery</td><td>1 / 2,000 gsf</td><td>21</td></tr><tr><td>Retail/Office/ Professional Services</td><td>0 – 10,000 gsf = 2 10,001 – 20,000 gsf = 4 20,001 – 40,000 gsf = 6 > 40,000 = 12</td><td>66</td></tr><tr><td>School</td><td>1 / 4,000 gsf</td><td>7</td></tr><tr><td>Fitness/Community Center</td><td>1 / 4,000 gsf</td><td>14</td></tr></table>			Land Use	Minimum Parking Rates	Estimated Supply	Residential	1 / 2 Units	4,450	Grocery	1 / 2,000 gsf	21	Retail/Office/ Professional Services	0 – 10,000 gsf = 2 10,001 – 20,000 gsf = 4 20,001 – 40,000 gsf = 6 > 40,000 = 12	66	School	1 / 4,000 gsf	7	Fitness/Community Center	1 / 4,000 gsf	14	<p>124 Units Per Tower 106 Class I Spaces per Tower Required 7 Class II Spaces per Tower Required</p> <p>Total 212 Class I Spaces Required Total 14 Class II Spaces Required</p> <p>250 Class I Bicycle Parking spaces Provided at Basement 1 Level 14 Class II Bicycle Parking spaces Provided at Basement 1 Level</p> <p>Refer to: DRA A2.12</p>	<p>See also <i>4.1.6 Provide carshare and bikeshare programs</i> (Parkmerced Transportation Plan):</p> <p>Similar to carsharing, bikesharing (also referred to as “bicycle libraries”) is a program that allows users to rent a bicycle for a given period of time. Bicycles are “checked out” at one station and returned at any other station within the system. Members pay based on the length of time they use the bicycle, thus reducing the costs associated with personal bicycle ownership. Typically, bikeshare members are able to identify the location of the nearest bicycle by phone or online.</p> <p>With stations located all over Parkmerced, these bicycles are meant to be used for short time periods only, and checked in and checked out at the start and end of each trip. Bikeshare programs are currently being implemented in the Bay Area and in other urban areas throughout the country, in Canada and in Europe, and have been gaining popularity in providing non-bicycle owners the opportunity to use bicycling for work, shopping or recreation trips.</p> <p>Parkmerced will work to attract a bikeshare company to install and operate bikeshare stations throughout Parkmerced. (Although Parkmerced may contract with an independent operator, efforts will be made to coordinate with City-sponsored bikeshare operators or programs, if any.) It is anticipated that these will be a series of small facilities (accommodating up to five bicycles at most locations), with larger stations (accommodating up to 10 bicycles) provided at the transit stations and the retail center. Figure 14 identifies the proposed locations of the 14 bikeshare centers, however alternate locations may be used if deemed appropriate by Parkmerced and the bike-share operator.</p> <p>The bikeshare operator will determine the appropriate number and distribution of bicycles to be located at each location. Typically, bikeshare stations are modular, and can be expanded to provide additional bicycle parking spaces. In addition, the bikeshare operator will be responsible for redistributing the bicycles throughout Parkmerced on a daily basis, or as needed based on parking locations.</p> <p>Proposed bikeshare measures shall include the following:</p> <ul style="list-style-type: none">- The TC will encourage the bikeshare operator to offer:<ul style="list-style-type: none">- Reduced membership fees or incentives for residents and employees; and- Separate fees for residents and employees at Parkmerced versus visitors;- Where feasible, the TC shall establish a long-term contract with the bicycle operator in order to ensure continuity of service and minimize costs to bikeshare users;- The availability of bike sharing and information on the various bikeshare operators will be included in all rental and leasing information and in real-time on the Parkmerced website (to the extent such information is available on the bikeshare operators’ websites);- Bikeshare center locations will be clearly identified by directional signage; and,- At full buildout of Parkmerced, a guaranteed minimum number of bicycles and bikeshare spaces will be provided (80 bicycles), with more to be added as warranted by demand as determined by the bikeshare operator.
	Land Use	Minimum Parking Rates	Estimated Supply																				
	Residential	1 / 2 Units	4,450																				
	Grocery	1 / 2,000 gsf	21																				
	Retail/Office/ Professional Services	0 – 10,000 gsf = 2 10,001 – 20,000 gsf = 4 20,001 – 40,000 gsf = 6 > 40,000 = 12	66																				
	School	1 / 4,000 gsf	7																				
	Fitness/Community Center	1 / 4,000 gsf	14																				
	Off-street bicycle parking must be provided for new buildings in the minimum quantities listed in Table 3 – Minimum Bicycle Parking , or quantities listed in the San Francisco Planning Code, whichever is greater. Residential, retail, office, institutional and educational uses must provide Class I bicycle parking for residents and employees. All other commercial uses and all visitor bicycle parking may be provided as Class II bicycle parking.																						
	Per Section 155.2.11 of the San Francisco Planning Code																						
	For buildings containing more than 100 dwelling units, 100 Class 1 spaces plus one Class 1 space for every four dwelling units over 100. 1 class II Bicycle space per ever 20 Units																						

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -																											
Standard Number	Standard	Block 06 Compliance	Implementing Standards																								
04.01.02 Support biking	<p>The number of shower and changing facilities must meet the sum of the requirements listed in Table 3 - Minimum Bicycle Parking. Shower and changing facilities in buildings within 600 feet of retail or commercial building entrances can be used to fulfill this requirement.</p> <table><tr><th>Land Use</th><th>Shower Facility</th></tr><tr><td>Residential</td><td>NA</td></tr><tr><td>Grocery</td><td>1 / 30,000 sf</td></tr><tr><td>Retail/Office/ Professional Services</td><td>1 / 30,000 sf</td></tr><tr><td>School</td><td>1 / 30,000 sf</td></tr><tr><td>Fitness/ Community Center</td><td>1 / 30,000 sf</td></tr></table>	Land Use	Shower Facility	Residential	NA	Grocery	1 / 30,000 sf	Retail/Office/ Professional Services	1 / 30,000 sf	School	1 / 30,000 sf	Fitness/ Community Center	1 / 30,000 sf	<p>Block 06 does not propose retail or community uses.</p>	<p>See also <i>4.1.7 Improve bicycle facilities</i> (Parkmerced Transportation Plan):</p> <p>To encourage the use of the bicycle as an everyday means of transportation, off-street bike parking will be incorporated in the renovation of existing buildings and included into new construction. Bicycle parking areas will be located on the ground floors of buildings, close to activity to provide convenience and increase security.</p> <p>The required off-street bicycle parking supply for the various new land uses proposed within Parkmerced is presented in Table 4, which meet or exceed the requirements listed in Section 155 of the San Francisco Planning Code and is consistent with the policy modifications proposed as part of the San Francisco Bicycle Plan. In the event that the City at a later date adopts bicycle parking requirements that require a greater number or different type of bicycle parking spaces than shown in the table below, those later requirements shall apply to all new construction at Parkmerced. It should be noted that for the retail and office uses, the amount of bicycle parking spaces to be provided will be based on the total square footage of the individual building, and not based on the size of individual tenants. Also, all existing residential units that will be retained currently provide bicycle parking; as such, no additional facilities for the retained residential buildings are required as part of this Plan.</p> <p>A combination of Class I and Class II spaces should be provided to meet this bicycle parking supply requirements. Class I bicycle parking facilities provide secure long-term bicycle storage by protecting the entire bicycle, including its components and accessories, against theft and inclement weather. Examples include lockers, check-in facilities, monitored bicycle parking, restricted access bicycle parking and personal storage. Class II bicycle parking facilities provide short-term bicycle parking and include bicycle racks that permit the locking of a bicycle frame and one wheel and support the bicycle in a stable position without damage to wheels, frame or components.</p> <p>Class I bicycle parking is required be provided at residential buildings, and a combination of Class I and Class II parking is required to be provided at retail and professional services uses, at the school and at the fitness/community center.</p> <p>Off-street bicycle parking will be augmented by on-street parking provided by racks and posts throughout Parkmerced.</p> <table><tr><th>Land Use</th><th>Minimum Bicycle Parking Rates</th></tr><tr><td>Residential</td><td>1 space per 2 units</td></tr><tr><td>Grocery</td><td>1 space per 2,000 gsf</td></tr><tr><td>Retail/Office/ Professional Services</td><td>0-10,000 gsf = 2 spaces 10,001-20,000 gsf = 4 spaces 20,001-40,000 gsf = 6 spaces > 40,000 gsf = 12 spaces</td></tr><tr><td>School</td><td>1 space per 4,000 gsf</td></tr><tr><td>Fitness/ Community Center</td><td>1 space per 4,000 gsf</td></tr></table>	Land Use	Minimum Bicycle Parking Rates	Residential	1 space per 2 units	Grocery	1 space per 2,000 gsf	Retail/Office/ Professional Services	0-10,000 gsf = 2 spaces 10,001-20,000 gsf = 4 spaces 20,001-40,000 gsf = 6 spaces > 40,000 gsf = 12 spaces	School	1 space per 4,000 gsf	Fitness/ Community Center	1 space per 4,000 gsf
Land Use	Shower Facility																										
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Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -														
Standard Number	Standard	Block 06 Compliance	Implementing Standards											
04.01.03 Car-Share	<div>Provide car-share vehicle parking in the amount listed in Table 4 - Minimum Car Share Parking.</div> <table><tr><th>Land Use</th><th>Minimum Car-Share Spaces</th></tr><tr><td rowspan="3">Residential</td><td>0 – 49 du = 0 car-share spaces</td></tr><tr><td>50 – 200 du = 1 car-share space</td></tr><tr><td>> 201 or more du = 2 car share spaces, plus 1 car share space for every 200 du over 200 du</td></tr><tr><td rowspan="2">Non-Residential</td><td>0 – 24 parking spaces = 0 car share spaces</td></tr><tr><td>25 – 49 parking spaces = 1 car share space</td></tr><tr><td></td><td>> 49 parking spaces = 1 car share space, plus 1 car share space for every 50 parking spaces over 50 parking spaces</td></tr></table>	Land Use	Minimum Car-Share Spaces	Residential	0 – 49 du = 0 car-share spaces	50 – 200 du = 1 car-share space	> 201 or more du = 2 car share spaces, plus 1 car share space for every 200 du over 200 du	Non-Residential	0 – 24 parking spaces = 0 car share spaces	25 – 49 parking spaces = 1 car share space		> 49 parking spaces = 1 car share space, plus 1 car share space for every 50 parking spaces over 50 parking spaces	Responses to this comment are provided in the Parkmerced Owner’s Responses to the Master Planning Comments	<p>Signage indicating such parking spaces must be provided, and the parking spaces must be within 200 feet of entrances to the buildings served. Car-share vehicles must be located at unstaffed, self-service locations (other than any incidental garage valet service), and generally be available for pickup by members 24 hours per day. Car-share parking spaces must be dedicated for current or future use by a certified car-share organization through a deed restriction, condition of approval or license agreement. Such deed restriction, condition of approval or license agreement must grant priority use to any certified car-share organization that can make use of the space, although such spaces may be occupied by other vehicles so long as no certified car-share organization can make use of the dedicated car-share spaces. Any off-street car-share parking space provided under this Section must be provided as an independently accessible parking space. In new parking facilities that do not provide any independently accessible spaces other than those spaces required for disabled parking, off-street car-share parking may be provided on vehicle lifts so long as the parking space is easily accessible on a self-service basis 24 hours per day to members of the certified car-share organization. Property owners may enact reasonable security measures to ensure such 24-hour access does not jeopardize the safety and security of the larger parking facility where the car-share parking space is located so long as such security measures do not prevent practical and ready access to the off-street car-share parking spaces.</p> <p>See also <i>4.1.6 Provide carshare and bikeshare programs</i> (Parkmerced Transportation Plan):</p> <p>Carsharing provides an effective incentive for participants to forego car ownership and rely on transit as a primary mode of travel because they know that a car is readily available when they need one. The growth and success of these programs in the Bay Area and in other urban areas throughout the country has shown their effectiveness in reducing auto dependency. Members pay based on how much they drive, thus reducing the fixed costs associated with private automobile ownership. Typically, carshare members are able to reserve a car by phone or online on an as-needed basis, and pick-up and drop-off the vehicle at each established carshare hub.</p> <p>The TC will work with local carsharing organizations to establish a network of carshare vehicles parked in hubs located throughout Parkmerced. The carshare operators will determine the appropriate number and distribution of cars to be located at each location. In general, the carshare facilities have limited physical infrastructure and therefore can be modified as needed to meet changes in future demand. It is anticipated that these hubs will be centralized at gathering areas, and therefore will serve multiple buildings and uses (accommodating between 5 and 15 vehicles at each location). Figure 15 identifies the proposed locations of the ten carshare hubs.</p> <p>Section 166 of the San Francisco Planning Code (as presented in Table 3) lists the requirements for the provision of carshare parking spaces based on the number of residential units (for residential uses) and the number of off-street automobile parking spaces (for commercial uses), which Parkmerced is committed to meeting at each phase of development. In addition, additional carshare spaces will be provided if warranted by demand (as determined by the TC). In addition, in the event that the City at a later date adopts car sharing requirements that require a greater number of carshare spaces than shown in the table below, that later requirement shall apply to all new construction at Parkmerced.</p>
Land Use	Minimum Car-Share Spaces													
Residential	0 – 49 du = 0 car-share spaces													
	50 – 200 du = 1 car-share space													
	> 201 or more du = 2 car share spaces, plus 1 car share space for every 200 du over 200 du													
Non-Residential	0 – 24 parking spaces = 0 car share spaces													
	25 – 49 parking spaces = 1 car share space													
	> 49 parking spaces = 1 car share space, plus 1 car share space for every 50 parking spaces over 50 parking spaces													

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -																	
Standard Number	Standard	Block 06 Compliance	Implementing Standards														
04.01.03 Car-Share (Continued)			<p>Proposed carshare measures shall include the following:</p> <ul style="list-style-type: none">- The TC will encourage carshare providers to offer reduced membership fees or incentives for residents and employees;- Long-term contracts with carshare operators will be established to ensure continuity and reduce costs;- The TC will encourage carshare providers to offer reduced fees for long-term carshare use. This would reduce the need for private vehicle ownership for vacations or weekend trips;- The availability of carsharing and information on the various carshare operators will be included in all rental and leasing information and in real-time on the Parkmerced website (to the extent such information is provided on the carshare operators’ websites); and- Carshare hub locations will be clearly identified by directional signage. <table><tr><th>Land Use</th><th>Required Carshare Spaces</th></tr><tr><td>Residential</td><td>0-49 units = 0 carshare spaces 50-200 units = 1 carshare space 201 or more units = 2 carshare spaces, plus 1 carshare space for every 200 units over 200 units</td></tr><tr><td>Non-Residential</td><td>0-24 parking spaces = 0 carshare spaces 25-49 parking spaces = 1 carshare space 50 or more parking spaces = 1 carshare space, plus 1 carshare space for every 50 parkings spaces over 50 parking spaces</td></tr></table>	Land Use	Required Carshare Spaces	Residential	0-49 units = 0 carshare spaces 50-200 units = 1 carshare space 201 or more units = 2 carshare spaces, plus 1 carshare space for every 200 units over 200 units	Non-Residential	0-24 parking spaces = 0 carshare spaces 25-49 parking spaces = 1 carshare space 50 or more parking spaces = 1 carshare space, plus 1 carshare space for every 50 parkings spaces over 50 parking spaces								
Land Use	Required Carshare Spaces																
Residential	0-49 units = 0 carshare spaces 50-200 units = 1 carshare space 201 or more units = 2 carshare spaces, plus 1 carshare space for every 200 units over 200 units																
Non-Residential	0-24 parking spaces = 0 carshare spaces 25-49 parking spaces = 1 carshare space 50 or more parking spaces = 1 carshare space, plus 1 carshare space for every 50 parkings spaces over 50 parking spaces																
04.02.01 Parking Location	<p>Off-street parking may be located only where indicated on the Parking Plan (Fig. 04.02.A). All off-street parking shall be below grade except where permitted to be above grade as indicated in the Parking Plan (Fig. 04.02.A). The number of new parking spaces in the each specific parking zone shall not exceed the maximums indicated in Table 5 - Parking Zones. Parking zones are defined as the following:</p> <p>Zone 1: Below grade only Zone 1a: Above grade permitted to the allowance of spaces listed in Table 5, plus below grade parking where number of spaces within both Zone 1 and Zone 1a does not exceed the number of spaces listed for Zone 1 Zone 2: Below grade only Zone 2 - Overlay: Above grade parking only</p> <table><tr><th>Zone</th><th>Maximum Parking Spaces</th></tr><tr><td>Zone 1</td><td>2,349 spaces</td></tr><tr><td>Zone 1a</td><td>201 spaces</td></tr><tr><td>Zone 2</td><td>5,766 spaces</td></tr><tr><td>Zone 2 – Overlay</td><td>25 spaces</td></tr><tr><td>Existing Parking</td><td>1,109 spaces</td></tr><tr><td>Total Parking</td><td>9,450 spaces</td></tr></table>	Zone	Maximum Parking Spaces	Zone 1	2,349 spaces	Zone 1a	201 spaces	Zone 2	5,766 spaces	Zone 2 – Overlay	25 spaces	Existing Parking	1,109 spaces	Total Parking	9,450 spaces	<p>Zone 2 – 5,766 Maximum Parking Spaces</p> <p>Block 06 - 452 Below Grade Parking spaces provided</p> <p>“Total number of units at completion of Phase 1A is estimated to be 3,613 units. Block 6 is providing 452 new parking spaces bringing the total parking count to 3,791. This yields a surplus of 178 spaces. The 178 spaces in excess of the 1:1 parking ratio during Subphase 1A will be cordoned off and brought on-line as new units are constructed during subsequent sub-phases”</p>	
Zone	Maximum Parking Spaces																
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Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -													
Standard Number	Standard	Block 06 Compliance	Implementing Standards										
04.02.02 Off-Street Parking	<div>Off-street parking shall not be required for any use. The number of off-street parking spaces shall not exceed the maximums listed in Table 6 - Off-Street Parking.</div> <table><tr><th>Zone</th><th>Maximum Parking Spaces</th></tr><tr><td>Residential</td><td>1 / du</td></tr><tr><td>Grocery Store</td><td>1 / 500 sf</td></tr><tr><td>Commercial/Retail</td><td>1 / 750 sf</td></tr><tr><td>Community/Fitness/School</td><td>1 / 1000 sf</td></tr></table>	Zone	Maximum Parking Spaces	Residential	1 / du	Grocery Store	1 / 500 sf	Commercial/Retail	1 / 750 sf	Community/Fitness/School	1 / 1000 sf	<div>434 Standard Stalls Provided 3 Car Share Stalls Provided 2 Van Spaces Provided 9 Handicap Stalls Provided</div> <div>448 Total Off-Street Below Grade Parking Stalls Provided</div>	<div>See also 4.1.8 Establish automobile parking program (Parkmerced Transportation Plan):</div> <div>The parking program is designed to control the overall usage of private automobiles through pricing, limitations to supply, new technology, and effective monitoring efforts. The following sections outline some of the key elements of the parking program.</div> <div>Off-street residential automobile parking strategies Residential parking will be based on a “parking storage” concept: many residents will not use their cars every day, and thus a resident’s parking space will not necessarily be adjacent to his or her unit. This approach will help reduce the amount of “convenience driving” and encourage residents to walk, bike or take the shuttle to access local destinations. Overall, less parking will be provided in the eastern half of the site, which will have enhanced transit service and high levels of walk/bike accessibility. In addition, this will help divert traffic away from Highway 1, and raise the competitiveness of walking, biking and transit in this high density residential area relative to driving. Residents choosing to live on the east side of Parkmerced can take advantage of easy proximity to the mixed-use center and its concentration of transit service, and therefore will not need to drive as often. As shown in Figure 17, this goal is accomplished by generally providing two levels of basement parking under the western blocks and one level under the eastern blocks of Parkmerced.</div> <div>The specific residential parking strategies shall include:</div> <div><div>- Residential parking will be unbundled from the units (e.g., each unit will not be sold or leased with a parking space);</div><div>- Each parking space will be sold or leased separately to individual units;</div><div>- Residential parking rates will be set to fair market value (to be updated annually, based on surveys conducted by the TC); and</div><div>- At full build-out of Parkmerced, parking will be provided at overall maximum rate of one space per residential unit.</div></div> <div>Off-street commercial automobile parking strategies Off-street commercial parking will be provided within the retail center area to support the proposed new retail, restaurant, office, and business services spaces. The off-street facilities will be unbundled and designed to promote shared parking uses. In other words, parking spaces would not be designated for certain uses or businesses so that all commercial spaces may be used by any commercial patron, provided, however, that approximately half of the grocery store-permitted spaces will be reserved for grocery store use only during normal grocery store business hours. All commercial spaces will be paid spaces, with rates that discourage long-term use.</div>
Zone	Maximum Parking Spaces												
Residential	1 / du												
Grocery Store	1 / 500 sf												
Commercial/Retail	1 / 750 sf												
Community/Fitness/School	1 / 1000 sf												

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
04.02.02 Off-Street Parking (Continued)			<p>The specific commercial parking strategies shall include:</p> <ul style="list-style-type: none">- All parking will be unbundled and designed to serve all commercial uses, with the exception of the spaces designed for exclusive use of the proposed grocery store (during store hours of operation only);- Where shared parking opportunities exist (e.g., where parking supports service uses during the day and a restaurant during the evening), parking requirements will be reduced;- All off-street parking will be paid parking, and will be charged at hourly rates;- Parking rates will be set equivalent to fair market value (to be updated annually, based on surveys conducted by the TC) and will not be subsidized by tenants or building operators; and- Discounts will not be allowed for “early bird” or “in-by/out-by” long-term parking, and discounted monthly parking passes will not be offered. <p>Automobile parking supply Separate off-street parking supplies shall be provided for the residential and commercial uses. Figure 17 illustrates the proposed off-street parking locations throughout Parkmerced.</p> <p>The allowable maximum off-street parking supply for the various land uses proposed within Parkmerced is presented in Table 5. It should be noted that for the retail and office uses, the amount of parking spaces to be provided will be based on the total square footage of the building, and not based on the size of individual tenants.</p> <p>As shown in Table 5, a total of one off-street parking space will be permitted for each residential unit. As noted earlier, all residential parking will be unbundled from the units, so that residents have the option to lease no spaces or multiple spaces, depending on their needs. In addition, off-street parking spaces will be permitted for the non-residential uses. All parking for the retail/office uses in Parkmerced's mixed-use center will be housed in adjacent structures and basement parking levels. No off-street parking will be permitted for the smaller neighborhood-serving retail hubs that would be distributed throughout the area. In addition, off-street parking will be permitted at the school and at the fitness/community center.</p> <p>Due to the phased nature of construction within Parkmerced, the concentration of parking in certain areas of the neighborhood, and the fact that each garage will serve multiple buildings, the ratio of constructed parking spaces to uses may exceed these maximums set forth in Table 3 temporarily.</p> <p>Electric vehicle parking To promote the use of electric passenger vehicles, a minimum of 1 percent of off-street residential parking spaces will be constructed with electric wiring conduits to permit wiring and hook-up of an electric vehicle charger.</p>

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
04.02.03 Parking Spaces	Parking spaces may be either independently accessible or space-efficient, except as required elsewhere in the Building Code for spaces specifically designated for persons with physical disabilities. Space efficient parking is parking in which vehicles are stored and accessed by valet, mechanical stackers or lifts, certain tandem spaces, or other space-efficient means. Off-street parking spaces may be located either on the same development block as the building served, or off-site within the Development Plan Area.	All parking provided is independently accessible per 04.02.03.	
04.02.04 Unbundled Parking	All off-street parking spaces for residential uses shall be leased or sold separately from and in addition to the rental or purchase fees for dwelling units for the life of the dwelling units. A minimum of one (1) separate, dedicated pedestrian entrance, visible and accessible from a public right-of-way or easement, shall be provided for the users of each individual off-street parking facility (Fig.04.02.A) .	Responses to this comment are provided in the Parkmerced Owner's Responses to the Master Planning Comments	
04.02.05 Parking Entrances	Vehicular entrances and exits to parking facilities shall have a maximum linear width of 11 feet parallel to the street if accommodating one direction of travel, and maximum linear width of 22 feet parallel to the street if accommodating both an exit and entrance at one opening. Entrances and/or exits that are shared with loading and service access may be 12 feet wide when accommodating one-way traffic and 24 feet wide when accommodating two-way traffic.	22'-0" wide garage entry and exit provided on Serrano and Gonzales Drives. Service access to Basement Levels is provided at these above locations. Refer to attached DRA A2.13, DRA A2.14	
04.02.06 Above Grade Parking	Above grade parking structures must be lined with a minimum of 18 feet of occupied habitable space facing public rights-of-way, dedicated open spaces, semiprivate open spaces, and easements, excluding the MUNI Easement. All other frontages must visually screen the interior from the exterior under daylighting and night lighting conditions.	No above parking structure provided at Block 06.	
04.02.07 Exposed Parking Decks	Parking decks that are exposed and open to the sky shall use paving materials with a solar reflectance index of at least 29 and one of the following strategies for 50% of the exposed parking deck. <ul style="list-style-type: none">• Provide shade from open structures, such as those supporting solar photovoltaic panels, canopied walkways, and vine pergolas, all with a solar reflectance index of at least 29.• Provide shade from tree canopy (within ten years of landscape installation).	No above parking structure provided at Block 06.	
04.02.08 Light Trespass	Parapet edges of the parking trays, including the roof, must be higher than vehicle headlights in order to screen adjacent properties. All lighting for parking areas must have a low cut-off angle in order to prevent light from casting beyond the parking area boundary.	No above parking structure provided at Block 06.	

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
04.03.01 Loading	Preferred on-street loading spaces and permitted routes related to specific loading vehicles are indicated on the Truck Routes and Loading Plan (Fig. 04.03.B). All streets have been designed for SU-30 vehicles.	2 street loading space required on Arballo Drive per 04.03.0B. 2 street loading space provided on Arballo Drive. Refer to DRA A1.01	<p>See also <i>4.1.9 Establish loading program</i> (Parkmerced Transportation Plan):</p> <p>The loading program is designed to facilitate access required by freight vehicles (commercial delivery and moving trucks), service vehicles (regular sized commercial passenger cars, trucks and vans for service calls and deliveries) and passenger vehicles (private vehicles, vans and shuttles), while reducing the negative impacts that loading and unloading activities might have on pedestrians and cyclists. The following sections outline the key elements of the loading plan, as shown in Figure 18.</p> <p>Off-street loading To provide access from the street, off-street loading spaces require curb cuts and driveways, which can be intrusive to the bicycling and pedestrian environment. In addition, the turning movements of vehicles leaving or entering the street can impede the flow of traffic, which is of particular concern with regard to transit vehicles. The following guidelines will apply to the location and design of any off-street loading spaces provided within Parkmerced:</p> <ul style="list-style-type: none">- A maximum of one curb cut is permitted every 250 linear feet of street;- Individual buildings are limited to one opening of up to 10 feet in width to provide access to off-street loading;- Shared openings for parking and loading are encouraged when both are provided along the same building frontage, with a maximum width of 24 feet;- Where possible, curb cuts and driveways providing access to off-street loading spaces will be consolidated into a single location on any block face;- No curb cuts accessing off-street loading are permitted on streets with light rail operations or on the local streets with bike lanes, where alternative frontages are available;- Off-street driveways to accommodate passenger loading or unloading (porte-cochères) are permitted only at high-density residential towers and may remain where currently existing;- Individual buildings would be limited to one opening of up to 12 feet in width to provide access to off-street loading. Shared openings for parking and loading would be encouraged, with a maxi-mum width of 24 feet; and- Loading spaces shall be designed to serve all commercial land uses. Where opportunities to share loading spaces exist (e.g., loading area for the grocery store with a peak of morning deliveries and restaurants with afternoon deliveries), the off-street loading requirements will be reduced accordingly. <p>The required on-street and off-street loading supply for the various land uses proposed within Parkmerced is presented in Table 6. It should be noted that for the retail and office uses, the amount of loading spaces to be provided will be based on the total square footage of the building, and not based on the size of individual tenants.</p>

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -			
Standard Number	Standard	Block 06 Compliance	Implementing Standards
04.03.01 Loading (Continued)			<p>In general, the residential buildings are not required to provide off-street loading spaces. However, to accommodate short-term loading requirements (such as for service calls), service vehicles spaces shall be included in the residential garages. As service vehicles are sized equivalent to standard passenger cars, trucks and vans, spaces may be provided where garages have a minimum ceiling height clearance of at least eight feet, two inches. These spaces should be located on the first level of the garage, with convenient access to a residential elevator. Two on-street loading spaces shall be provided per block, as shown in Figure 18. These spaces could be used by pick-ups/drop-offs, or by delivery/service vehicles that would not fit within the residential garages.</p> <p>Move-ins and move-outs may be accommodated either through the service vehicles spaces within the residential garages, or through the on-street loading spaces. If moving vehicles cannot fit in either location (for instance, a semi-tractor trailer), special arrangements with the Parkmerced management team shall be required. Residents needing accommodations for longer moving vehicles will be required to contact the management team the Friday prior to the move-in/move-out day. Each Monday, the management team will then coordinate with the appropriate agencies (SFMTA and the San Francisco Police Department) to temporarily reserve a section of on-street parking spaces for move-in/move-out use.</p> <p>For the planned grocery store, at least two off-street freight loading spaces are required to accommodate the anticipated demand for the daily delivery of produce and goods. Also, two on-street loading spaces are required to accommodate pick-ups/drop-offs and taxis. In addition, the individual grocery store operator shall be required to develop a loading program to minimize disruptions to local streets and to limit the number of trucks during peak commute times.</p> <p>No off-street loading supply is required for the proposed office and business service land use. In general, these uses have a minimal demand for loading, usually limited to short-term deliveries or service calls. As such, all loading will be accommodated through on-street loading spaces which must be located at either the front or rear of the building.</p>

Block 6 Parkmerced DS&G — Design Review Compliance Checklist for Buildings – WB Revision 06.01.15 – R1. WB Revision 07.08.15- R2.WB -WB Revision 07.21.15- R3.WB - WB Revision 07.24.15- R4.WB -																	
Standard Number	Standard	Block 06 Compliance	Implementing Standards														
04.03.02 Loading Spaces	<p>The maximum number of loading spaces by use is listed in Table 7 - Required Loading Spaces. Residential loading spaces are provided on-street and are specifically identified on the Truck Routes and Loading Plan (Fig. 04.03.B).</p> <ul style="list-style-type: none">• On-street loading spaces may be used as regular vehicular parking spaces and scheduled for loading.• On-street loading spaces must be sized to accommodate vehicles up to those identified for each specific street on the Truck Routes and Loading Plan (Fig. 04.03.B).	<p>Block 06 Compliance</p> <p>1 street loading space per building tower required. 0 off-street loading spaces required.</p> <p>1 street loading space per tower provided on Arballo Drive. 0 off-street loading spaces provided.</p> <p>Refer to DRA A0.01</p>															
	<table><tr><th>Land Use</th><th>Maximum Parking Spaces</th><th>Off-Street Loading Spaces</th></tr><tr><td rowspan="2">Residential</td><td>1 space/building (between 0 and 199 units)</td><td>0</td></tr><tr><td>2 spaces/building (over 200 du)</td><td>Service vehicle spaces should be provided within garages</td></tr><tr><td>Grocery Store</td><td>2 spaces</td><td>2 spaces</td></tr><tr><td>Retail/Office/ Professional Services</td><td>1 space/building</td><td>0</td></tr></table>	Land Use	Maximum Parking Spaces	Off-Street Loading Spaces	Residential	1 space/building (between 0 and 199 units)	0	2 spaces/building (over 200 du)	Service vehicle spaces should be provided within garages	Grocery Store	2 spaces	2 spaces	Retail/Office/ Professional Services	1 space/building	0		
Land Use	Maximum Parking Spaces	Off-Street Loading Spaces															
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Grocery Store	2 spaces	2 spaces															
Retail/Office/ Professional Services	1 space/building	0															
04.03.03 Off-Street Loading Spaces	Individual off-street loading spaces shall have a maximum width of 10 feet and a maximum vertical clearance of 16 feet.	No off-street loading will be provided															
04.03.04 Loading Access	Off-street loading access is not permitted along Juan Bautista Circle, Crespi Drive, Font Boulevard and Gonzalez Drive.	No off-street loading access is not provided along Gonzales Drive.															
04.03.05 Limited Impact	A maximum of one curb cut for loading and service is permitted every 250 linear feet of street frontage.	1 curb cut provided on Serrano Drive and 1 curb cut provided on Gonzales Drive.															
04.03.06 Loading Entrances	Off-street loading entrances are restricted to a maximum linear width of 24 feet for combined entrance and exit areas.	No off-street loading provided.															
04.03.07 Visual Impact	<p>Loading and service areas must include either opaque or translucent garage door panels.</p> <p>Exterior wall finishes and architectural treatments must extend a minimum of 30 inches into the loading and service entries beyond the garage door.</p> <p>Loading entries must be well lit at night and obscure views into loading areas under daylight and night light conditions.</p>	No off-street loading provided.															